

NOTICE OF A REGULAR MEETING

Pursuant to Section 54954.2 of the Government Code of the State of California, a Regular meeting of the City of Tracy Planning Commission is hereby called for:

Date/Time: Wednesday, August 27, 2014
7:00 P.M. (or as soon thereafter as possible)

Location: City of Tracy Council Chambers
333 Civic Center Plaza

Government Code Section 54954.3 states that every public meeting shall provide an opportunity for the public to address the Planning Commission on any item, before or during consideration of the item, however no action shall be taken on any item not on the agenda.

REGULAR MEETING AGENDA

CALL TO ORDER

PLEDGE OF ALLEGIANCE

ROLL CALL

MINUTES APPROVAL

DIRECTOR'S REPORT REGARDING THIS AGENDA

ITEMS FROM THE AUDIENCE - - *In accordance with Procedures for Preparation, Posting and Distribution of Agendas and the Conduct of Public Meetings, adopted by Resolution 2008-140 any item not on the agenda brought up by the public at a meeting, shall be automatically referred to staff. If staff is not able to resolve the matter satisfactorily, the member of the public may request a Commission Member to sponsor the item for discussion at a future meeting.*

1. OLD BUSINESS
2. NEW BUSINESS
 - A. PUBLIC HEARING TO CONSIDER A CONDITIONAL USE PERMIT APPLICATION TO ALLOW A SPORTS AND FITNESS TRAINING FACILITY AT 3402 MARS CT. APPLICANT IS ELITE TRAINING ACADEMY OF CALIFORNIA, LLC. PROPERTY OWNER IS STIP SPE I, LLC. APPLICATION NUMBER CUP14-0004
 - B. PUBLIC HEARING TO CONSIDER A CONDITIONAL USE PERMIT APPLICATION TO ALLOW A DOG DAYCARE/ KENNEL AT 157 SLOAN CT. APPLICANT IS LARRY DUNN. PROPERTY OWNER IS JOHN PERINE. APPLICATION NUMBER CUP14-0008
 - C. PUBLIC HEARING TO CONSIDER A PROPOSAL TO AMEND THE I-205 CORRIDOR SPECIFIC PLAN DESIGNATION FROM GENERAL COMMERCIAL TO HIGH DENSITY RESIDENTIAL (APPLICATION NUMBER SPA14-0001), APPROVE A PLANNED UNIT DEVELOPMENT (PUD) PRELIMINARY AND FINAL DEVELOPMENT PLAN (APPLICATION NUMBER

D14-0011) FOR A 441-UNIT MULTI-FAMILY APARTMENT PROJECT, APPROVE AN OFF-STREET PARKING SPACE REDUCTION FOR THE APARTMENT PROJECT, AND APPROVE AN ADDENDUM TO THE FILIOS/DOBLER ANNEXATION AND DEVELOPMENT PROJECT ENVIRONMENTAL IMPACT REPORT (SCH NO. 2010072043). THE APPROXIMATELY 22.22-ACRE SUBJECT PROPERTY PROPOSED FOR SPECIFIC PLAN AMENDMENT IS LOCATED ON THE SOUTH SIDE OF GRANT LINE ROAD, NORTH OF THE UNION PACIFIC RAILROAD TRACKS, EAST OF BYRON ROAD AND APPROXIMATELY 600 FEET WEST OF LAMMERS ROAD, (ASSESSOR'S PARCEL NUMBERS 209-270-10, 11, 30, AND 31). THE PUD PRELIMINARY AND FINAL DEVELOPMENT PLAN IS PROPOSED FOR APPROXIMATELY 20.04 ACRES ON THE EAST SIDE OF THE SITE, ASSESSOR'S PARCEL NUMBERS 209-270-30 AND 31. THE APPLICANT IS DENTON KELLEY, SR95 VENTURES, LLC.

D . REPORT OF GENERAL PLAN CONSISTENCY FOR CAPITAL IMPROVEMENT PROGRAM PROJECTS FOR FISCAL YEAR 2014/2015 THROUGH FISCAL YEAR 2018/2019 - APPLICATION NUMBER DET14-001

3. ITEMS FROM THE AUDIENCE
4. DIRECTOR'S REPORT
5. ITEMS FROM THE COMMISSION
6. ADJOURNMENT

Posted: **August 21, 2014**

The City of Tracy complies with the Americans with Disabilities Act and makes all reasonable accommodations for the disabled to participate in public meetings. Persons requiring assistance or auxiliary aids in order to participate should call City Hall (209-831-6000), at least 24 hours prior to the meeting.

Any materials distributed to the majority of the Planning Commission regarding any item on this agenda will be made available for public inspection in the Development and Engineering Services department located at 333 Civic Center Plaza during normal business hours.

August 27, 2014

AGENDA ITEM 2 A

REQUEST

PUBLIC HEARING TO CONSIDER A CONDITIONAL USE PERMIT APPLICATION TO ALLOW A SPORTS AND FITNESS TRAINING FACILITY AT 3402 MARS CT. APPLICANT IS ELITE TRAINING ACADEMY OF CALIFORNIA, LLC. PROPERTY OWNER IS STIP SPE I, LLC. APPLICATION NUMBER CUP14-0004

DISCUSSION

The Applicant has requested that this item be removed from the Planning Commission agenda because they are reevaluating the project and may change the project description and/or the project site.

RECOMMENDATION

Staff recommends that Planning Commission remove this item from the agenda and take no action.

Prepared by: Scott Claar, Associate Planner
Reviewed by: Bill Dean, Assistant Development Services Director
Approved by: Andrew Malik, Development Services Director

AGENDA ITEM 2 B

REQUEST

**PUBLIC HEARING TO CONSIDER A CONDITIONAL USE PERMIT
APPLICATION TO ALLOW A DOG DAYCARE/ KENNEL AT 157 SLOAN CT.
APPLICANT IS LARRY DUNN. PROPERTY OWNER IS JOHN PERINE.
APPLICATION NUMBER CUP14-0008**

DISCUSSION

Site and Project Description

The subject property is located at 157 Sloan Court, which is north of Larch Road and west of Holly Drive (Attachment A: Location Map). The site is approximately 1.7 acres and contains an approximately 30,000 s.f. building.

The proposed use consists of a dog daycare/kennel. The facility would be located in an approximately 19,900 s.f. tenant space (Attachment B: Site Plan and Floor Plan). The facility would include approximately 63 kennels, which could house one dog each. Two dogs per kennel could also be accommodated for dogs from the same family. The facility would include indoor play areas for the dogs, separated into areas for small, medium, and large-size dogs. An outdoor play area is also proposed. The outdoor play area would be located adjacent to the west side of the building and enclosed by a six-foot high chain link fence. The business would operate with approximately four to five employees per shift.

The site is zoned Light Industrial (M1) and designated Industrial by the City's General Plan. The surrounding areas to the north, west, south, and east are also zoned Light Industrial (M1). The proposed use is allowed in the M1 zone with approval of a Conditional Use Permit.

Land Use Compatibility

The proposed use could potentially produce loud noises, such as dog barking, at intermittent times throughout the day and night. Due to these potential noises, this type of use would be more compatible in an industrial area than in a retail area or residential area. Industrial areas are typically more tolerant of higher levels of noise. There are no sensitive noise receptors located near the site, such as schools, hospitals, rest homes, long-term care and mental-care facilities. This particular building currently contains a food manufacturer/distributor. The proposed use would be compatible with the uses permitted on this site and in the surrounding area.

Parking

The Tracy Municipal Code requires one parking space per staff person for daycare uses. This requirement is for child daycare, but from the standpoint of parking, a dog daycare would function virtually the same.

The existing site has a total of 40 parking spaces, which equates to one parking space per 750 square feet. The proposed tenant space has 26 parking spaces associated with it. The proposed use would operate with approximately four to five employees per shift. Based on the above requirement for daycare uses, the minimum parking requirement would be 5 spaces. Therefore, the site would have a surplus of 21 parking spaces for the proposed use.

Environmental Document

This project is categorically exempt from the California Environmental Quality Act (CEQA) pursuant to CEQA Guidelines Section 15332, which pertains to certain infill development projects, because the project is consistent with the General Plan and Zoning; occurs within City limits on a project site of no more than five acres substantially surrounded by urban uses; has no value as habitat for endangered, rare or threatened species; would not result in any significant effects relating to traffic, noise, air quality, or water quality; and can be adequately served by all required utilities and public services. Therefore, no further environmental assessment is necessary.

RECOMMENDATION

Staff recommends that the Planning Commission approve a Conditional Use Permit to allow a dog daycare/ kennel at 157 Sloan Ct., Application Number CUP14-0008, based on the findings and subject to the conditions contained in the Planning Commission Resolution dated August 27, 2014 (Attachment C: Planning Commission Resolution).

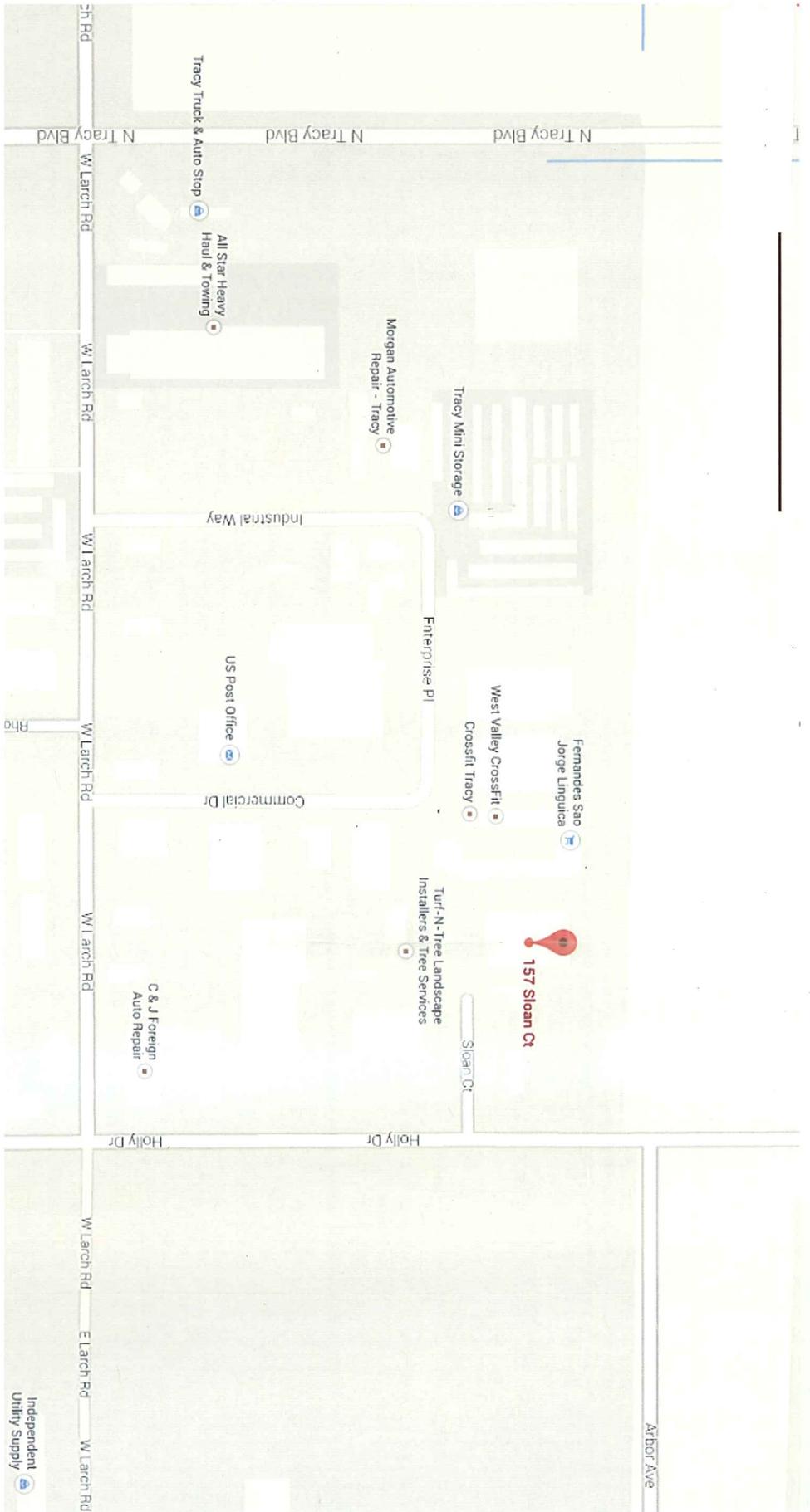
MOTION

Move that the Planning Commission approve a Conditional Use Permit to allow a dog daycare/kennel at 157 Sloan Court, Application Number CUP14-0008, based on the findings and subject to the conditions contained in the Planning Commission Resolution dated August 27, 2014.

Prepared by: Scott Claar, Associate Planner
Reviewed by: Alan Bell, Senior Planner
Approved by: Andrew Malik, Development Services Director

ATTACHMENTS

- A: Location Map
- B: Site Plan and Floor Plan
- C: Planning Commission Resolution



Independent
Utility Supply

RESOLUTION _____

APPROVAL OF A CONDITIONAL USE PERMIT TO ALLOW
A DOG DAYCARE/ KENNEL AT 157 SLOAN COURT
APPLICATION NUMBER CUP14-0008

WHEREAS, On July 15, 2014, Larry Dunn submitted an application to the Development Services Department for a Conditional Use Permit to allow a dog daycare/ kennel at 157 Sloan Court (Application Number CUP14-0008), and

WHEREAS, The subject property is zoned Light Industrial (M1) and designated Industrial by the City's General Plan, and

WHEREAS, The proposed use is a conditionally permitted use in the M1 Zone, and

WHEREAS, In accordance with Section 10.08.4250 of the Tracy Municipal Code, the Planning Commission is empowered to grant or to deny applications for Conditional Use Permits and to impose reasonable conditions upon the granting of use permits, and

WHEREAS, The project is categorically exempt from the California Environmental Quality Act (CEQA) pursuant to CEQA Guidelines Section 15332, which pertains to certain infill development projects, because the project is consistent with the General Plan and Zoning; occurs within City limits on a project site of no more than five acres substantially surrounded by urban uses; has no value as habitat for endangered, rare or threatened species; would not result in any significant effects relating to traffic, noise, air quality, or water quality; and can be adequately served by all required utilities and public services, and

WHEREAS, The Planning Commission held a public hearing to review and consider the Conditional Use Permit application on August 27, 2014;

NOW, THEREFORE BE IT RESOLVED, That the Planning Commission does hereby approve a Conditional Use Permit to allow a dog daycare/kennel at 157 Sloan Court. The approval of Application Number CUP14-0008 is based on the following findings and subject to the conditions as stated in Exhibit "1" attached and made part hereof:

1. There are circumstances or conditions applicable to the land, structure, or use that make the granting of a use permit necessary for the preservation and enjoyment of a substantial property right because the proposed use is not allowed unless the Planning Commission grants approval of a Conditional Use Permit.
2. The proposed location of the conditional use is in accordance with the objectives of Tracy Municipal Code Chapter 10.08 and the purposes of the zone in which the site is located because the proposed use is allowed in the Light Industrial (M1) Zone if the Planning Commission approves a Conditional Use Permit.
3. The proposed location of the use and the conditions under which it would be operated or maintained will not be detrimental to the public health, safety, or welfare or materially injurious to, or inharmonious with, properties or improvements in the vicinity because there are no sensitive noise receptors located near the site, such as schools, hospitals, rest homes, long-term care and mental-care facilities; the proposed use would be compatible

with the uses permitted on this site and in the surrounding area; and the proposed use will comply with the City of Tracy General Plan, Light Industrial (M1) Zone, and applicable requirements of Chapter 10.08 of the Tracy Municipal Code.

- 4. The proposed use will comply with each of the applicable provisions of Chapter 10.08 of the Tracy Municipal Code, Zoning Ordinance because subject to approval by the Planning Commission for a Conditional Use Permit, the proposed project will be required to comply with all applicable provisions including, but not limited to, the Tracy Municipal Code, the California Building Code, the City of Tracy Standard Plans, and the Uniform Fire Code.

* * * * *

The foregoing Resolution _____ was adopted by the Planning Commission on the 27th day of August 2014, by the following vote:

AYES: COMMISSION MEMBERS
NOES: COMMISSION MEMBERS
ABSENT: COMMISSION MEMBERS
ABSTAIN: COMMISSION MEMBERS

CHAIR

ATTEST:

STAFF LIAISON

**CITY OF TRACY
CONDITIONS OF APPROVAL FOR A CONDITIONAL USE PERMIT
TO ALLOW A DOG DAYCARE/KENNEL AT 157 SLOAN COURT
ASSESSOR'S PARCEL NUMBER 212-220-03
APPLICATION NUMBER CUP14-0008**

A. General Provisions and Definitions

1. These Conditions of Approval shall apply to the Conditional Use Permit approval for a dog daycare/kennel at 157 Sloan Court, Assessor's Parcel Number 212-220-03, Application Number CUP14-0008 (hereinafter "Project") proposed by Larry Dunn (hereinafter "Applicant").
2. The following definitions shall apply to these Conditions of Approval:
 - a. "Applicant" means any person, or other legal entity, defined as a "Developer".
 - b. "City Engineer" means the City Engineer of the City of Tracy, or any other duly licensed engineer designated by the City Manager, the Development Services Director, or the City Engineer to perform the duties set forth herein.
 - c. "City Regulations" mean all written laws, rules, and policies established by the City, including those set forth in the City of Tracy General Plan, the Tracy Municipal Code, ordinances, resolutions, policies, procedures, and the City's Design Documents (including the Standard Plans, Standard Specifications, Design Standards, and relevant Public Facility Master Plans).
 - d. "Conditions of Approval" shall mean the conditions of approval applicable to the Conditional Use Permit for Application Number CUP14-0008.
 - e. "Developer" means any person, or other legal entity, who applies to the City to divide or cause to be divided real property within the Project boundaries, or who applies to the City to develop or improve any portion of the real property within the Project boundaries. The term "Developer" shall include all successors in interest.
 - f. "Development Services Director" means the Development Services Director of the City of Tracy, or any other person designated by the City Manager or the Development Services Director to perform the duties set forth herein.
 - g. "Project" means the Conditional Use Permit approval for a dog daycare/kennel at 157 Sloan Court, Assessor's Parcel Number 212-220-03, Application Number CUP14-0008.
 - h. "Property" means the real property located at 157 Sloan Court, Assessor's Parcel Number 212-220-03, which is the subject of a Conditional Use Permit approval for a dog daycare/kennel, Application Number CUP14-0008.

3. The Developer shall comply with all laws (federal, state, and local) related to the development of real property within the Project, including, but not limited to: the Planning and Zoning Law (Government Code sections 65000, et seq.), the California Environmental Quality Act (Public Resources Code sections 21000, et seq., "CEQA"), the Guidelines for California Environmental Quality Act (California Administrative Code, title 14, sections 1500, et seq., "CEQA Guidelines"), Uniform Building Code, and Uniform Fire Code.
4. Unless specifically modified by these Conditions of Approval, the Developer shall comply with all City Regulations.
5. Any violation of State or Federal Law or local ordinances shall be grounds for revocation of the conditional use permit.
6. Pursuant to Government Code section 65009, including section 65009(e)(1), the City HEREBY NOTIFIES the applicant that any action challenging these conditions must be commenced, in writing, within 90 days of the approval of this conditional use permit.

B. Planning Division Conditions

1. The project shall be developed in substantial compliance with the project description received by the Development Services Department on July 15, 2014, and the site plan and floor plan received August 19, 2014, except as modified herein, to the satisfaction of the Development Services Director.
2. All dog urine and feces shall be drained to the City's wastewater (sanitary sewer) system or disposed in a solid waste (garbage) container, to the satisfaction of the Public Works Director. No urine or feces shall be drained to the City's storm drain system. Prior to issuance of a Building Permit, the Applicant shall submit plans that demonstrate how this requirement will be satisfied, to the satisfaction of the Public Works Director.
3. Prior to occupancy, the Applicant shall comply with all requirements of the California State Building Code and Fire Code, to the satisfaction of the City's Chief Building Official.
4. All new roof mounted equipment, including, but not limited to, HVAC units, vents, fans, antennas, sky lights and dishes shall be screened from view from any public right-of-way by parapet walls at least equal in height to the height of the equipment installed, to the satisfaction of the Development Services Director. No roof-mounted equipment, whether proposed as part of this application, potential future equipment, or any portion thereof, shall be visible from the public right-of-way.

5. No signs are approved as part of this application. All signs for the project shall be reviewed under a separate sign permit application in accordance with Tracy Municipal Code standards.

AGENDA ITEM 2 C

REQUEST

PUBLIC HEARING TO CONSIDER A PROPOSAL TO AMEND THE I-205 CORRIDOR SPECIFIC PLAN DESIGNATION FROM GENERAL COMMERCIAL TO HIGH DENSITY RESIDENTIAL (APPLICATION NUMBER SPA14-0001), APPROVE A PLANNED UNIT DEVELOPMENT (PUD) PRELIMINARY AND FINAL DEVELOPMENT PLAN (APPLICATION NUMBER D14-0011) FOR A 441-UNIT MULTI-FAMILY APARTMENT PROJECT, APPROVE AN OFF-STREET PARKING SPACE REDUCTION FOR THE APARTMENT PROJECT, AND APPROVE AN ADDENDUM TO THE FILIOS/DOBLER ANNEXATION AND DEVELOPMENT PROJECT ENVIRONMENTAL IMPACT REPORT (SCH NO. 2010072043). THE APPROXIMATELY 22.22-ACRE SUBJECT PROPERTY PROPOSED FOR SPECIFIC PLAN AMENDMENT IS LOCATED ON THE SOUTH SIDE OF GRANT LINE ROAD, NORTH OF THE UNION PACIFIC RAILROAD TRACKS, EAST OF BYRON ROAD AND APPROXIMATELY 600 FEET WEST OF LAMMERS ROAD, (ASSESSOR'S PARCEL NUMBERS 209-270-10, 11, 30, AND 31). THE PUD PRELIMINARY AND FINAL DEVELOPMENT PLAN IS PROPOSED FOR APPROXIMATELY 20.04 ACRES ON THE EAST SIDE OF THE SITE, ASSESSOR'S PARCEL NUMBERS 209-270-30 AND 31. THE APPLICANT IS DENTON KELLEY, SR95 VENTURES, LLC.

DISCUSSION

Project Description

The proposal is to construct a 441-unit, multi-family residential project on approximately 20 acres (Attachment A). The existing single-family home and related structures on the site will be removed as part of the Project. Attachments B through P contain the Project's site plan, exterior building elevations, floor plans, and conceptual landscape plan.

The Project proposes a two-phase development, with roughly the eastern half of the Project (10.5 acres and 231 apartment units) developed in Phase I, and the western portion of the project (9.6 acres and 210 apartment units) developed in Phase II. An alternative phasing plan may be considered as the Project is developed. The average residential density of the Project is 22 dwelling units per acre, consistent with the General Plan and I-205 Corridor Specific Plan policies.

Two building types are proposed for the apartments. Both are 3 stories and 45-feet 3-inches in height above the finished floor grade. There will be a mix in unit types in the Project, with a total of 171 1-bedroom units, 252 2-bedroom units, and 18 3-bedroom units.

The design of the apartment buildings is modern. The buildings will utilize a neutral, earth-tone palette with colors of grey, brown and white trim. Construction materials will consist of cementitious board and batten siding, and lap siding, along with use of exterior plaster, wood eaves, steel awnings and guardrails, and composition asphalt shingle roofs. The apartments will be set back a minimum of 15 feet, 3 inches from the

front property line along Grant Line Road.

A central clubhouse with swimming pool, seating areas and playground will be provided within each of the two phases of development, and available for use by all Project residents. The clubhouses will be approximately 28-feet high, and each will contain a fitness room, mail room, lounge, conference room, restrooms, small dining room, and office space. Restroom buildings will be located by the swimming pools, also containing space for pool and maintenance equipment. The clubhouse and restroom buildings will be designed consistent with the primary apartment buildings, though will utilize decorative stove siding and metal seam roofs. Tot lots will also be provided in each Project phase.

A total of 796 parking spaces will be provided, equaling 1.80 spaces/apartment unit. The majority of these spaces will be in carports, ensuring at least one covered parking space per apartment unit. The number of parking spaces would be less than required by the Municipal Code, and the applicant has requested an adjustment to the City's parking requirements (see discussion below.) There will also be 88 guest parking spaces (1 space per each 5 apartment units). Additionally, there will be 102 garage units. The garages will be available for rent to any of the residents on a first come, first served basis. The garages will be located in a series of buildings, approximately 17-feet high, along the southern project boundary, providing a partial noise and visual buffer from the adjoining Union Pacific Railroad line. The garage building design and use of construction materials will be consistent with the primary apartment buildings.

Trash enclosures and enclosed trash compactor areas will be located at several areas in the parking lots, within walking distance of each apartment unit. Landscaping will include use of a combination of trees (including Chinese elm, valley oak and red oak, Whitehouse pear and fruitless olive trees), and extensive shrub and groundcover. A combination of Project perimeter fencing and walls is proposed, using six-foot tall concrete masonry walls and ornamental metal fencing.

The project is proposed to be gated. Access to the property would come from a central, signalized driveway connecting to Grant Line Road. Two additional driveways would also provide access to Grant Line Road, though each would have restricted (right-in, right-out) turn movements. A fourth encroachment onto Grant Line Road would be located at the east edge of the Project site, and be gated and used as an emergency vehicle access until such time as other development along Grant Line Road occurs, at which time, the access may be converted to a driveway for resident use. A series of interior driveways will provide access to on-site parking areas. The project would connect to City water, wastewater and storm drainage systems.

Engineering Division recommended Condition of Approval Number C.3.2.1 (on page 17, Exhibit 2 to the Resolution) references an "Option To Build One Major Entrance". The One Major Entrance option would replace the two primary driveways to Grant Line Road (Attachment B) with one driveway centrally located between them. The central driveway would be signalized and located where the "dead-end" parking area drive aisle terminates at Grant Line Road near the center of the site along Grant Line Road. The main entrances shown on Attachment B would be replaced with dead-end parking area drive aisles. The total number of units is not proposed to change with the One Major Entrance option. This concept was introduced late in the review process and a site plan illustrating this option has not yet been prepared. If the modifications to the site plan are modest, and otherwise

in conformance with the Project as proposed (similar number of units, parking, building design, buildings fronting the street, etc.) staff is recommending that this option be evaluated and approved by the Development Services Director for conformance with City standards prior to issuance of grading or building permits. If the Planning Commission prefers this level of modification be reviewed by the Planning Commission and City Council, Condition of Approval Number C.3.2.1 should be removed and the developer would need to obtain Planning Commission and City Council review through an amendment to the Development Plan if the developer decides to pursue the One Major Entrance option.

I-205 Corridor Specific Plan Amendment – Adjoining Maibes Property

The subject property was annexed to the City in 2011 as part of the 43-acre, Filios/Dobler annexation. It was added to the I-205 Corridor Specific Plan and designated General Commercial. No specific improvements or development applications were proposed to the City at the time, or since.

The application proposes the amendment of the I-205 Corridor Specific Plan land use designation from General Commercial to High Density Residential. The proposed land use designation would be consistent with the overall land use program prescribed by the Specific Plan. High Density Residential uses would support existing and planned commercial and mixed uses in the area, and would be fully supported by necessary infrastructure and public services. The City does not currently experience a shortage of vacant commercial property. In fact, the significant supply of vacant commercial property along I-205 within Cordes Ranch, the Tracy Hills project, and elsewhere in town suggests a supply for many years of new commercial development. A General Plan Amendment is not required because high density residential development (12.1 to 25.0 units per acre) is allowed under the project site's Commercial General Plan designation. The Project would result in a residential density of 22 units/acre, consistent with the General Plan.

Two additional parcels (known as the Maibes property) are included in the Tracy Apartments project Specific Plan amendment. The Maibes property totals 2.18 acres, contains a single-family home, trailer, and shop/storage buildings, and extends immediately west of the Tracy Apartments properties to the intersection of Grant Line Road and Byron Road. While no development is proposed at this time on this 2.18 acres, City staff is requesting the inclusion of the Maibes property in the I-205 Corridor Specific Plan amendment action, changing the General Commercial designation to High Density Residential. For the purposes of this action, the Maibes property is assumed for ultimate development of multi-family residential housing at a density comparable to the Tracy Apartments project, resulting in approximately 48 units.

The owners of the Maibes property are not associated with the project applicant and did not request a Specific Plan amendment of their property from General Commercial to High Density Residential (just as they were not an applicant of the annexation application or resulting change from San Joaquin County zoning to City of Tracy commercial zoning in 2011).

Nevertheless, as with the annexation application several years ago, City staff and the Maibes property representatives have met and discussed the current proposal during the

past several months. At the time of publication of this report, the Maibes property owners have not expressed specific support of or opposition to the proposed Specific Plan amendment from General Commercial to High Density Residential.

City staff requested the Maibes property be included in the Specific Plan amendment, even though the Tracy Apartments Project does not include the Maibes property, based on the following considerations: vehicle access, land use compatibility, and orderly growth of the City. The triangular-shaped Maibes property will be isolated from other properties with the Union Pacific rail line on its southwest side and Grant Line Road (future four-lane arterial) on the north. Upon development, the Maibes property may not be permitted direct access to Grant Line Road, or only right-in/right-out, due to the road's travel speed, proximity to other intersections, and curved configuration of Grant Line Road at this location in the future. The City's Transportation Master Plan shows that at build-out, Grant Line Road will not intersect with Byron Road, and Grant Line Road will, instead, curve north as it heads west from the Tracy Apartments site.

Due to the shape and size of the Maibes property, it is conceivable that it could be available only to a single user, such as a gas station, fast food restaurant, or retail store. Such smaller, strip-type commercial development is inconsistent with Tracy's commercial development patterns of the past 25 years that have focused on larger, commercial centers along arterial streets where customers can exit the roadway and have access to multiple retailers before travelling back on to the public right-of-way. Small commercial sites are more appropriate for neighborhood shopping spots along smaller, collector streets.

The Maibes property was added to the Filios/Dobler annexation of 2011 because it was a logical extension for connection and integrated development with the larger, adjacent (now proposed Tracy Apartments) site. If the Specific Plan designation of the 20-acre Tracy Apartments site is amended from General Commercial to High Density Residential, staff believes the designation of the Maibes property should be amended as well, to help ensure compatible, adjoining land uses; coordinated access and utilities; and appropriate land use on this otherwise isolated parcel, too small to create a meaningful commercial center on its own.

Parking – Tracy Municipal Code Amendment

City parking standards require 1.5 off-street parking spaces per one-bedroom unit, 2.0 spaces per unit with two or more bedrooms, and 1.0 guest space for every five units. This 441-unit project would require 885 off-street parking spaces under City parking standards, as shown in the following summary table.

Unit Type	City Parking Standard	Number of Units Proposed	Off-Street Parking Required by Code
1 Bedroom	1.5 spaces/unit	171	257
2 Bedroom	2.0 spaces/unit	252	504
3 Bedroom	2.0 spaces/unit	18	36
Guest Parking	1 space/5 units	441	88
		441 Units	885 Spaces

The Project, by contrast, proposes a total of 796 off-street parking spaces – 89 (or 10%) fewer spaces than is required by City parking standards.

The number of off-street parking spaces required for multi-family projects by the City of Tracy is higher than many other jurisdictions. Recognizing this, the City Council adopted an ordinance in 2012, consistent with City General Plan and Sustainability Action Plan policies, providing for a reduction of up to 20% of a project's required parking when a parking survey or study demonstrates that the number of spaces otherwise required will not be necessary to mitigate off-street parking demand.

This Code provision allows the City to consider reductions to the number of required off-street parking spaces on a case-by-case basis.

For this Project, the applicant prepared a parking survey (Attachment Q) to compare the number of parking spaces appropriate for this project.

The parking study demonstrates that 796 off-street parking spaces (1.8 parking spaces per dwelling unit) for this project will adequately mitigate the on-site parking demand. Each of the 441 apartment units would be assigned a carport space, with another 289 surface spaces unassigned and 102 garage spaces available for rent to Project residents.

The parking space reduction requested is 10% of the total number of spaces otherwise required. During the past two years, the City Council (after review by the Planning Commission) approved parking space reductions for the 301-unit Aspire Apartment project (14% reduction) and the 60-unit MacDonald Apartment project (15.4% reduction). Neither of these projects has yet been constructed.

Staff is recommending approval of the parking reduction request, determining that 1.8 parking spaces per dwelling unit are adequate for this Project.

Public Schools

The Project site is located within the Tracy Unified School District related to K through 12th grade education. School-aged children who reside within the Project would be in the attendance boundary areas for Melville S. Jacobsen Elementary School, Monte Vista Middle School, and West High School.

Attachment R is correspondence from Tracy Unified School District, summarizing their request for normal, school facilities mitigation through the payment of school mitigation fees. Recommended Condition of Approval Number B.32 references the requirement for school fee payment by the project.

CEQA DOCUMENTATION

An Addendum was prepared for the proposed Project in accordance with the California Environmental Quality Act (CEQA) and the CEQA Guidelines. The Addendum is to the previously certified Environmental Impact Report (EIR) (State Clearinghouse Number 2010072043) for the Filios/Dobler Annexation and Development Project, approved by

the City in 2011. The City of Tracy is the lead agency for the environmental review of the proposed project modifications. CEQA Guidelines Section 15164 defines an Addendum as:

The lead agency or responsible agency shall prepare an addendum to a previously certified EIR if some changes or additions are necessary but none of the conditions described in Section 15162 calling for preparation of a subsequent EIR have occurred...A brief explanation of the decision not to prepare a subsequent EIR pursuant to Section 15162 should be included in an addendum to an EIR, the lead agency's findings on the project, or elsewhere in the record.

The proposed changes to the Filios/Dobler Annexation and Development EIR, in evaluating the proposed Tracy Apartments and Maibes property, do not meet the criteria for preparing a subsequent EIR or negative declaration. These criteria include evaluation of whether there has been any substantial change proposed in the project not evaluated in the previous EIR which will require new environmental analysis due to potential for significant new environmental impacts; if there are significant changes in circumstances under which the project will be built that could not have been evaluated at the time the previous EIR was prepared; and if new information has become available that was not known at the time of the previous EIR was prepared. With the Tracy Apartments and Maibes property project, none of the above criteria are met, and therefore an Addendum to the previously certified Filios/Dobler EIR is the appropriate CEQA document. The EIR Addendum is contained in Exhibit 1 to the Planning Commission Resolution, Attachment S.

RECOMMENDATION

Staff recommends that the Planning Commission recommends that the City Council take the following action:

1. Approve the Addendum to the Filios/Dobler EIR in support of the proposed Project.
2. Approve the Amendment to the I-205 Corridor Specific Plan, changing the Tracy Apartments and Maibes properties land use designations from General Commercial to High Density Residential.
3. Approve the proposed parking reduction, determining that 1.8 off-street parking spaces per unit are sufficient to mitigate parking demands of the Project.
4. Approve a Planned Unit Development (PUD) Preliminary and Final Development Plan for a 441-unit multi-family apartment project.

MOTION

Move that the Planning Commission recommends that the City Council take the following action, as documented in the August 27, 2014 Planning Commission Resolution:

1. Approve the Addendum to the Filios/Dobler EIR in support of the proposed Project.
2. Approve the Amendment to the I-205 Corridor Specific Plan, changing the Tracy Apartments and Maibes properties land use designations from General Commercial to High Density Residential.
3. Approve the proposed parking reduction, determining that 1.8 off-street parking

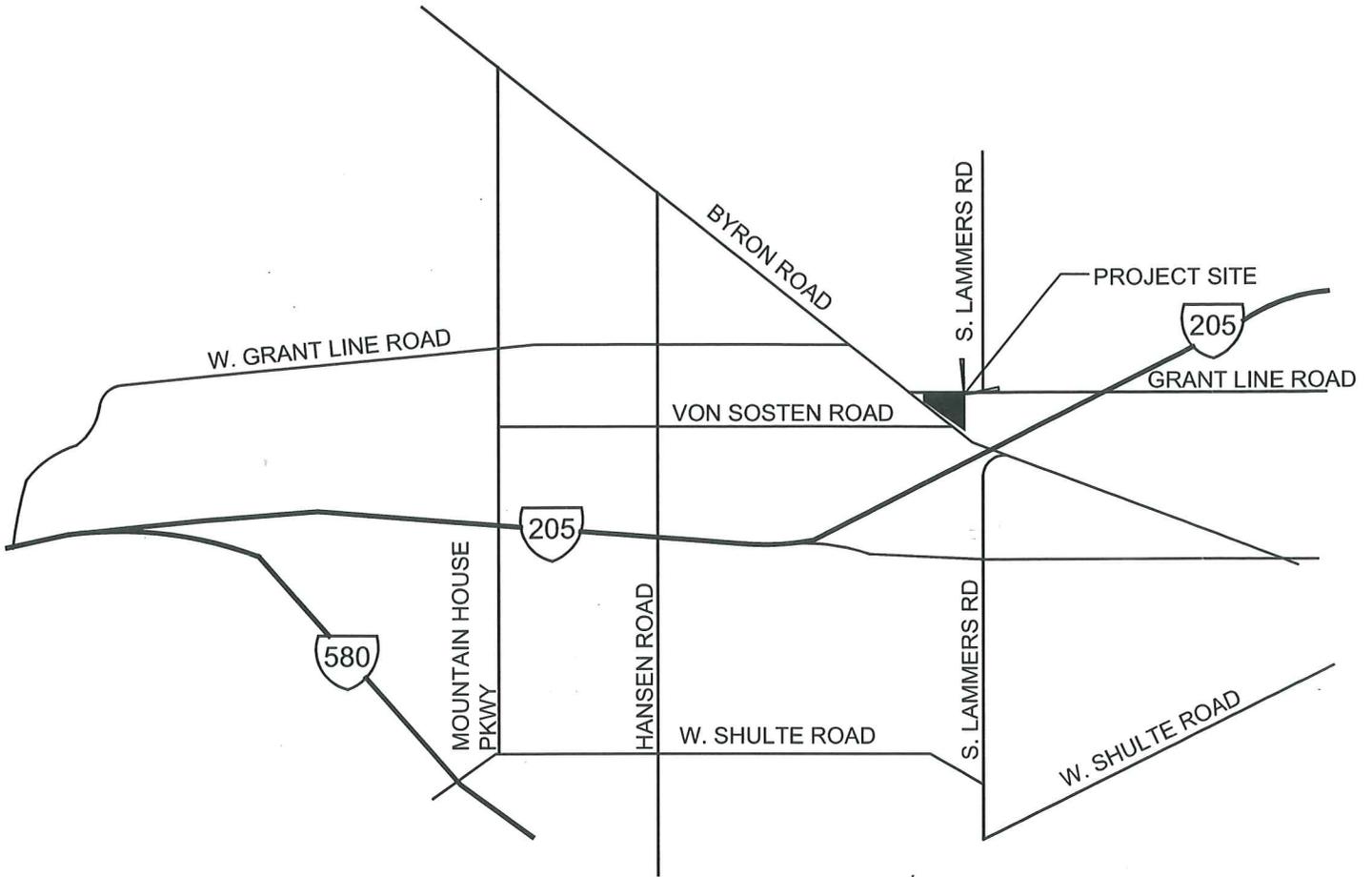
- spaces per unit are sufficient to mitigate parking demands of the Project.
4. Approve a Planned Unit Development (PUD) Preliminary and Final Development Plan for a 441-unit multi-family apartment project.

Prepared by: Brian Millar, Planning Consultant, and Alan Bell, Senior Planner
Reviewed by: Bill Dean, Assistant Development Services Director
Approved by: Andrew Malik, Development Services Director

ATTACHMENTS

Attachment A – Location Map/Specific Plan Amendment
Attachment B – Site Plan
Attachment C – Building 1 Elevations
Attachment D – Building 1 First and Second Floor Plans
Attachment E – Building 1 Third Floor and Roof Plans
Attachment F – Building 1 Perspectives
Attachment G – Building 1 Perspectives
Attachment H – Building 2 Elevations
Attachment I – Building 2 First and Second Floor Plans
Attachment J – Building 2 Third Floor and Roof Plans
Attachment K – Building 2 Perspectives
Attachment L – Building 2 Perspectives
Attachment M – Clubhouse Plans and Elevations
Attachment N – Clubhouse Perspectives
Attachment O – Restroom and Garage Plans
Attachment P – Landscape Plan
Attachment Q – Applicant's Parking Study
Attachment R – August 20, 2014 Correspondence from Tracy Unified School District
Attachment S – Planning Commission Resolution with Project Conditions
 Exhibit 1 – EIR Addendum
 Exhibit 2 – Project Conditions of Approval

Oversized Plans – Distributed with Planning Commission packet and available for review in Development Services Department, City Hall



VICINITY MAP
N.T.S.



TRACY APARTMENTS

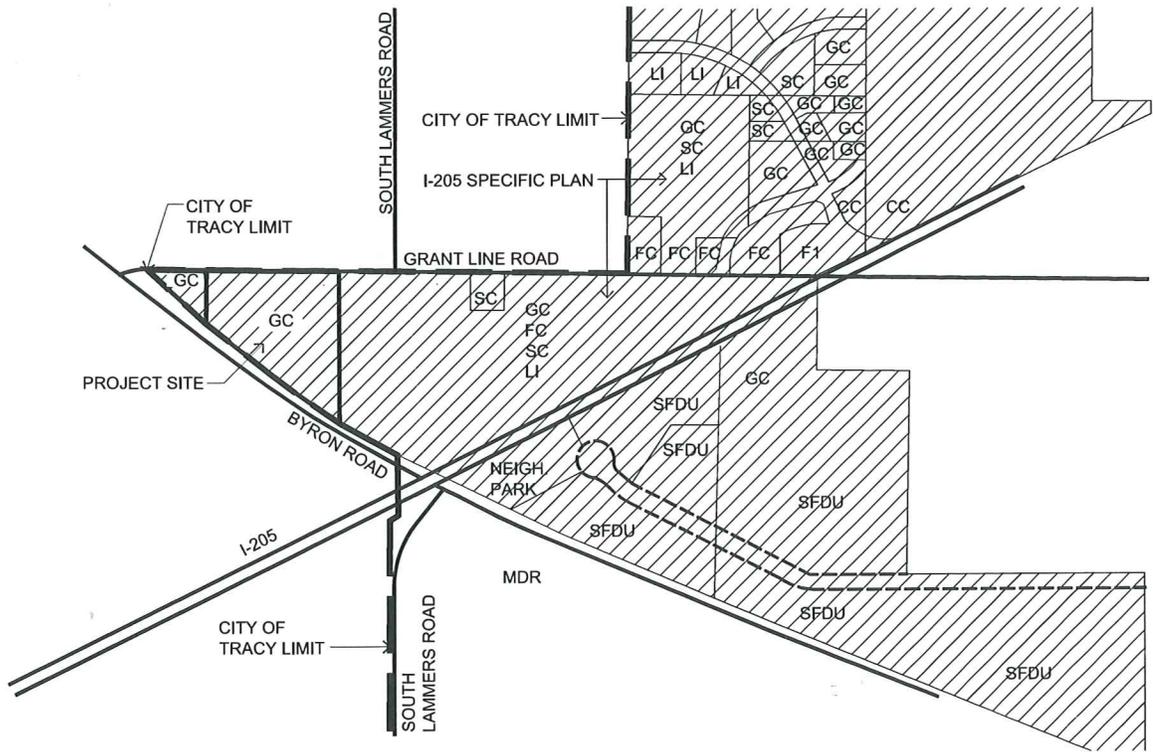
GRANT LINE ROAD

TRACY, CA

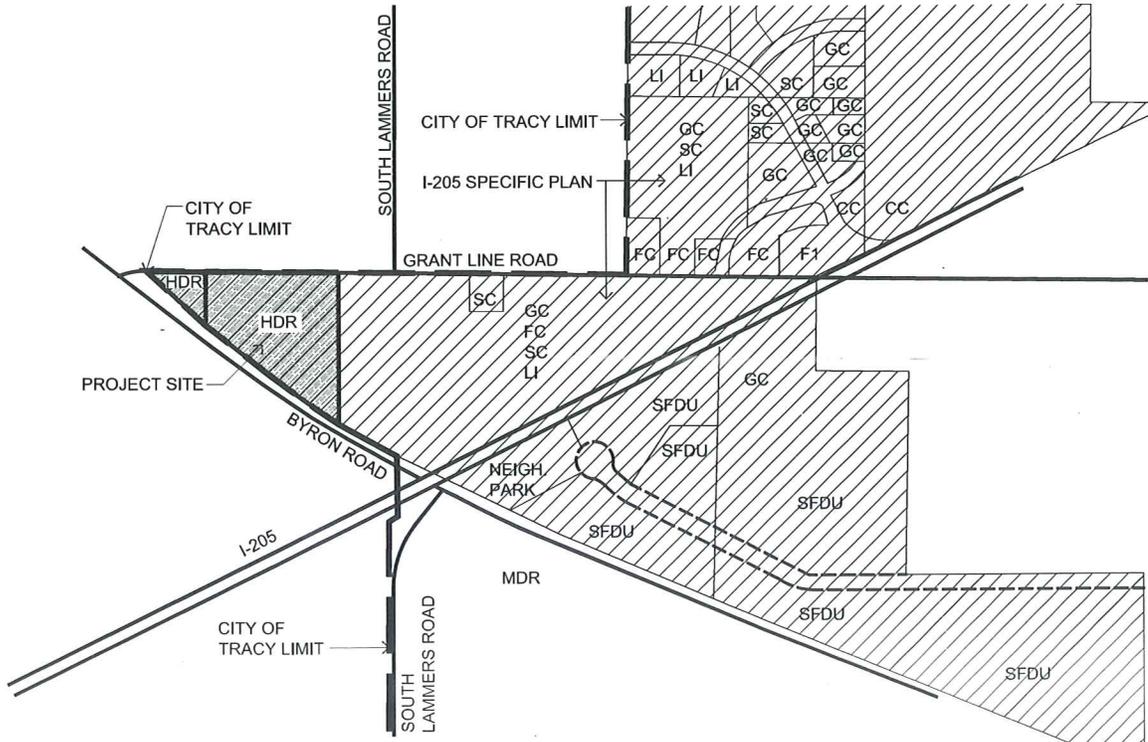
1083-0001
08.19.2014

LPAS

2484 Natomas Park Drive Suite 100 Sacramento CA 95833
916 443 0335 lpasdesign.com Architecture • Design



EXISTING I-205 SPECIFIC PLAN



PROPOSED I-205 SPECIFIC PLAN



TRACY APARTMENTS

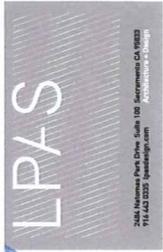
GRANT LINE ROAD

TRACY, CA

1083-0001
08.19.2014

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916 443 0335 lpasdesign.com Architecture + Design



SCALE: 1"=40'-0"

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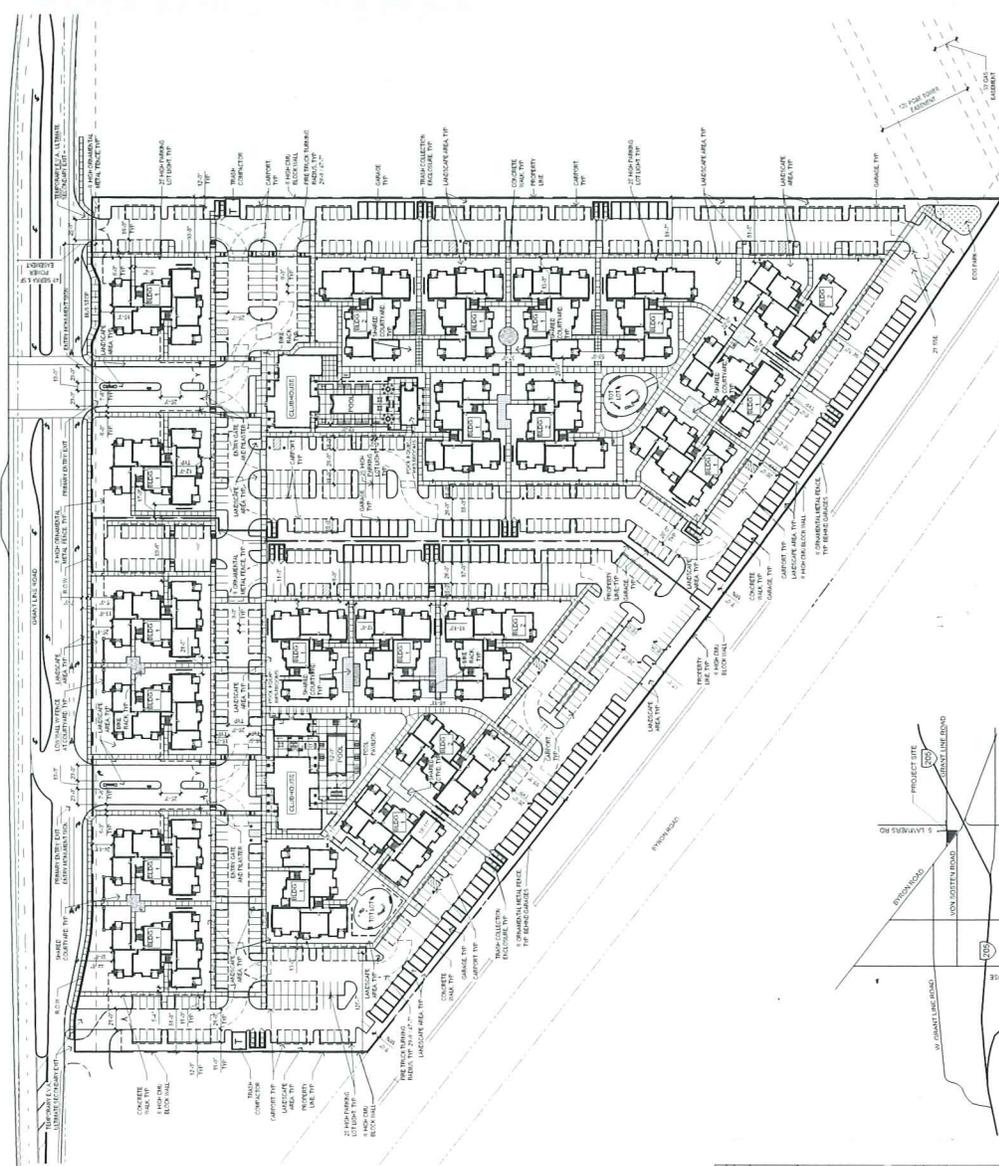


OVERALL SITE PLAN

DATE: 08-15-2014
PROJECT NO: 1183-001
SCALE:
SHEET: A1.00

N.T.S.

VICINITY MAP



LPAS

8/17/14

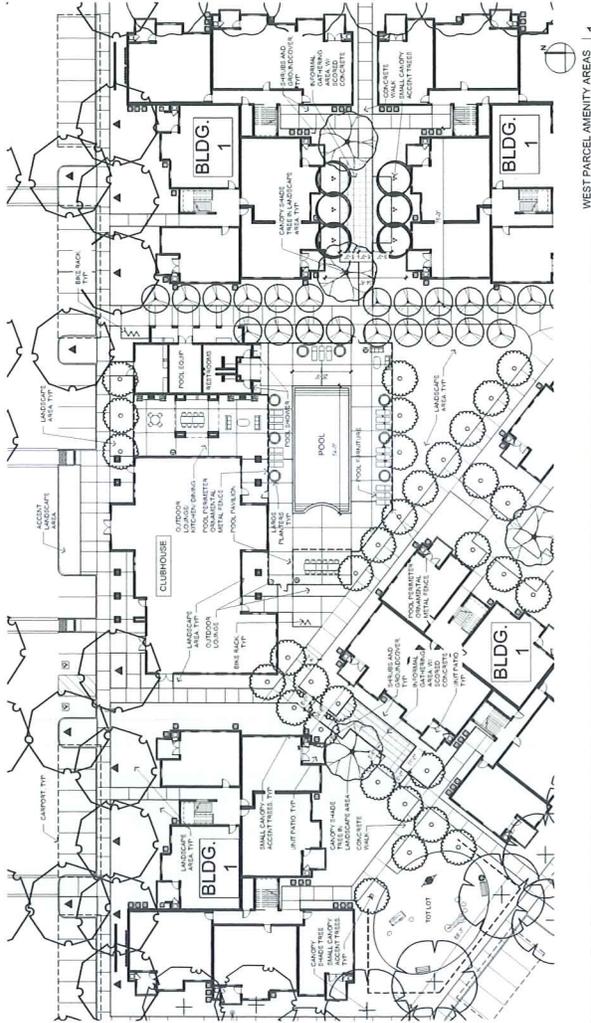
Tracy - Grant Line Road Apartments
Tracy, CA LLC
Project No.: 1183-001

USE CATEGORY	MINIMUM AREA (SQ FT)	MAXIMUM AREA (SQ FT)	MINIMUM HEIGHT (FT)	MAXIMUM HEIGHT (FT)	MINIMUM SETBACK (FT)	MAXIMUM SETBACK (FT)	MINIMUM LOT COVERAGE (%)	MAXIMUM LOT COVERAGE (%)	MINIMUM OPEN SPACE (%)	MINIMUM OPEN SPACE (SQ FT)	MINIMUM OPEN SPACE (SQ FT)
Office	10,000	100,000	10	100	10	10	10	10	10	10,000	100,000
Residential	10,000	100,000	10	100	10	10	10	10	10	10,000	100,000
Commercial	10,000	100,000	10	100	10	10	10	10	10	10,000	100,000
Industrial	10,000	100,000	10	100	10	10	10	10	10	10,000	100,000
Public Use	10,000	100,000	10	100	10	10	10	10	10	10,000	100,000
Other	10,000	100,000	10	100	10	10	10	10	10	10,000	100,000

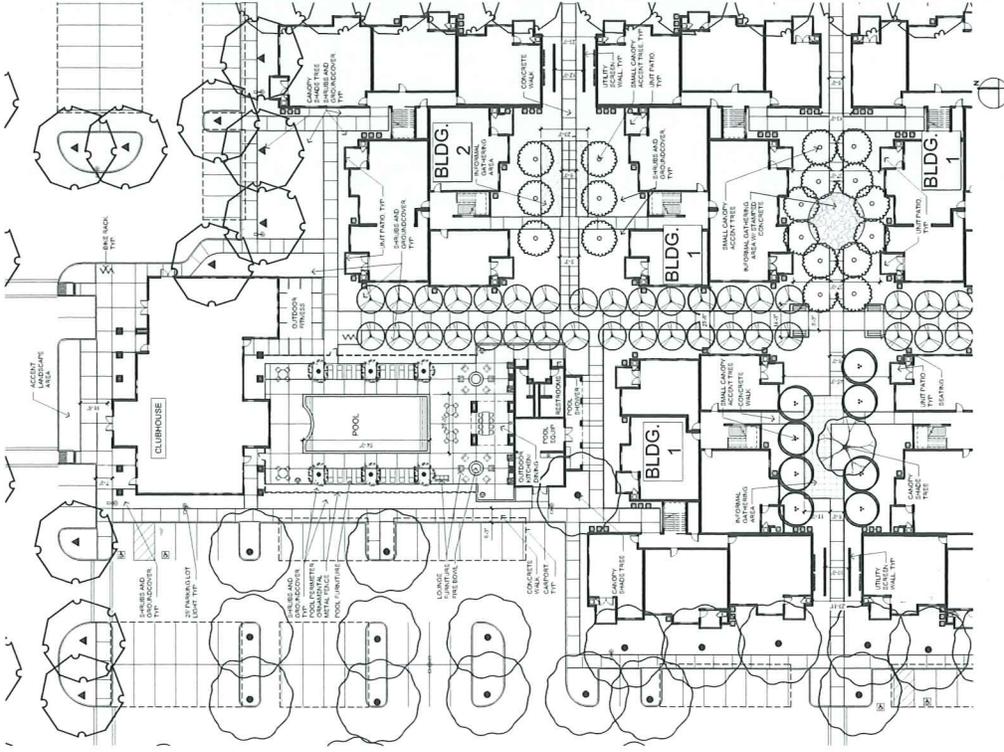
BUILDING DETAIL	TOTAL AREA (SQ FT)		TOTAL HEIGHT (FT)		TOTAL SETBACK (FT)		TOTAL LOT COVERAGE (%)		TOTAL OPEN SPACE (%)		TOTAL OPEN SPACE (SQ FT)	
	MIN	MAX	MIN	MAX	MIN	MAX	MIN	MAX	MIN	MAX	MIN	MAX
Office	10,000	100,000	10	100	10	10	10	10	10	10	10,000	100,000
Residential	10,000	100,000	10	100	10	10	10	10	10	10	10,000	100,000
Commercial	10,000	100,000	10	100	10	10	10	10	10	10	10,000	100,000
Industrial	10,000	100,000	10	100	10	10	10	10	10	10	10,000	100,000
Public Use	10,000	100,000	10	100	10	10	10	10	10	10	10,000	100,000
Other	10,000	100,000	10	100	10	10	10	10	10	10	10,000	100,000

PROJECT DATA

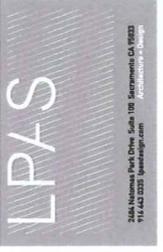
TRACY APARTMENTS
GRANT LINE ROAD
TRACY, CALIFORNIA



WEST PARCEL AMENITY AREAS
11/12/10 1



EAST PARCEL AMENITY AREAS
11/12/10 2



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ENLARGED LANDSCAPE PLAN -
AMENITIES

DATE: 08/15/2014
PROJECT NO: 1183-0001
SCALE:
SHEET: LI 01

TRACY APARTMENTS
GRANT LINE ROAD
TRACY, CALIFORNIA

SCALE: 1/8"=1'-0"



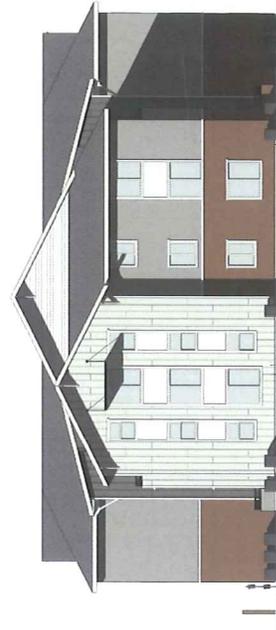
LEFT ELEVATION | 2



FRONT ELEVATION | 1



RIGHT ELEVATION | 4



REAR ELEVATION | 3

- MATERIAL LEGEND**
- 1 COMPOSITE PLANTER-BASED ROOF AT ENTRY-1/10
 - 2 CENTRIFUGAL-BONDING AND BONDING BEAMS
 - 3 CENTRIFUGAL-ADHESIVE/TAPE BEAMS
 - 4 EXTERIOR PLASTER
 - 5 CENTRIFUGAL-24/20/20/20 TRIM
 - 6 2X4 WOOD SIDING AND TRIMS
 - 7 STAINING SCALIMETAL ROOF
 - 8 CONCRETE/STAINLESS STEEL
 - 9 DECORATIVE POLYURETHANE BASED AND FLOORINGS
 - 10 4.44X0.008X1000MM BRUSH
 - 11 PAINTED FIBER STEEL UNDERLAY
 - 12 PAINTED FIBER STEEL DECORATIVE FLOOR FINISH
 - 13 PAINTED FIBER STEEL UNDERLAY
 - 14 PAINTED FIBER STEEL DECORATIVE FLOOR FINISH
 - 15 PLASTER FINISHED WALL AT 10'0" HIGH TO CEILING GAS METER

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BUILDING 1 - EXTERIOR ELEVATIONS

DATE: 8/15/14
 PROJECT NO: 1183-001
 SCALE
 SHEET: A1.3

TRACY APARTMENTS AT GRANT LINE ROAD

TRACY, CA

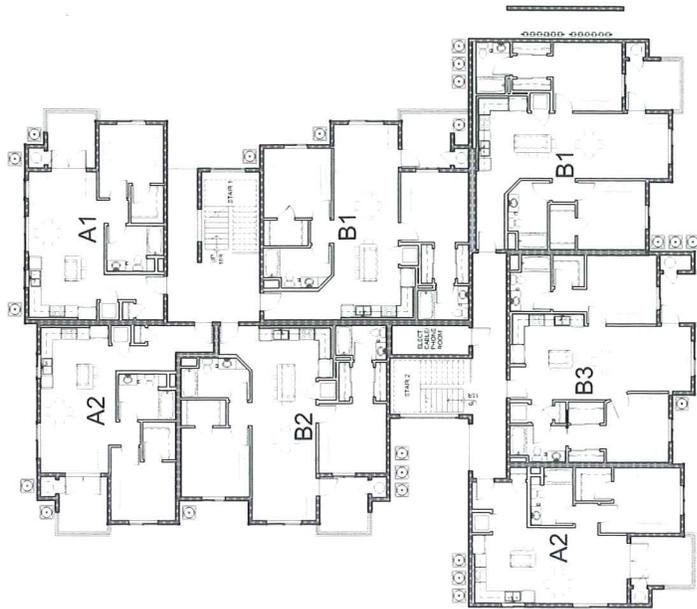
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CITY OF TRACY

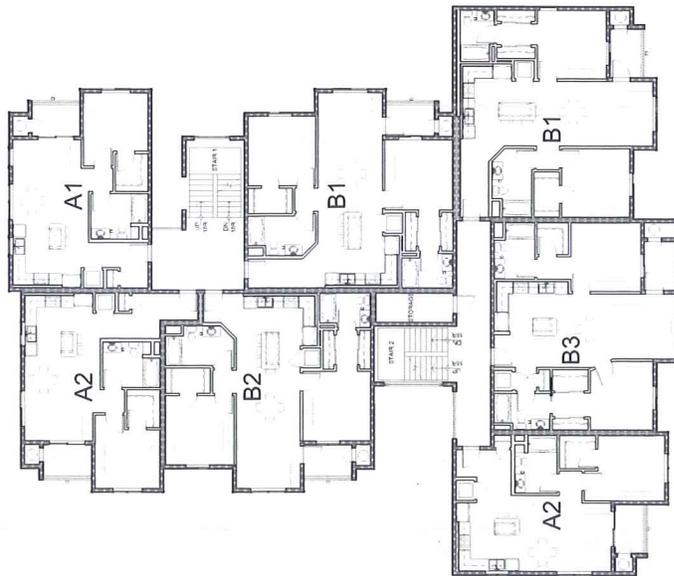
LPFS

2188 Maricopa Park Drive, Suite 100, Sacramento, CA 95822
754.241.0000 | www.lpfsc.com



FIRST FLOOR BUILDING PLAN

SECOND FLOOR BUILDING PLAN



BUILDING 1 - FIRST & SECOND FLOOR PLAN

DATE: 8/15/14
PROJECT NO: 1183-0001
SCALE:
SHEET: A1.1

TRACY APARTMENTS
AT GRANT LINE ROAD

TRACY, CA



BUILDING 1 - THIRD FLOOR AND ROOF PLAN

DATE: 8/15/14
PROJECT NO: 1183-0001
SCALE:
SHEET: A1.2

**TRACY APARTMENTS
AT GRANT LINE ROAD**

TRACY, CA

THIRD FLOOR BUILDING PLAN

ROOF PLAN

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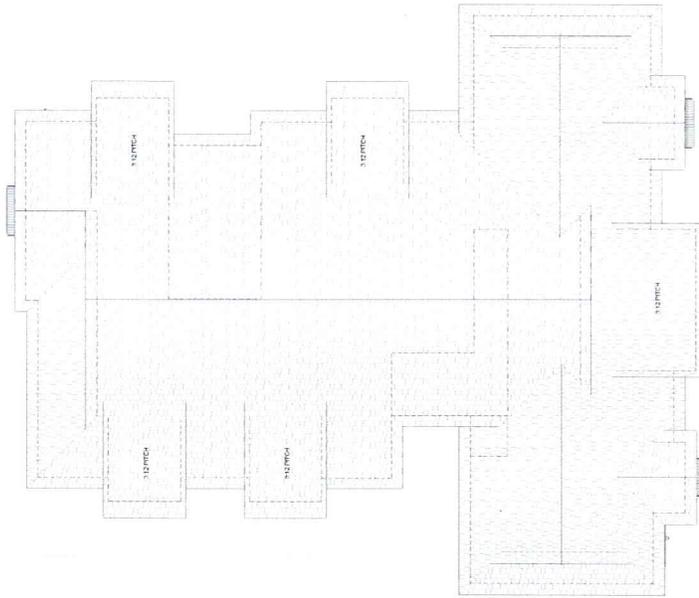
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ROOF PLAN

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CITY OF TRACY

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1100 North First Street, Suite 400 | Sacramento, CA 95833
Tel: 916.441.4200 | Fax: 916.441.4201



BUILDING 1 - PERSPECTIVES

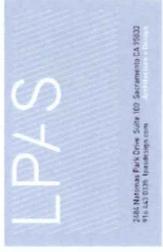
DATE: 8/15/14
PROJECT NO.: 1183-0001
SCALE:
SHEET: A1.4

TRACY APARTMENTS
AT GRANT LINE ROAD

TRACY, CA



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BUILDING 1 - PERSPECTIVES

DATE: 8/15/14
 PROJECT NO: 1183-0001
 SCALE:
 SHEET: A15

TRACY APARTMENTS
 AT GRANT LINE ROAD

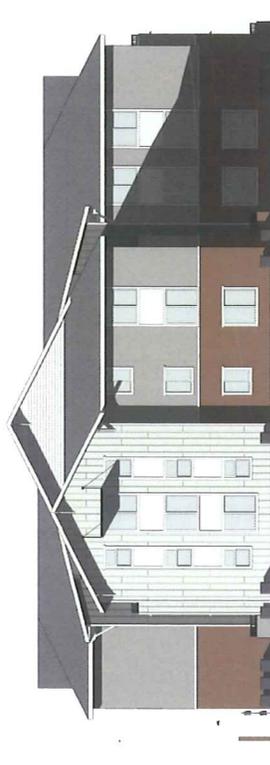
TRACY, CA



FRONT ELEVATION
1 of 4



LEFT ELEVATION
3 of 4



REAR ELEVATION
4 of 4



RIGHT ELEVATION
2 of 4

MATERIAL LEGEND

- 1 COMPOSITE ASPHALT SHINGLE ROOF AT 1/2" PITCH (A-D)
- 2 CONCRETE BLOCK AND BRICK (B&D)
- 3 CONCRETE BLOCK (A, C, D) & LAY BRICK
- 4 INTERIOR PLASTER
- 5 CONCRETE BLOCK (A, C, D)
- 6 2X4 WOOD RAFTERS AND TRUSSES
- 7 BRACING SLAB METAL ROOF
- 8 CORRUGATED METAL ROOF
- 9 EXTERIOR POLYURETHANE INSULATION ON FLOORING
- 10 2X4 WOOD STUDS (WOOD SHOWN IN PLAN)
- 11 FINISHED FLOOR (WOOD)
- 12 FINISHED FLOOR (CONCRETE) (WOOD FINISH)
- 13 PAINT (WALLS)
- 14 FINISH LACE FRAME (WOOD) DOORS
- 15 PLASTER FINISH (WALLS) (WOOD FINISH) DOORS (WALLS)

RECEIVED

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CITY OF TRACY



BUILDING 2 - EXTERIOR ELEVATIONS

DATE: 8/15/14
PROJECT NO: 1183-001
SCALE:
SHEET: A2.3

**TRACY APARTMENTS
AT GRANT LINE ROAD**

TRACY, CA



BUILDING 2 - FIRST & SECOND FLOOR PLAN

DATE: 8/15/14
PROJECT NO: 1183-0001
SCALE:
SHEET: A2.1

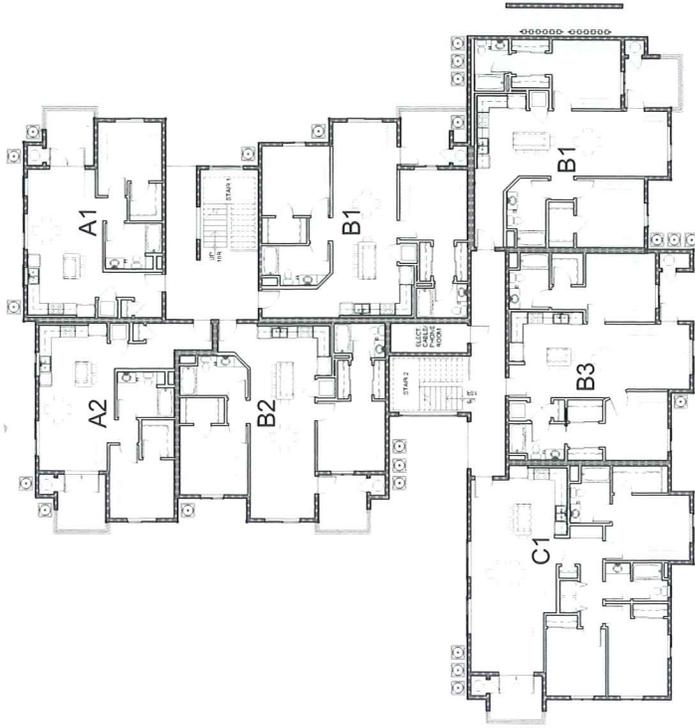
**TRACY APARTMENTS
AT GRANT LINE ROAD**

TRACY, CA

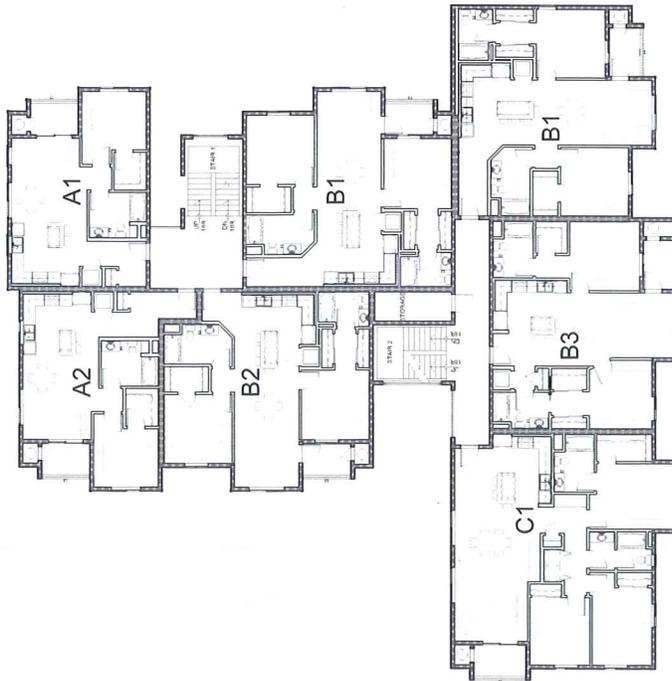
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SECOND FLOOR BUILDING PLAN | 2





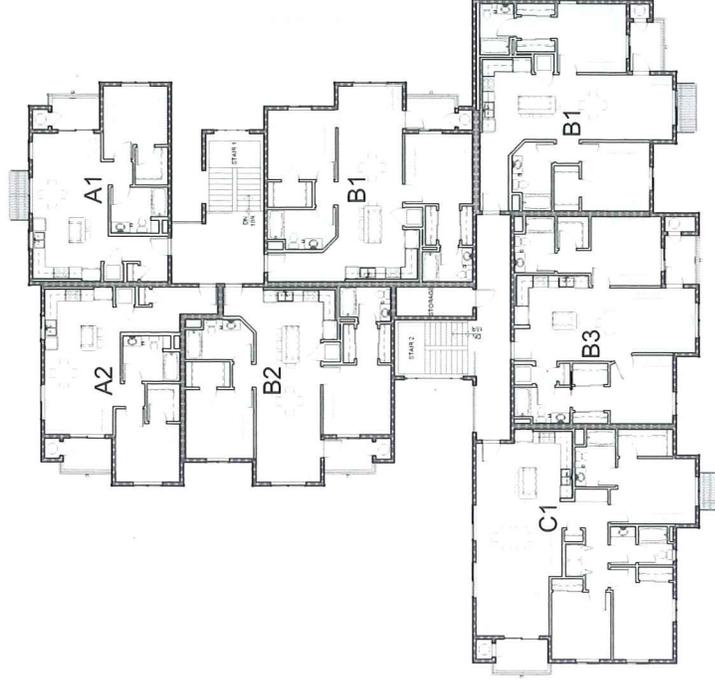
BUILDING 2 - THIRD FLOOR AND ROOF PLAN

DATE: 8/15/14
PROJECT NO: 1103-0001
SCALE:
SHEET: A2.2

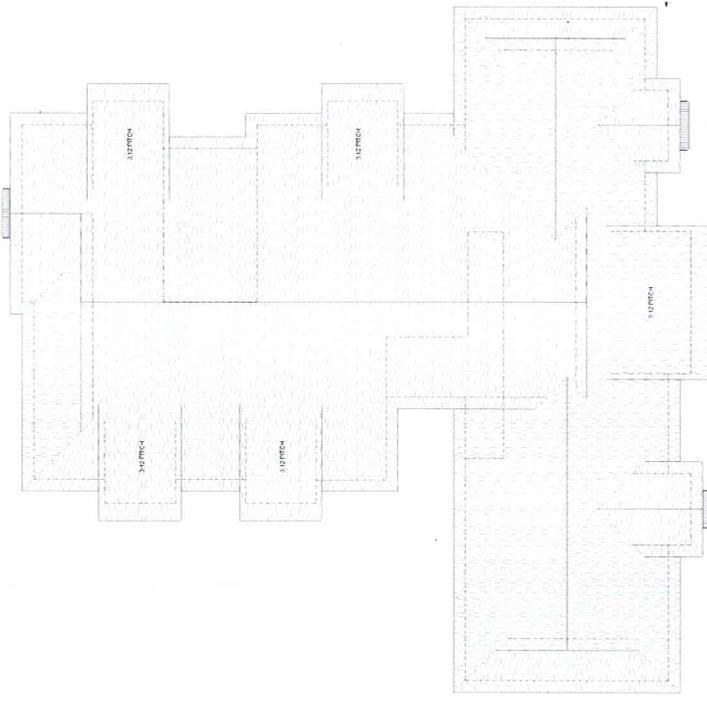
TRACY APARTMENTS AT GRANT LINE ROAD

TRACY, CA

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CITY OF TRACY, CA



THIRD FLOOR BUILDING PLAN 3



ROOF PLAN 4



TRACY APARTMENTS
 AT GRANT LINE ROAD

TRACY, CA

BUILDING 2 - PERSPECTIVES

DATE: 8/15/14
 PROJECT NO: 1183-0001
 SCALE:
 SHEET: A2.4



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BUILDING 2 - PERSPECTIVES

DATE: 8/15/14
 PROJECT NO: 1185-0001
 SCALE:
 SHEET: A2.5

TRACY APARTMENTS
 AT GRANT LINE ROAD

TRACY, CA



LEFT ELEVATION
1/8" = 1'-0"

5



RIGHT ELEVATION
1/8" = 1'-0"

6



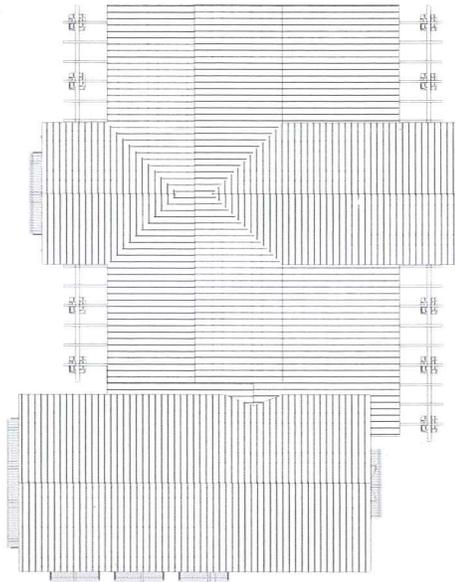
FRONT ELEVATION
1/8" = 1'-0"

3



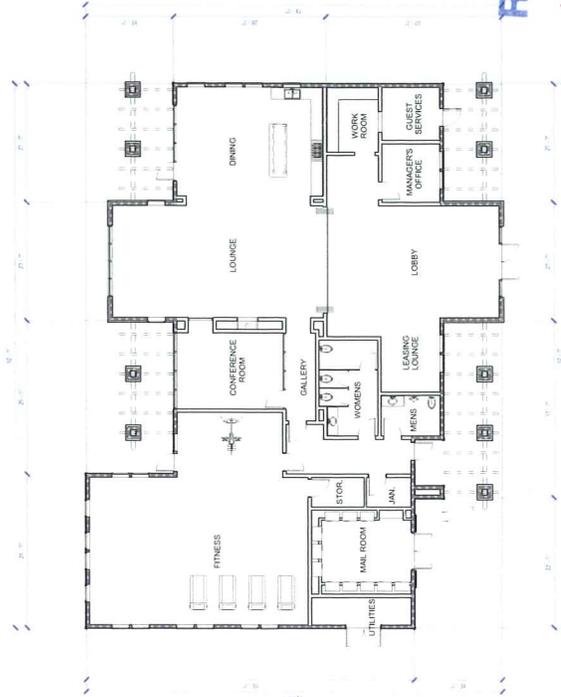
REAR ELEVATION
1/8" = 1'-0"

4



CLUBHOUSE ROOF PLAN
1/8" = 1'-0"

2



CLUBHOUSE FLOOR PLAN
1/8" = 1'-0"

1

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CITY OF TRACY

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408.798.8888

CLUBHOUSE - PLANS &
ELEVATIONS

DATE: 8/15/14
PROJECT NO: 1183-0001
SCALE:
SHEET: A3.1

TRACY APARTMENTS
AT GRANT LINE ROAD

TRACY, CA



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CLUBHOUSE - PERSPECTIVES

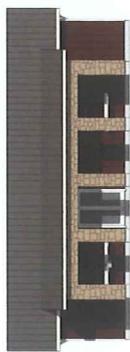
DATE: 8/15/14
PROJECT NO.: 116B-0001
SCALE:
SHEET: A3.2

TRACY APARTMENTS
AT GRANT LINE ROAD

TRACY, CA



RESTROOM FRONT ELEVATION
10'-0" x 10'-0"



RESTROOM REAR ELEVATION
10'-0" x 10'-0"



RESTROOM LEFT ELEVATION
10'-0" x 10'-0"



RESTROOM RIGHT ELEVATION
10'-0" x 10'-0"



FRONT ELEVATION
10'-0" x 10'-0"



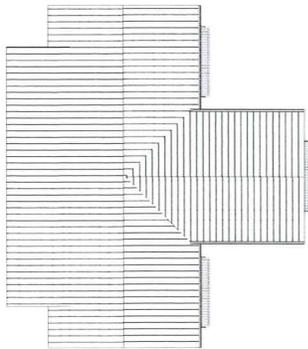
REAR ELEVATION
10'-0" x 10'-0"



LEFT ELEVATION
10'-0" x 10'-0"



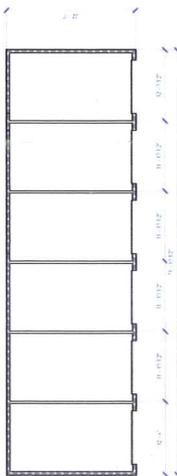
RIGHT ELEVATION
10'-0" x 10'-0"



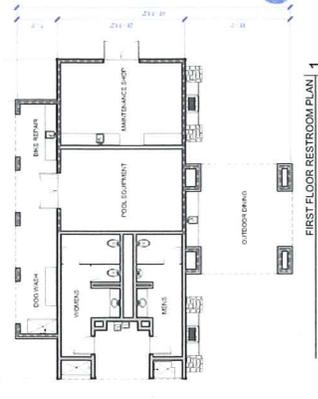
RESTROOM ROOF PLAN
10'-0" x 10'-0"



ROOF PLAN
10'-0" x 10'-0"



FIRST FLOOR GARAGE PLAN
10'-0" x 10'-0"



FIRST FLOOR RESTROOM PLAN
10'-0" x 10'-0"

RECEIVED

AUG 15 2014

CITY OF TRACY

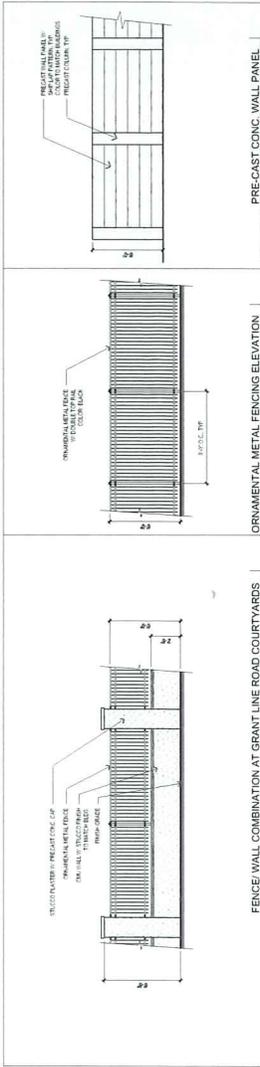
TRACY APARTMENTS
AT GRANT LINE ROAD

TRACY, CA

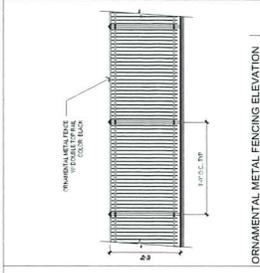
RESTROOM & GARAGE
BUILDING

DATE: 8/15/14
PROJECT NO: 1185-0001
SCALE:
SHEET: AA1

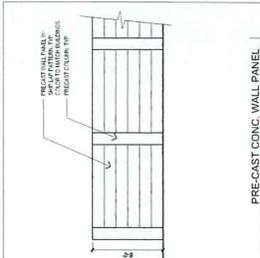




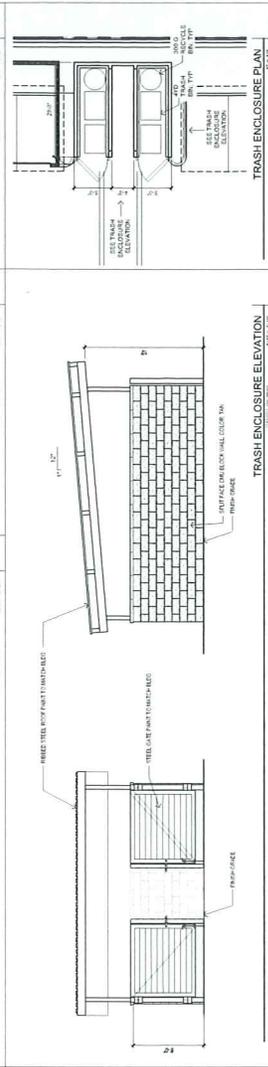
FENCE/WALL COMBINATION AT GRANT LINE ROAD COURTYARDS
1/4" = 1'-0"



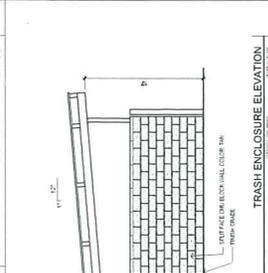
ORNAMENTAL METAL FENCING ELEVATION
1/4" = 1'-0"



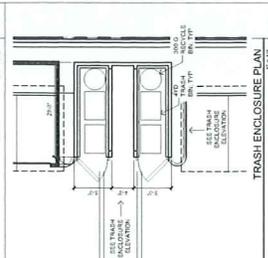
PRE-CAST CONCRETE WALL PANEL
1/4" = 1'-0"



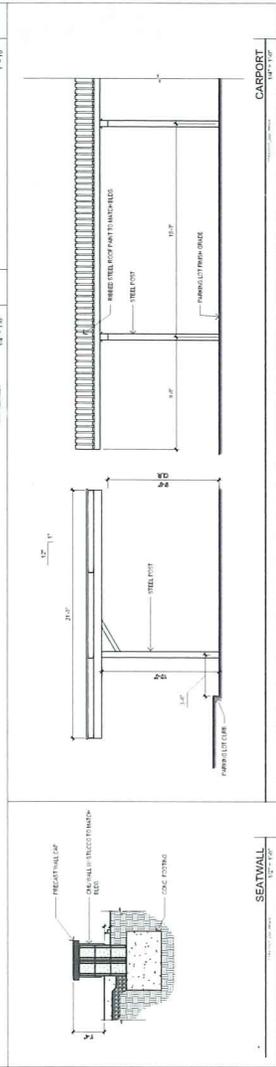
TRASH COMPACTOR ENCLOSURE ELEVATION
1/4" = 1'-0"



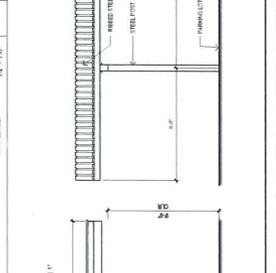
TRASH ENCLOSURE ELEVATION
1/4" = 1'-0"



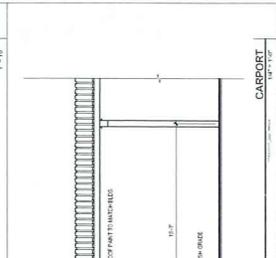
TRASH ENCLOSURE PLAN
1/4" = 1'-0"



TRASH ENCLOSURE ELEVATION
1/4" = 1'-0"



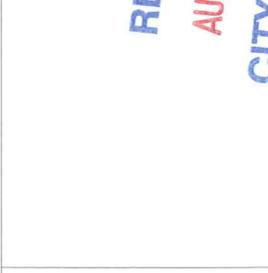
TRASH ENCLOSURE ELEVATION
1/4" = 1'-0"



TRASH ENCLOSURE PLAN
1/4" = 1'-0"



TRASH ENCLOSURE ELEVATION
1/4" = 1'-0"



TRASH ENCLOSURE ELEVATION
1/4" = 1'-0"



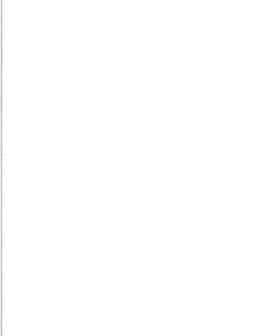
TRASH ENCLOSURE PLAN
1/4" = 1'-0"



BIKE RACK
1/4" = 1'-0"



SIGHT LIGHTING
1/4" = 1'-0"



MONUMENT SIGN
1/4" = 1'-0"



TRASH/RECYCLE RECEPTACLE
1/4" = 1'-0"



SEAT WALL
1/4" = 1'-0"

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CITY OF TRACY



SITE DETAILS

DATE: 08/15/2014
PROJECT NO: 1185-0001
SCALE:
SHEET: A1/11

TRACY APARTMENTS
GRANT LINE ROAD
TRACY, CALIFORNIA



RECEIVED
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 CITY OF TRACY

SCALE 1" = 50'-0"



COLOR SITE PLAN

DATE: 08.15.2014
 PROJECT NO: 1183-0001
 SCALE: 1" = 50'
 SHEET: L2.00

PLANT SCHEDULE

TREES	BOTANICAL NAME / COMMON NAME	QTY	DETAIL
	Cercis canadensis / Flowering Cherry	24" DBH	26
	Lagotis arvensis / Meadow / Caper Bush	15 gal	45
	Quercus laevis / Live Oak	15 gal	47
	Prunella americana / Candy Island Plum	15 gal	50
	Prunella virginiana / Whiteflower / Whiteflower Pear	15 gal	77
	Quercus laevis / Live Oak	24" DBH	75
	Quercus laevis / Live Oak	24" DBH	73
	Ulmus americana / Elm	15 gal	111
	Washingtonia robusta / Mexican Fan Palm	15 gal	6
	Zelkova serotina / Winter Green / Lombard Zelkova	15 gal	65

TRACY APARTMENTS
 GRANT LINE ROAD

TRACY, CALIFORNIA



the
garibaldi
company

August 14, 2014

City of Tracy Planning Department

Re: Tracy Grant Line Project

In order to plan effectively for the Tracy Grant Line project we completed a survey of parking ratios for all of the garden style apartment communities currently owned/managed by The Garibaldi Company. The properties included in the survey represent a good sampling of a variety of conditions as they differ based on the date of construction, unit types, location, and resident demographic.

The three main data figures that we were interested in determining are as follows:

1. Parking Ratio: The parking ratio is the total number of parking spaces (not including garages) compared to the total number of units at the property.
2. Parking Ratio w/Garage: The parking ratio with garages is the total number of parking spaces (including garages) compared to the total number of units at the property.
3. Parking w/o Garage by # of Bedrooms: The parking without garages by number of bedrooms is the total number of parking spaces (not including garages) compared to the number of bedrooms at the property.

As illustrated in the attached parking analysis, both phases of the Tracy Grant Line project generally conform to the company average in all three of the categories.

	Parking Ratio	Parking Ratio (w/Garage)	Parking w/o Garage by # of Bedrooms
Company Average	1.56	1.64	.95
Tracy Grant Line (East)	1.57	1.81	.94
Tracy Grant Line (West)	1.54	1.77	.95

Based on our findings we are confident that Tracy Grant Line will have ample parking. The parking ratios reflected for the other properties within our survey are sufficient for the various resident demographics reflected.

management • **apartment specialists** • consultants

The Garibaldi Company Parking Analysis

Property	City	# of Units	# of Bedrooms	Covered	Uncovered	Total Spaces	Parking Ratio	Garages	Total Spaces	Parking Ratio w/Garage	Parking w/o Garage by # of Bedrooms
Avalon Place	Manteca	72	144	76	1	77	1.07	30	107	1.49	0.53
Brookside	Stockton	218	392	128	210	338	1.55	90	428	1.96	0.86
Carriage House Cottages	Fremont	123	181	87	92	179	1.46	0	179	1.46	0.99
Emerald Pointe	Orangevale	184	268	184	116	300	1.63	0	300	1.63	1.12
Lakeview Village	Rocklin	164	304	164	134	298	1.82	48	346	2.11	0.98
Laurel Oaks	Stockton	120	200	128	71	199	1.66	0	199	1.66	1.00
Lincoln Village West	Sacramento	520	936	520	289	809	1.56	0	809	1.56	0.86
Marina Village	Stockton	112	134	48	102	150	1.34	0	150	1.34	1.12
Paseo Villas	Stockton	528	760	538	261	799	1.51	0	799	1.51	1.05
Sand Creek	Manteca	293	526	297	286	583	1.99	48	631	2.15	1.11
The Fountians	Lodi	130	220	130	55	185	1.42	0	185	1.42	0.84
Venetian	Lodi	149	292	145	131	276	1.85	0	276	1.85	0.95
Villa Del Rio	Stockton	354	539	330	239	569	1.61	24	593	1.68	1.06
Vineyards	Sacramento	180	310	176	74	250	1.39	0	250	1.39	0.81
Woodmore Manor	Lodi	160	284	160	97	257	1.61	0	257	1.61	0.90
	Citrus Heights	110	163	110	55	165	1.50	0	165	1.50	1.01
Total/Average		3417					1.56			1.64	0.95

Tracy Grant Line (East)		231	387	232	131	363	1.57	54	417	1.81	0.94
Tracy Grant Line (West)		210	342	211	113	324	1.54	48	372	1.77	0.95



TRACY
UNIFIED SCHOOL DISTRICT

August 20, 2014

Mr. Alan Bell
City of Tracy
Development Services Department
Planning Division
333 Civic Center Plaza
Tracy, CA 95376
Email: Alan.Bell@ci.tracy.ca.us

RECEIVED
AUG 20 2014
CITY OF TRACY

RE: Tracy Apartments at Grantline Road
Application No.: SPA1A-0001 and D14-0011

*"The future belongs
to the educated"*

Dear Mr. Bell:

The proposed development referenced above will allow up to 470 residential apartment units to be built within the Tracy Unified School District boundaries. It is estimated that the proposed project will generate 158 new K – 8 students and 52 new 9 – 12 students. Tracy schools are operating at or above their design capacity and the District does not have the financial ability to adequately house the new students that will be generated by this project.

Dr. Brian R. Stephens
Superintendent
(209) 830-3201
(209) 830-3204 Fax

Dr. Casey J. Goodall
Associate Superintendent
of Business Services
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(209) 830-3259 Fax

Dr. Sheila Harrison
Assistant Superintendent
of Educational Services:
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(209) 830-3209 Fax
& Human Resources:
(209) 830-3260
(209) 830-3264 Fax

Bonny Carter
Director of Facilities and
Planning
(209) 830-3245
(209) 830-3249 Fax

The District hereby requests that a condition be placed on this project's Tentative Subdivision Map, pursuant to City Resolution 96-189 and the City's Growth Management Ordinance, imposing a condition that the applicant must show compliance with the District's Comprehensive School Facilities Capital Improvement and Finance Plan as further evidenced in the District's School Facilities Needs Analysis. Please also be advised that the District's Comprehensive School Facilities Capital Improvement and Finance Plan is currently being updated and should be completed within the next three months.

Entering into a Memorandum of Understanding (MOU) with the District will enable the developer to "evidence compliance" with the Plan. This requirement is important because it keeps all developments consistent with other developments within Tracy School District boundaries and does not economically disadvantage one development compared to another development. It also ensures sufficient funding to maintain high quality schools for Tracy's residents.

If you have any questions please give me a call at (209) 830-3245 or email at bcarter@tusd.net.

www.tracy.k12.ca.us
[Facebook.com/TracyUnifiedSD](https://www.facebook.com/TracyUnifiedSD)

Sincerely,

Bonny Carter
Director of Facilities & Planning

RESOLUTION 2014 - _____

RECOMMENDING APPROVAL OF AN AMENDMENT TO THE I-205 CORRIDOR SPECIFIC PLAN (SPA14-0001), APPROVAL OF A PLANNED UNIT DEVELOPMENT PRELIMINARY AND FINAL DEVELOPMENT PLAN (PUD14-0011), APPROVAL OF AN OFF-STREET PARKING REDUCTION, AND APPROVAL OF AN ADDENDUM TO THE FILIOS/DOBLER ANNEXATION AND DEVELOPMENT PROJECT ENVIRONMENTAL IMPACT REPORT FOR THE TRACY APARTMENTS AT GRANT LINE ROAD

WHEREAS, Applications have been filed or initiated to amend the I-205 Corridor Specific Plan designation from General Commercial to High Density Residential (HDR); approve a Planned Unit Development Preliminary and Final Development Plan for a 441-unit residential apartment complex; and to approve an off-street parking reduction to allow 1.8 parking spaces per dwelling unit, collectively, the "Project," and

WHEREAS, The approximately 22.22-acre subject property proposed for Specific Plan amendment is located on the south side of Grant Line Road, north of the Union Pacific Railroad tracks, east of Byron Road and approximately 600 feet west of Lammers Road, (Assessor's Parcel Numbers 209-270-10, 11, 30, and 31), and

WHEREAS, The PUD Preliminary and Final Development Plan is proposed for approximately 20.04 acres on the east side of the site, Assessor's Parcel Numbers 209-270-30 and 31, and

WHEREAS, The I-205 Corridor Specific Plan Amendment includes two adjacent parcels (known as the Maibes property) to the proposed Tracy Apartments at Grant Line Road site. The Maibes property totals 2.18 acres and extends immediately west of the Tracy Apartments properties to the intersection of Grant Line Road and Byron Road. While no development is proposed at this time on this 2.18 acres, the Maibes property is included in the I-205 Corridor Specific Plan amendment action, changing the General Commercial designation to High Density Residential, in order to promote orderly circulation and growth of the City and land use compatibility, and

WHEREAS, In addition to retail, office, and other commercial land uses, the General Plan's Commercial land use designation for the Project site provides for residential development in the density range permitted in the Residential High designation (12.1 to 25 dwelling units per gross acre), and

WHEREAS, The Project includes a request to amend the I-205 Corridor Specific Plan designation of the site to High Density Residential in order support the proposed residential land use of this Project, and

WHEREAS, The subject property is well suited for high density residential development because of its close proximity to a grocery store, retail and consumer services (along Grant Line Road, to the east of the Project site), employment opportunities in the area, availability of public services to the Project site, and convenient access to I-205, and

WHEREAS, The Project promotes a compact development pattern, minimizes consumption of open space lands and resources, and provides for high-density housing opportunities which assist the City in achieving housing goals established in the City's General Plan Housing Element, and

WHEREAS, The Project is consistent with General Plan Housing Element Goals and Policies, including Policy 3.1 (“Provide for a range of residential densities and products, including ... higher-density apartments”), and

WHEREAS, The Filios/Dobler Annexation and Development Project EIR (SCH#2010072043) was certified on November 11, 2011 by the Tracy City Council, and resulted in annexation of approximately 43 acres of unincorporated land to the City, amended the City General Plan land use designation of the site from Urban Reserve 2 (UR-2) to Commercial, and amended the I-205 Corridor Specific Plan to add the site to the Specific Plan area and designate the annexed lands General Commercial (GC), and

WHEREAS, The Project is a revision to the Filios/Dobler Annexation and Development Project EIR and therefore, an Addendum to the Filios/Dobler EIR has been prepared in accordance with California Environmental Quality Act (CEQA) Guidelines Section 15164 and is proposed for approval, and

WHEREAS, The Planning Commission conducted a public hearing to receive public input and review the Project on August 27, 2014;

NOW, THEREFORE, BE IT RESOLVED, by the Planning Commission as follows:

1. Addendum to the Filios/Dobler Annexation and Development EIR

a. The Filios Dobler EIR, which addressed environmental impacts from development of the Project site, was certified by the City Council on November 11, 2011.

b. In accordance with CEQA Guidelines Section 15164, a lead agency shall prepare an Addendum to a previously certified EIR if some changes or additions are necessary but none of the conditions described in Guidelines Section 15162 calling for the preparation of a Subsequent EIR (summarized in the attached Addendum, Exhibit 1) have occurred.

c. The Filios/Dobler EIR Addendum (Exhibit 1) concludes that no new significant impacts would result, and no substantial increase in the severity of impacts from those previously identified in the EIR would occur.

d. The Planning Commission recommends that the City Council approve the Filios/Dobler EIR Addendum, Exhibit 1.

2. I-205 Corridor Specific Plan Amendment (Application Number SPA14-0001)

a. The Specific Plan amendment is consistent with the General Plan, because the permitted uses of the Specific Plan’s High Density Residential designation are allowed within the General Plan designation of Residential High and are consistent with goals and policies of the Housing Element. Also, the Specific Plan’s Design Standards and Guidelines, particularly related to siting requirements (site planning, architecture, and parking) and design standards (transitions/edges, commercial interface, auxiliary site features, fencing and walls) are consistent with Land Use and Community Character Element goals and policies.

b. The General Plan identifies apartments, specifically, among the “characteristic housing” for property designated Residential High, in the density range from 12.1 to 25 units per gross acre. The Specific Plan is consistent with the General Plan in that it, too, identifies apartments (and other similar uses as the General Plan) as permitted uses in the High Density Residential Designation.

c. The project site is subject to development impact fees and other infrastructure mitigation requirements to mitigate the Project's fair share of infrastructure costs required by the City's infrastructure master plans.

d. The Planning Commission recommends that the City Council approve the Specific Plan amendment to change the designation of the approximately 22.22-acre site (including APNs 209-270-10, 11, 30, and 31) from General Commercial to High Density Residential.

3. Planned Unit Development Preliminary and Final Development Plan (Application Number PUD14-0011)

a. The Project includes site plan and design elements consistent with City design goals and standards, such as placement of buildings close to the public street to create an inviting, pedestrian-oriented environment; and compliance with all City standards, including number and design of parking spaces, circulation, land use, and landscaping.

b. The Project is consistent with Specific Plan architecture Design Standards and Guidelines. The architecture incorporates elements such as variation in texture, materials, and the building facades include relief to avoid monotonous appearance. Building elevations facing streets have windows and other architectural features. Stairwells are covered and integrated into overall building design and private spaces (patios and balconies) are included for each unit.

c. The Planning Commission recommends that the City Council approve the Project Planned Unit Development Preliminary and Final Development Plan of the approximately 20.04-acre site (including APNs 209-270-30 and 31), subject to conditions contained in Exhibit 2.

4. Off-Street Parking Reduction

a. The property owner submitted a parking study documenting that 89 of the project's 885 off-street parking spaces will not be necessary to mitigate the potential parking demands of the project.

b. The parking study contains a survey of 16 existing multi-family projects in the Central/Northern California area containing an average parking ration of 1.56 spaces per dwelling unit.

c. The Project is constructing of a City bus shelter along Grant Line Road (in front of the Project), and will be within walking distance to shopping along Grant Line Road (shopping centers to the east), and no other off-street parking space reductions are approved or anticipated within the vicinity of this project

d. The Planning Commission recommends that the City Council approve an Off-Street Parking Space Reduction of 1.8 parking spaces per dwelling unit for this project.

* * * * *

The foregoing Resolution 2014-_____was adopted by the Planning Commission on the 27th day of August, 2014, by the following vote:

AYES: COMMISSION MEMBERS:

NOES: COMMISSION MEMBERS:

ABSENT: COMMISSION MEMBERS:

ABSTAIN: COMMISSION MEMBERS:

CHAIR

ATTEST:

STAFF LIAISON

Exhibit 1 – Addendum to the Filios/Dobler Annexation and
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Exhibit 2 – Project Conditions of Approval



TRACY
UNIFIED SCHOOL DISTRICT

August 20, 2014

Mr. Alan Bell
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Development Services Department
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RECEIVED
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* * * * *

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AYES: COMMISSION MEMBERS:

NOES: COMMISSION MEMBERS:

ABSENT: COMMISSION MEMBERS:

ABSTAIN: COMMISSION MEMBERS:

CHAIR

ATTEST:

STAFF LIAISON

Exhibit 1 – Addendum to the Filios/Dobler Annexation and
Development Project

Exhibit 2 – Project Conditions of Approval

ADDENDUM TO THE FILIOS/DOBLER
ANNEXATION AND DEVELOPMENT PROJECT EIR

FOR THE

TRACY APARTMENTS AT GRANT LINE ROAD PROJECT

STATE CLEARINGHOUSE # 2010072043

JULY 2014

Prepared for:

City of Tracy
Department of Development Services
333 Civic Center Plaza
Tracy, CA 95676

Prepared by:

De Novo Planning Group
1020 Suncast Lane, Suite 106
El Dorado Hills, CA
(916) 949-3231

D e N o v o P l a n n i n g G r o u p

A Land Use Planning, Design, and Environmental Firm

ADDENDUM TO THE FILIOS/DOBLER ANNEXATION AND
DEVELOPMENT PROJECT EIR

FOR THE

TRACY APARTMENTS AT GRANT LINE ROAD PROJECT

State Clearinghouse # 2010072043

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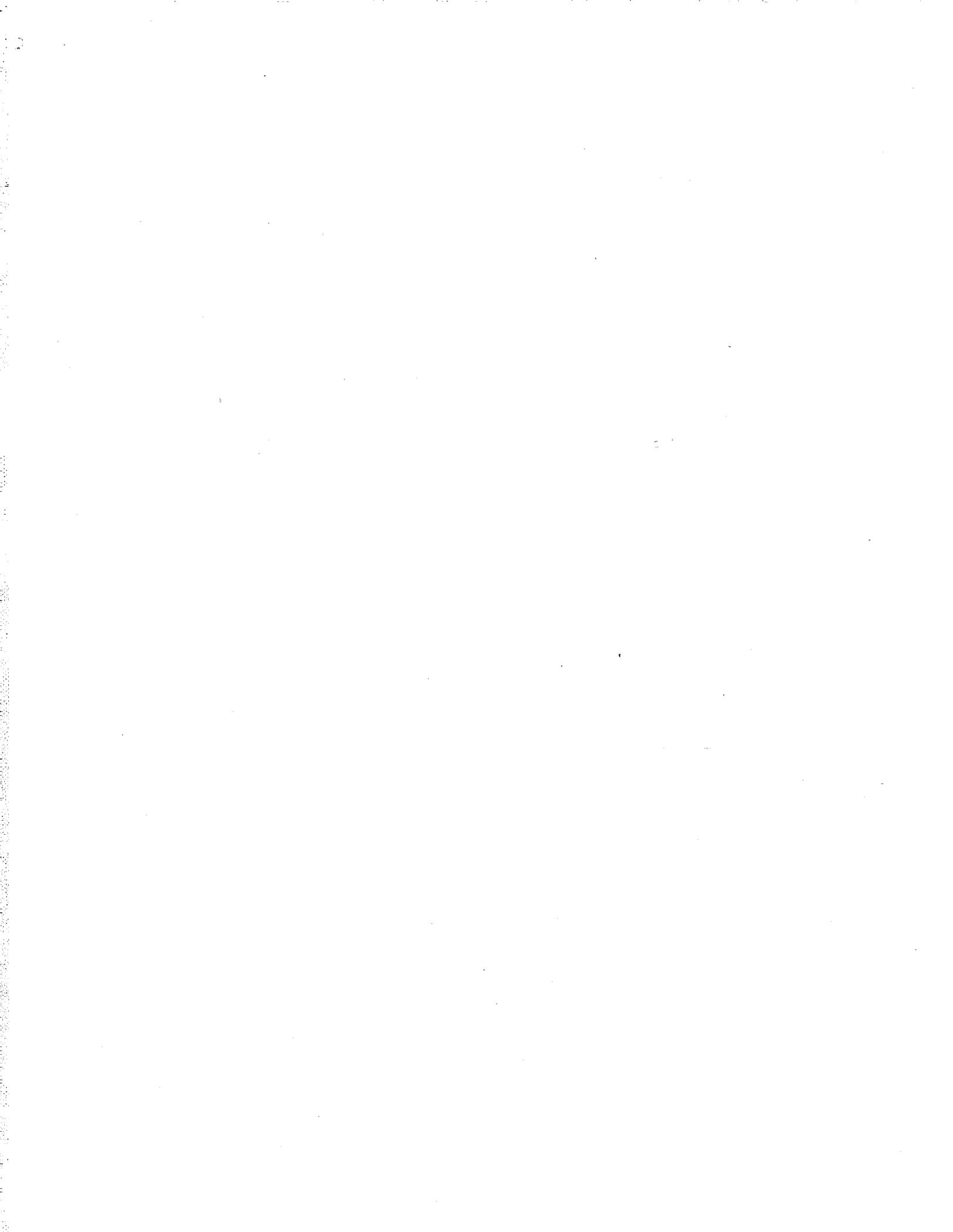


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1.0 INTRODUCTION

This Addendum was prepared in accordance with the California Environmental Quality Act (CEQA) and the CEQA Guidelines. This document has been prepared to serve as an Addendum to the previously certified EIR (State Clearinghouse Number 2010072043) for the Filios/Dobler Annexation and Development Project (Original Project). The City of Tracy is the lead agency for the environmental review of the proposed Project modifications (Modified Project).

This Addendum addresses the proposed modifications in relation to the previous environmental review prepared for the Filios/Dobler Annexation and Development Project. CEQA Guidelines Section 15164 defines an Addendum as:

The lead agency or responsible agency shall prepare an addendum to a previously certified EIR if some changes or additions are necessary but none of the conditions described in Section 15162 calling for preparation of a subsequent EIR have occurred.

....A brief explanation of the decision not to prepare a subsequent EIR pursuant to Section 15162 should be included in an addendum to an EIR, the lead agency's findings on the project, or elsewhere in the record.

Information and technical analyses from the Filios/Dobler Annexation and Development Project EIR are utilized throughout this Addendum. Relevant passages from this document (consisting of the Filios/Dobler Annexation and Development Project EIR) are cited and available for review at:

City of Tracy
Development Services Department
333 Civic Center Plaza, Tracy, CA 95376
Alan Bell, AICP, Senior Planner
<http://www.ci.tracy.ca.us/?navId=595>

1.1 BACKGROUND AND PURPOSE OF THE EIR ADDENDUM

The Filios/Dobler Annexation and Development Project EIR (SCH #2010072043) was certified on November 11, 2011 by the Tracy City Council. The Filios/Dobler Annexation and Development Project annexed approximately 43 acres of unincorporated land to the City; amended the City General Plan land use designation of the site from Urban Reserve 2 (UR 2) to Commercial; amended the I-205 Corridor Specific Plan to add the site to the Specific Plan area, and designated it General Commercial (GC); and Prezoned the site Planned Unit Development (PUD). In addition, the Filios/Dobler Annexation and Development Project EIR included analysis of up to 466,000 square feet of commercial/office uses to be built on the site. The Filios/Dobler Annexation and Development Project is referred to as the "Original Project" in this EIR Addendum.

Since certification of the EIR, annexation of the site into the City of Tracy, and approval of the General Plan and Specific Plan amendments identified above, a detailed site plan and development proposal for the construction of 441 multi-family housing units has been submitted to the City of Tracy. This development project is known as the Tracy Apartments at Grant Line Road Project (Tracy Apartments Project, Modified Project, or Project). The 441 proposed multi-family housing units within the Project would be located on approximately 20 acres of the approximately 43-acre site addressed in the Filios/Dobler Annexation and Development Project EIR. The Project includes two parcels: APNs 209-270-30 and 209-270-31. Approval of the proposed Project, which is

**EIR ADDENDUM – TRACY APARTMENTS AT GRANT LINE ROAD PROJECT, FILIOS/DOBLER
ANNEXATION EIR**

described in greater detail under Section 2.0 below, would include an amendment to the I-205 Corridor Specific Plan from General Commercial to High Density Residential, and approve the development plan for the Project. A General Plan Amendment will not be included because high density residential development (12.1 to 25 units per acre) is allowed in the Commercial General Plan designation.

In addition to the two parcels encompassed by the Tracy Apartments Project site, two additional parcels (known as the Maibes property) totaling 2.18 acres (APNs 209-270-10 and 209-270-11), extending west of the Tracy Apartments properties to the intersection of Grant Line Road and Byron Road, are included in the project description for this EIR Addendum. While no development is proposed at this time on this additional 2.18 acres, the City anticipates including the Maibes property in the I-205 Corridor Specific Plan amendment action, changing the General Commercial designation to High Density Residential. For the purposes of this EIR Addendum, the Maibes property is assumed for ultimate development of multi-family residential housing at a density comparable to the Tracy Apartments Project.

In determining whether an Addendum is the appropriate document to analyze the proposed modifications to the project and its approval, CEQA Guidelines Section 15164 (Addendum to an EIR or Negative Declaration) states:

- a) *The lead agency or a responsible agency shall prepare an addendum to a previously certified EIR if some changes or additions are necessary but none of the conditions described in Section 15162 calling for preparation of a subsequent EIR have occurred.*
- b) *An addendum to an adopted negative declaration may be prepared if only minor technical changes or additions are necessary or none of the conditions described in Section 15162 calling for the preparation of a subsequent EIR or negative declaration have occurred.*
- c) *An addendum need not be circulated for public review but can be included in or attached to the final EIR or adopted negative declaration.*
- d) *The decision-making body shall consider the addendum with the final EIR or adopted negative declaration prior to making a decision on the project.*
- e) *A brief explanation of the decision not to prepare a subsequent EIR pursuant to Section 15162 should be included in an addendum to an EIR, the lead agency's required findings on the project, or elsewhere in the record. The explanation must be supported by substantial evidence.*

1.2 BASIS FOR DECISION TO PREPARE AN ADDENDUM

When an environmental impact report has been certified for a project, Public Resources Code Section 21166 and CEQA Guidelines Sections 15162 and 15164 set forth the criteria for determining whether a subsequent EIR, subsequent negative declaration, addendum, or no further documentation be prepared in support of further agency action on the project. Under these Guidelines, a subsequent EIR or negative declaration shall be prepared if any of the following criteria are met:

**EIR ADDENDUM – TRACY APARTMENTS AT GRANT LINE ROAD PROJECT, FILIOS/DOBLER
ANNEXATION EIR**

- (a) *When an EIR has been certified or negative declaration adopted for a project, no subsequent EIR shall be prepared for that project unless the lead agency determines, on the basis of substantial evidence in the light of the whole record, one or more of the following:*
- (1) *Substantial changes are proposed in the project which will require major revisions of the previous EIR or negative declaration due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects;*
 - (2) *Substantial changes occur with respect to the circumstances under which the project is undertaken which will require major revisions of the previous EIR or negative declaration due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects; or*
 - (3) *New information of substantial importance, which was not known and could not have been known with the exercise of reasonable diligence at the time the previous EIR was certified as complete or the negative declaration was adopted, shows any of the following:*
 - (A) *The project will have one or more significant effects not discussed in the previous EIR or negative declaration;*
 - (B) *Significant effects previously examined will be substantially more severe than shown in the previous EIR;*
 - (C) *Mitigation measures or alternatives previously found not to be feasible would in fact be feasible and would substantially reduce one or more significant effects of the project, but the project proponents decline to adopt the mitigation measure or alternative; or*
 - (D) *Mitigation measures or alternatives which are considerably different from those analyzed in the previous EIR would substantially reduce one or more significant effects on the environment, but the project proponents decline to adopt the mitigation measure or alternative.*
- (b) *If changes to a project or its circumstances occur or new information becomes available after adoption of a negative declaration, the lead agency shall prepare a subsequent EIR if required under subdivision (a). Otherwise the lead agency shall determine whether to prepare a subsequent negative declaration, and addendum, or no further documentation.*

As demonstrated in the environmental analysis provided in Section 3.0 (Environmental Analysis), the proposed changes do not meet the criteria for preparing a subsequent EIR or negative declaration. An addendum is appropriate here because, as explained in Section 3.0, none of the conditions calling for preparation of a subsequent EIR or negative declaration have occurred.

2.0 PROJECT DESCRIPTION

This section provides a detailed description of the proposed project. The reader is referred to Section 3.0 (Environmental Analysis) for the analysis of environmental effects of the proposed modifications in relation to the analysis contained in the previously certified Filios/Dobler Annexation and Development Project EIR (State Clearinghouse #2010072043).

2.1 PROJECT LOCATION

The project site is located in the northwest corner of the City of Tracy. The site is located immediately south of Grant Line Road, east of the intersection of Grant Line Road and Byron Road, north of the Union Pacific Rail Road (UPRR) tracks, and west of the Tracy Marketplace Shopping Center. The project's regional location is shown on Figure 1. The project site is shown on Figure 2.

2.2 SURROUNDING LAND USES

The project site is primarily surrounded by agricultural land and commercial uses, with some rural residential development sparsely scattered throughout the surrounding agricultural area. Agricultural land is located north of the project site, across Grant Line Road, and southwest of the site, across Byron Road. Vacant land lies between Byron Road and the agricultural land further to the southwest. Three residences are located northeast of Grant Line Road, at the intersection of Grant Line Road and Lammers Road, one residence is located southwest of Byron Road (northwest of the intersection of Byron Road and Von Sosten Road) and another residence is located west of the project site, west of the intersection of Grant Line Road and Byron Road. Commercial uses that are part of the Tracy Marketplace Shopping Center are located to the east. These uses include retail stores, such as WalMart and Costco, and restaurants, such as the Golden Corral Buffet and Grill. The Tracy Pavilion Shopping Center, another retail center, is located north of the Tracy Marketplace Shopping Center. The Tracy Pavilion Shopping Center contains several retail stores, including Home Depot and Marshalls. An auto mall is located north of the Tracy Pavilion Shopping Center and the West Valley Mall, a regional shopping center, is located just east of the auto mall. Other surrounding uses include Interstate 205 (I-205), which is generally south of the project site and the previously noted UPRR line that forms the southwestern site border.

2.3 SITE CHARACTERISTICS

The majority of the project site consists of predominantly flat land that until recently was in active agricultural production for hay. There are three single-family residences and their associated accessory structures, as well as a welding shop located along the Grant Line Road frontage. Various ornamental landscaping surrounds the residences and the welding shop. A Pacific Gas and Electric (PG&E) easement containing two power transmission lines and an underground natural gas pipeline are located in the southeastern tip of the project site. Additionally, a 60' wide Sierra & SF Power Co. easement is located on the property along the Grant Line Road frontage. The Hansen Sewer easement, containing a 30-inch diameter vylon close profile sewer pipe, is located along the PG&E easement in the southeastern tip of the project site.

2.4 PROJECT CHARACTERISTICS AND DESCRIPTION

The proposed project would modify the approved Filios/Dobler Annexation and Development Project to allow for high-density residential uses on approximately 22.18 acres, roughly the western half within the 43-acre Filios/Dobler site, as shown on Figure 2.

As shown on Figure 3, a detailed site plan has been prepared and submitted for approval on approximately 20 acres of the 22.18-acre project site. A total of 441 multi-family housing units are proposed within the Project site, and an estimated additional 48 multi-family units would be located immediately west of the Tracy Apartments Project site, on the two parcels totaling approximately 2.18 acres, known as the Maibes Property (see Figure 2). The proposed project analyzed in this EIR Addendum therefore includes up to 489 total multi-family housing units.

TRACY APARTMENTS AT GRANT LINE ROAD PROJECT (MODIFIED PROJECT)

The proposed 441 unit apartment project occupies 2 parcels containing approximately 20.1 acres along Grant Line Road in the easternmost triangle of the I-205 Specific Plan in Tracy, CA. The project would contain a mix of one, two and three-bedroom units, with adaptable ground floor units to allow for accessibility. The overall density is approximately 22.4 units per acre, and the maximum building height is three stories. The project would provide approximately 806 parking spaces with a combination of surface parking, carports, and garages distributed throughout the site, providing a minimum of one covered space per unit.

Other project features would include common areas, totaling approximately 86,500 square feet, two 5,000 square foot clubhouse buildings, two swimming pools and associated restrooms and pavilion areas, two maintenance buildings totaling 3,150 square feet, and miscellaneous landscaping and open space areas. The project is proposed to be built in two phases, with 210 units built in Phase I on the east half of the site, and 231 units built in Phase II on the west half of the site.

A combination of six-foot tall masonry block walls and six-foot tall decorative metal fences are proposed along the western, southwestern and eastern edges of the project site. Garages are proposed along the interior southwestern edge of the project site.

Access to the site would be provided via four driveways along the northern boundary of the site, providing access to and from Grant Line Road.

The Project would be served by 8-inch diameter on-site water mains, connected to: (1) an extension of the existing 16-inch diameter water main located on Grant Line Road, and (2) an extension of the existing 12-inch diameter water main located on South Lammers Road. Extensions of these pipelines in Grant Line Road and onto Byron Road to the west were identified in the 2012 Citywide Water System Master Plan (Master Plan), and their associated costs were included in the City's adopted development impact fees. It should be noted that both of these water mains would need to be further extended to the west as other projects located west of this Project develop. The proposed connections to these two existing water mains provides for a looped connection of the Project to the City's Pressure Zone 1 water distribution system.

The Project would initially connect to the Hansen sewer line to provide connectivity to the City's Wastewater Treatment Plant. Connection to the Hansen sewer line would be an interim connection point. The permanent wastewater connection point for the Project would be from the Project site

**EIR ADDENDUM – TRACY APARTMENTS AT GRANT LINE ROAD PROJECT, FILIOS/DOBLER
ANNEXATION EIR**

to the future sewer line in the Lammers Road Collection System, as shown in the Tracy Wastewater Master Plan.

MAIBES PROPERTY

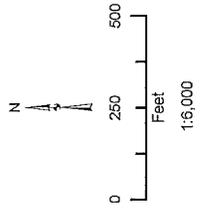
This portion of the project site is assumed to include up to 48 multi-family residential units, with a maximum building height of three stories. There is no development plan submittal at this time for the Maibes property. Analysis of the Maibes property is included in this EIR Addendum at the direction of the City to ensure comprehensive planning and land use considerations. As such, a Specific Plan change from General Commercial to High Density Residential is being considered under this EIR Addendum, but any future development would be subject to submittal of a separate development application to develop the Maibes property. The proposed Tracy Apartments Project would be required to extend water, wastewater and storm drainage improvements to the adjacent Maibes property. These infrastructure lines would be stubbed at the western end of the Tracy Apartments Project site, and appropriately sized to provide for future development of the Maibes property.

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**GRANT LINE APARTMENTS PROJECT
FILIOS/DOBLER ANNEXATION EIR ADDENDUM**

Figure 2: Project Site



- Legend**
- Project Site
 - Filios/Dobler Annexation EIR Project Area
 - Parcels within Project Area

De Novo Planning Group
A Land Use Planning, Design, and Environmental Firm

Data sources: San Joaquin County GIS; ESRI StreetMap; North America; ArcGIS Online Imagery. Map date: June 9, 2014

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LPAS

Tracy, Grant Line Road Apartments
 Date: 08.16.2014
 Project No: 118C-0001
 Sheet: A1.00

Item	Description	Quantity	Unit	Value
1.00	Site Preparation	1.00	Acres	100,000.00
2.00	Site Grading	1.00	Acres	100,000.00
3.00	Site Erosion Control	1.00	Acres	100,000.00
4.00	Site Fencing	1.00	Acres	100,000.00
5.00	Site Security	1.00	Acres	100,000.00
6.00	Site Access	1.00	Acres	100,000.00
7.00	Site Utilities	1.00	Acres	100,000.00
8.00	Site Water	1.00	Acres	100,000.00
9.00	Site Sewer	1.00	Acres	100,000.00
10.00	Site Stormwater	1.00	Acres	100,000.00
11.00	Site Paving	1.00	Acres	100,000.00
12.00	Site Landscaping	1.00	Acres	100,000.00
13.00	Site Lighting	1.00	Acres	100,000.00
14.00	Site Signage	1.00	Acres	100,000.00
15.00	Site Security	1.00	Acres	100,000.00
16.00	Site Access	1.00	Acres	100,000.00
17.00	Site Utilities	1.00	Acres	100,000.00
18.00	Site Water	1.00	Acres	100,000.00
19.00	Site Sewer	1.00	Acres	100,000.00
20.00	Site Stormwater	1.00	Acres	100,000.00
21.00	Site Paving	1.00	Acres	100,000.00
22.00	Site Landscaping	1.00	Acres	100,000.00
23.00	Site Lighting	1.00	Acres	100,000.00
24.00	Site Signage	1.00	Acres	100,000.00
25.00	Site Security	1.00	Acres	100,000.00
26.00	Site Access	1.00	Acres	100,000.00
27.00	Site Utilities	1.00	Acres	100,000.00
28.00	Site Water	1.00	Acres	100,000.00
29.00	Site Sewer	1.00	Acres	100,000.00
30.00	Site Stormwater	1.00	Acres	100,000.00
31.00	Site Paving	1.00	Acres	100,000.00
32.00	Site Landscaping	1.00	Acres	100,000.00
33.00	Site Lighting	1.00	Acres	100,000.00
34.00	Site Signage	1.00	Acres	100,000.00
35.00	Site Security	1.00	Acres	100,000.00
36.00	Site Access	1.00	Acres	100,000.00
37.00	Site Utilities	1.00	Acres	100,000.00
38.00	Site Water	1.00	Acres	100,000.00
39.00	Site Sewer	1.00	Acres	100,000.00
40.00	Site Stormwater	1.00	Acres	100,000.00
41.00	Site Paving	1.00	Acres	100,000.00
42.00	Site Landscaping	1.00	Acres	100,000.00
43.00	Site Lighting	1.00	Acres	100,000.00
44.00	Site Signage	1.00	Acres	100,000.00
45.00	Site Security	1.00	Acres	100,000.00
46.00	Site Access	1.00	Acres	100,000.00
47.00	Site Utilities	1.00	Acres	100,000.00
48.00	Site Water	1.00	Acres	100,000.00
49.00	Site Sewer	1.00	Acres	100,000.00
50.00	Site Stormwater	1.00	Acres	100,000.00

Item	Description	Quantity	Unit	Value
1.00	Site Preparation	1.00	Acres	100,000.00
2.00	Site Grading	1.00	Acres	100,000.00
3.00	Site Erosion Control	1.00	Acres	100,000.00
4.00	Site Fencing	1.00	Acres	100,000.00
5.00	Site Security	1.00	Acres	100,000.00
6.00	Site Access	1.00	Acres	100,000.00
7.00	Site Utilities	1.00	Acres	100,000.00
8.00	Site Water	1.00	Acres	100,000.00
9.00	Site Sewer	1.00	Acres	100,000.00
10.00	Site Stormwater	1.00	Acres	100,000.00
11.00	Site Paving	1.00	Acres	100,000.00
12.00	Site Landscaping	1.00	Acres	100,000.00
13.00	Site Lighting	1.00	Acres	100,000.00
14.00	Site Signage	1.00	Acres	100,000.00
15.00	Site Security	1.00	Acres	100,000.00
16.00	Site Access	1.00	Acres	100,000.00
17.00	Site Utilities	1.00	Acres	100,000.00
18.00	Site Water	1.00	Acres	100,000.00
19.00	Site Sewer	1.00	Acres	100,000.00
20.00	Site Stormwater	1.00	Acres	100,000.00
21.00	Site Paving	1.00	Acres	100,000.00
22.00	Site Landscaping	1.00	Acres	100,000.00
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25.00	Site Security	1.00	Acres	100,000.00
26.00	Site Access	1.00	Acres	100,000.00
27.00	Site Utilities	1.00	Acres	100,000.00
28.00	Site Water	1.00	Acres	100,000.00
29.00	Site Sewer	1.00	Acres	100,000.00
30.00	Site Stormwater	1.00	Acres	100,000.00
31.00	Site Paving	1.00	Acres	100,000.00
32.00	Site Landscaping	1.00	Acres	100,000.00
33.00	Site Lighting	1.00	Acres	100,000.00
34.00	Site Signage	1.00	Acres	100,000.00
35.00	Site Security	1.00	Acres	100,000.00
36.00	Site Access	1.00	Acres	100,000.00
37.00	Site Utilities	1.00	Acres	100,000.00
38.00	Site Water	1.00	Acres	100,000.00
39.00	Site Sewer	1.00	Acres	100,000.00
40.00	Site Stormwater	1.00	Acres	100,000.00
41.00	Site Paving	1.00	Acres	100,000.00
42.00	Site Landscaping	1.00	Acres	100,000.00
43.00	Site Lighting	1.00	Acres	100,000.00
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48.00	Site Water	1.00	Acres	100,000.00
49.00	Site Sewer	1.00	Acres	100,000.00
50.00	Site Stormwater	1.00	Acres	100,000.00

PROJECT DATA

TRACY APARTMENTS
 GRANT LINE ROAD
 TRACY, CALIFORNIA

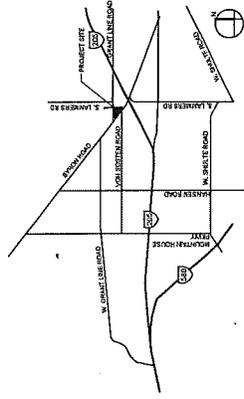
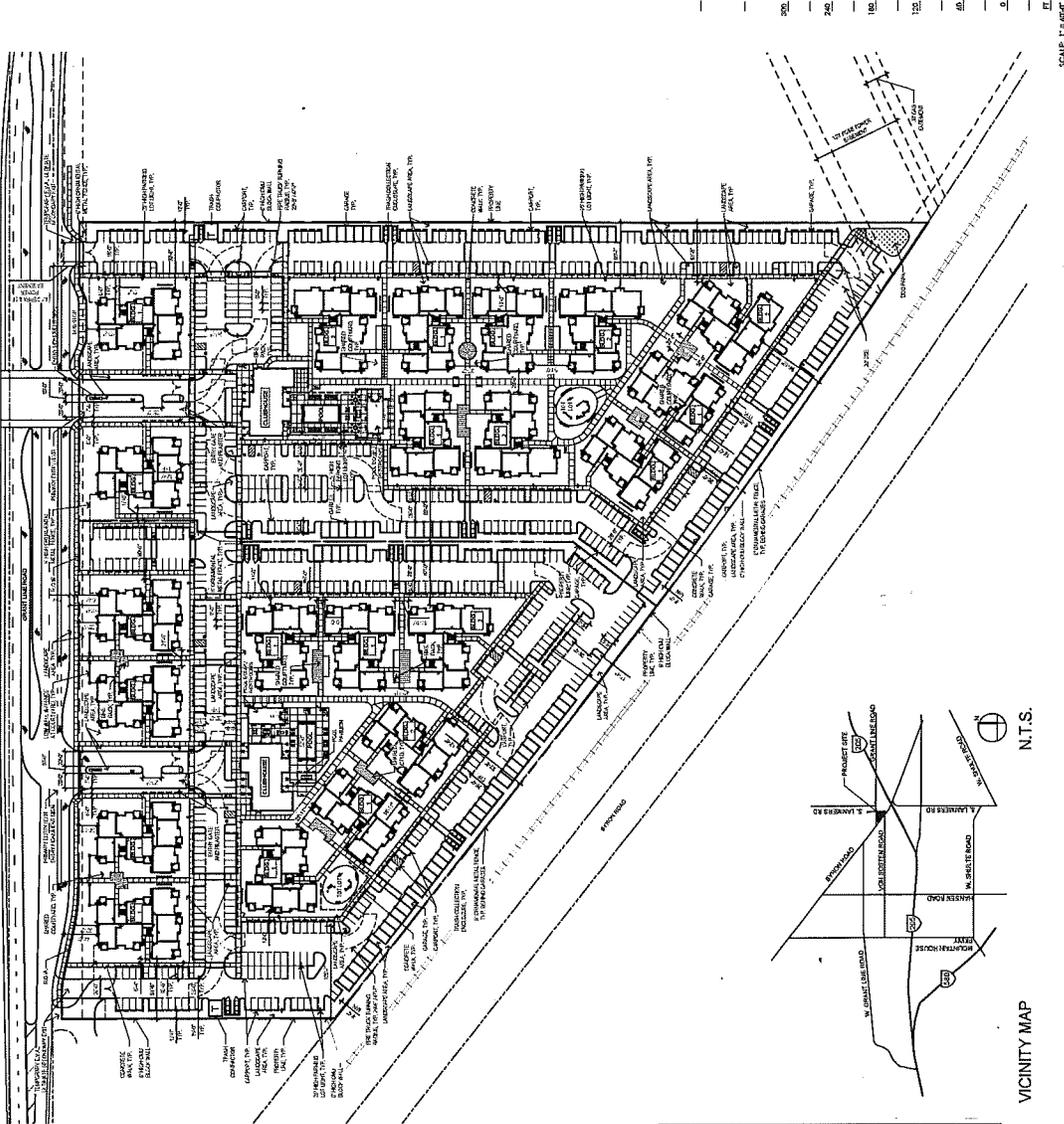


FIGURE 3: OVERALL SITE PLAN

DATE: 08.16.2014
 PROJECT NO: 118C-0001
 SCALE: AS SHOWN
 SHEET: A1.00



N.T.S.

VICINITY MAP

3.0 ENVIRONMENTAL ANALYSIS

This section of the Addendum provides analysis and cites substantial evidence that support's the City's determination that the proposed modifications to the Filios/Dobler Annexation and Development Project do not meet the criteria for preparing a subsequent or supplemental EIR under CEQA Guidelines Section 15162.

As addressed in the analysis below, the proposed modifications to the Filios/Dobler Annexation and Development Project are not substantial changes to the originally anticipated project. The proposed modifications to the Filios/Dobler Annexation and Development Project would not cause a new significant impact or substantially increase the severity of a previously identified significant impact from the Final EIR (CEQA Guidelines Section 15162[a][1]) that would require major revisions to the EIR. All impacts would be nearly equivalent to the impacts previously analyzed in the Final EIR. Relatedly, the proposed modifications to the Filios/Dobler Annexation and Development Project are not inconsistent with the General Plan, Zoning Ordinance, or adopted Mitigation Measures for this project.

The proposed changes do not cause a new significant impact or substantially increase the severity of a previously identified significant impact, and there have been no other changes in the circumstances that meet this criterion (CEQA Guidelines Section 15162[a][2]). There have been no changes in the environmental conditions on the property not contemplated and analyzed in the EIR that would result in new or substantially more severe environmental impacts.

There is no new information of substantial importance (which was not known or could not have been known at the time of the application, that identifies: a new significant impact (condition "A" under CEQA Guidelines Section 15162[a][3]); a substantial increase in the severity of a previously identified significant impact (condition "B" CEQA Guidelines Section 15162[a][3]); mitigation measures or alternatives previously found infeasible that would now be feasible and would substantially reduce one or more significant effects; or mitigation measures or alternatives which are considerably different from those analyzed in the EIR which would substantially reduce one or more significant effects on the environment (conditions "C" and "D" CEQA Guidelines Section 15162[a][3]). The reader is referred to City Resolution No. 2011-210 regarding findings on the feasibility of alternatives evaluated in the EIR. None of the "new information" conditions listed in the CEQA Guidelines Section 15162[a][3] are present here to trigger the need for a Subsequent or Supplemental EIR.

CEQA Guidelines Section 15164 states that "The lead agency or a responsible agency shall prepare an addendum to a previously certified EIR if some changes or additions are necessary but none of the conditions described in Section 15162 calling for preparation of a subsequent EIR have occurred." An addendum is appropriate here because, as explained above, none of the conditions calling for preparation of a subsequent EIR have occurred.

The following includes a detailed discussion of applicable impacts identified under the EIR in relation to the Filios/Dobler Annexation and Development Project. All impacts identified under the EIR have been determined to be less than significant, less than significant with mitigation, or significant and unavoidable. The City adopted CEQA Findings of Fact relative to each impact (City Resolution NO. 2011-210) at the time the EIR was certified for the Filios/Dobler Annexation and

**EIR ADDENDUM – TRACY APARTMENTS AT GRANT LINE ROAD PROJECT, FILIOS/DOBLER
ANNEXATION EIR**

Development Project. Additionally, the City adopted Statement of Overriding Considerations relative to each significant and unavoidable impact (City Resolution No 2011-210) at the time the EIR was certified for the Filios/Dobler Annexation and Development Project. Mitigation measures that were identified in the EIR for the purpose of lessening an impact to the extent feasible are embodied in a Mitigation Monitoring and Reporting Program that the City adopted at the time the EIR was certified (City Resolution No 2011-210).

The table below identifies the environmental topics addressed in the EIR, provides a summary of impacts associated with the Original Project, as described in the EIR, and includes an analysis of the potential impacts associated with the Modified Project when compared to the Original Project.

TABLE 1: COMPARISON OF APPROVED PROJECT IMPACTS AND MODIFIED PROJECT IMPACTS

Environmental Issues	Filios/Dobler Annexation Project Impacts	Tracy Apartments at Grant Line Road Project Impacts
<p><u>Aesthetics</u> a through d) <i>Would the project result in substantial adverse effects regarding a scenic vista, scenic resources, visual character, and light or glare?</i></p>	<p>The EIR identifies that Project construction would create temporary views of construction debris and construction-related activities, which may result in the degradation of character of the project area and affect the view of the site from nearby residences and passing motorists. This routine impact from typical, temporary construction activities would be short-term in duration and, therefore, result in a less than significant impact. With implementation of the standards and regulations required by the I-205 Corridor Specific Plan and the City's process for review of development applications, future development at the project site would be consistent with the existing commercial land uses in the area. Thus, impacts in this regard would be less than significant. In addition, the project would introduce new temporary construction-related and permanent sources of light and glare from street lights, building lights, security lights and parking lot lights. Mitigation identified in Section 4.3 (Aesthetics) would reduce this impact to less than significant.</p>	<p>The EIR found that the approved project would result in temporary Aesthetic impacts during the construction phase of the project. However, this impact is typical and short term in duration, thus results in a less than significant impact with implementation of mitigation measures required by the I-205 Corridor Specific Plan, and the City's review process.</p> <p>The proposed modifications to the Project are not substantial changes to the originally anticipated project relating to Aesthetics. The Modified Project does not designate any new sites for development that were not contemplated and analyzed for development in the EIR, and would not result in any changes to the location or footprint of development. Therefore, the Modified Project would not result in changes to development or development patterns that would further interfere with views of scenic resources, would not change views of or from scenic highways, and would not result in changes in development that would result in increased light or glare. The Modified Project would not result in any new potential aesthetic impacts and would not increase the significance of any aesthetic impacts. Mitigation Measures identified in Section 4.3 (Aesthetics) for the originally anticipated (approved) project would be sufficient in addressing the requirements for the Modified Project. Additionally, the Modified Project is subject to the City of Tracy's development and design review criteria, which would ensure that the exterior facades of the proposed residential structures, landscaping, streetscape improvements and exterior lighting improvements are compatible with the surrounding land uses.</p>
<p><u>Agricultural and Forest Resources</u> a through e) <i>Would the project convert Farmland to non-agricultural use, conflict with existing agricultural</i></p>	<p>The EIR identifies that the Project would result in the conversion of approximately 43 acres of land designated Prime Farmland to non-agricultural use, which would be a significant and unavoidable impact. In addition, the proposed Project would result in a significant and unavoidable cumulative</p>	<p>The EIR found that the approved project would result in significant impacts to agricultural lands and agricultural resources. Implementation of the mitigation measures identified in the EIR would not reduce this impact to a less than significant level, given that the loss of agricultural land is a permanent condition. However the potential for conversion</p>

TABLE 1: COMPARISON OF APPROVED PROJECT IMPACTS AND MODIFIED PROJECT IMPACTS

Environmental Issues	Filiros/Dobler Annexation Project Impacts	Tracy Apartments at Grant Line Road Project Impacts
<p><i>zoning or Williamson Act contracts, involve other changes that could convert farmland, or involve changes that could convert forest and timber resources to non-residential uses?</i></p>	<p>impact from the loss of agricultural land within San Joaquin County and the State. Proposed mitigation would not reduce impacts to less than significant. Future development of commercial and office uses on the Project site could conflict with adjacent agricultural land uses, leading to their indirect conversion to non-agricultural use. However, buffers created by Grant Line Road, the Union Pacific Railroad (UPRR) line and Byron Road would prevent and potential for the Project to indirectly convert adjacent agricultural land to non-agricultural use, resulting in a less than significant impact.</p>	<p>of adjacent agricultural lands to non-agricultural uses is less than significant, due to buffers created by Grant Line Road, the Union Pacific Railroad (UPRR) line and Byron Road.</p> <p>The proposed modifications to the Filiros/Dobler Annexation and Development Project are not substantial changes to the originally anticipated project relating to agricultural and forest resources. Resources of this type are site specific, and the Modified Project does not designate any new sites for development, and would not result in any changes to the location or footprint of development contemplated in the EIR. The Modified Project would not result in any new potential impacts to agricultural and forest resources beyond those addressed in the EIR, and would not increase the severity of any impacts related to agricultural and forest Resources.</p>
<p><i>Air Quality a through e) Would the project conflict with or obstruct implementation of the applicable air quality plan, violate or contribute to violation of an air quality standard, result in a cumulatively considerable net increase of a non-attainment criteria pollutant, expose sensitive receptors to substantial pollutant concentrations, or create objectionable odors affecting a substantial number of people?</i></p>	<p>The EIR identifies the Project would result in potentially significant short-term increases in particulate (fugitive dust) and exhaust emissions that could be reduced to less than significant with implementation of mitigation measures identified in Section 4.5 (Air Quality). During the operational stage, traffic associated with the proposed Commercial/office uses would increase air pollution concentrations and exceed established thresholds. Mitigation identified in Section 4.5 would reduce air pollutant concentrations caused by Project traffic, but not to a less than significant level, resulting in a significant and unavoidable impact. The Project would not exceed established thresholds for carbon monoxide (CO) hotspots, odor or toxic air contaminants and related impacts would be less than significant. However, because the Project would exceed established thresholds for criteria pollutants even after the implementation of design features and recommended mitigation measures, it would result in a significant and unavoidable conflict with the applicable air quality attainment plan. Finally, the</p>	<p>Construction Emissions</p> <p>The EIR found that the Original Project would result in potentially significant construction-related air quality impacts. Construction of the Modified Project would have comparable construction-related air quality impacts. The San Joaquin Valley Air Pollution Control District's (SJVAPCD) approach to analysis of construction impacts is to require implementation of effective and comprehensive control measures, rather than to require detailed quantification of emission concentrations for modeling of direct impacts. PM10 emitted during construction can vary greatly depending on the level of activity, the specific operations taking place, the equipment being operated, local soils, weather conditions, and other factors, making quantification difficult. Despite this variability in emissions, experience has shown that there are a number of feasible control measures that can be reasonably implemented to significantly reduce PM10 emissions from construction activities. The SJVAPCD has determined that compliance with Regulation VIII for all sites and implementation of all other control measures indicated in Tables 6-2 and 6-3 of the <i>Guide for Assessing and</i></p>

EIR ADDENDUM – TRACY APARTMENTS AT GRANT LINE ROAD PROJECT, FILIOS/DOBLER ANNEXATION EIR

TABLE 1: COMPARISON OF APPROVED PROJECT IMPACTS AND MODIFIED PROJECT IMPACTS

Environmental Issues	Filiros/Dobler Annexation Project Impacts	Tracy Apartments at Grant Line Road Project Impacts
	<p>Project would result in a significant and unavoidable cumulative impact from increases in criteria air pollutants.</p>	<p><i>Mitigating Air Quality Impacts</i> (as appropriate) would constitute sufficient mitigation to reduce PM10 impacts to a level considered less than significant.</p> <p>Construction would result in numerous activities that would generate dust. The fine, silty soils in the project area and often strong afternoon winds exacerbate the potential for dust, particularly in the summer months. Grading, leveling, earthmoving and excavation are the activities that generate the most particulate emissions. Impacts would be localized and variable. Construction impacts would last for a period of several months. The initial phase of project construction would involve grading and leveling the project site and associated improvements such as parking area improvements and supporting underground infrastructure, such as water, sewer, and electrical lines.</p> <p>Construction activities that could generate dust and vehicle emissions are primarily related to grading and other ground-preparation activities in order to prepare the project site for the construction of the apartment units and parking areas.</p> <p>Control measures are required and enforced by the SJVAPCD under Regulation VIII. The SJVAPCD considers construction-related emissions from all projects in this region to be mitigated to a less than significant level if SJVAPCD-recommended PM10 fugitive dust rules and equipment exhaust emissions controls are implemented.</p> <p>Operational Emissions</p> <p>The EIR found that the Original Project would result in significant and unavoidable operational air quality emissions. The primary source of operational emissions from the Original Project are related to mobile source emissions, as shown in Table 4.5-4 of the EIR. The Modified Project would generate fewer vehicle trips than the Original Project, and as such, mobile source emissions would be reduced under the Modified Project. Operational emissions for the Modified</p>

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TABLE 1: COMPARISON OF APPROVED PROJECT IMPACTS AND MODIFIED PROJECT IMPACTS

Environmental Issues	Filios/Dobler Annexation Project Impacts	Tracy Apartments at Grant Line Road Project Impacts
<p><i>Biological Resources</i> <i>a through f) Would the project cause a substantial adverse effect on special-status species, sensitive habitat, federally protected wetlands, wildlife movement corridors, local policies and ordinances adopted to protect biological resources, and adopted habitat or conservation plan?</i></p>	<p>The EIR identifies that Project construction activities have the potential to disrupt the foraging activities of several special status bird species, including Swainson's hawk, burrowing owl, white-tailed kite, and loggerhead shrike, as well as SJMSCP covered species, the California horned lark. In addition, during the nesting season, Project construction could result in the incidental loss of fertile eggs or nestlings, or nest abandonment for Swainson's Hawk, burrowing owl, white-tailed kite, loggerhead shrike and the California horned lark, as well as, direct injury and-or mortality of burrowing owls, white tailed kite, loggerhead shrike and the California horned lark. Moreover, construction activities associated with the Project have the potential to result in the injury or mortality of individuals of several special-status bat species and the loss of one or more maternity colonies. Project construction also has the potential to result in San Joaquin kit fox injury or mortality. Construction activities have the potential to cause short-term noise impacts on nearby foraging and nesting Swainson's hawks and burrowing owls within and adjacent to the project site. In addition, development</p>	<p>Project were calculated using the CalEEMod (v.2013.2.2) computer program. The Modified Project would generate 5.35 tons/year of ROG, which is below the SJVAPCD threshold of 10 tons/year; 9.99 tons/year of NOx, which is below the SJVAPCD threshold of 10 tons/year; and 3.69 tons/year of PM10, which is below the SJVAPCD threshold of 15 tons/year. Detailed air quality emissions calculations are provided in Appendix A.</p> <p>The Modified Project would not result in any new potential impacts related to air quality beyond those addressed in the EIR, and would not increase the severity of any impacts related to air quality.</p> <p>The EIR found that the approved project would result in less than significant Biological Resources impacts with mitigation measures implemented.</p> <p>The proposed modifications to the Filios/Dobler Annexation and Development Project are not substantial changes to the originally anticipated project relating to Biological Resources. The Modified Project does not designate any new sites for development and would not result in any changes to the location or footprint of development. Due to the site-specific nature of biological resources, the Modified Project would not result in new impacts or cause increases in the severity of previously identified impacts to biological resources when compared to the approved project. The Modified Project would not result in changes to development that would have an adverse effect on special-status species, resulting in impacts to sensitive habitats, including foraging areas, or wildlife movement corridors, and would not interfere to a greater extent with local policies, ordinances, or plans adopted relating to biological resources. Biological Resources mitigation measures identified for the originally anticipated (approved) project would be sufficient in addressing the requirements for the Modified Project.</p>

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Environmental Issues	Filios/Dobler Annexation Project Impacts	Tracy Apartments at Grant Line Road Project Impacts
<p><u>Cultural Resources</u> <i>a through d) Would the project cause a substantial adverse change in the significance of a historical, archaeological, paleontological, or geologic resource or disturb human remains?</i></p>	<p>of the Project site with the proposed commercial/office uses would result in the loss of burrowing owl habitat, Swainson's hawk foraging habitat, and potential foraging and refuge habitat for the San Joaquin kit fox. Mitigation identified in Section 4.7 (Biological Resources) requiring compensation for the loss of habitat and its conversion to urban uses, would fulfill the mitigation requirements of the SJMSCP and reduce impacts to less than significant. In addition, the Project would result in a less than significant impact due to the loss of regionally abundant vegetation types and would be unlikely to spread or introduce invasive plant and animal species resulting in less than significant impacts in this regard.</p>	
	<p>The EIR identifies that Project construction activities could expose potentially significant unrecorded cultural resources, including human remains, resulting in their damage or destruction. Mitigation measures identified in Section 4.8 (Cultural Resources) would reduce this potentially significant impact to less than significant.</p>	<p>The EIR found that the approved project would result in less-than significant Cultural Resources impacts when mitigation measures are implemented.</p> <p>The proposed modifications to the Filios/Dobler Annexation and Development Project are not substantial changes to the originally anticipated project related to cultural resources. Due to the site-specific nature of cultural resources, the Modified Project would cause no increases in severity to cultural resources when compared to the approved project. The Modified Project does not designate any new sites for development and would not result in any substantial changes to the construction methods, location or footprint of development. The Modified Project would not result in any changes to potential development that would change potential impacts associated with the disturbance of historical, archaeological, paleontological, or geologic resources. The Modified Project would also not result in any changes that would change the potential to disturb human remains. The Modified Project would not result in any new potential impacts to cultural resources and would not</p>

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Environmental Issues	Filios/Dobler Annexation Project Impacts	Tracy Apartments at Grant Line Road Project Impacts
<p><i>Geology and Soils</i> <i>a through e) Would the project expose people or structures to potential substantial adverse effects associated with seismicity, geologic or soil instability, expansive soil, result in substantial soil erosion or loss, or have soils incapable of supporting septic or alternative wastewater disposal systems?</i></p>	<p>The EIR identifies that moderate ground shaking and secondary events such as liquefaction could threaten the integrity of the proposed commercial and office buildings and the safety of people present on the Project site. Future development of the Project site would involve vegetation removal and grading, which would expose soils and increase the potential for soil erosion from wind or stormwater runoff. Underlying Project soils have high liquefaction potential, which could damage the proposed commercial and office buildings during an earthquake. Finally, the expansive nature of the Project site soils could create severe structural damage and endanger occupants and visitors of the proposed commercial and office buildings. All these potentially significant impacts could be reduced to less than significant with the implementation of the requirements of the California Building Code and the implementation of mitigation measures identified in Section 4.9 (Geology and Soils).</p>	<p>increase the significance of any potential impacts to Cultural Resources. Cultural Resources mitigation identified for the originally anticipated (approved) project in Section 4.8 (Cultural Resources) would be sufficient in addressing the requirements for the Modified Project.</p> <p>The EIR found that the approved project would result in less-than significant Geology and Soils impacts when mitigation measures are implemented.</p> <p>The proposed modifications to the Filios/Dobler Annexation and Development Project are not substantial changes to the originally anticipated project relating to Geology and Soils. Due to the site-specific nature of geologic and soils impacts, the Modified Project would cause no increases in severity to these impacts when compared to the approved project. The Modified Project would not result in changes to development patterns and does not designate any new sites for development or result in any substantial changes to the construction methods, location, or footprint of development that would change the potential for development to be exposed to geologic and soil hazards. Therefore, the Modified Project would also not result in increased impacts associated with soil erosion or septic/alternative wastewater issues. Geology and Soils mitigation identified in Section 4.9 (Geology and Soils) for the originally anticipated (approved) project would be sufficient in addressing the requirements for the Modified Project.</p>
<p><u>Greenhouse Gases</u> <i>a and b) Generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment or conflict with a plan, policy or regulation reducing greenhouse gas emission?</i></p>	<p>As identified in section 4.6 (greenhouse Gas Emissions), Project design features and implementation of measures identifies in the City's Sustainability Action Plan would not reduce Project-related greenhouse gas (GHG) emissions below San Joaquin Valley Air Pollution Control District (SJVAPCD) criteria. Thus, impacts would be significant and unavoidable. In addition, the Project's cumulative impacts associated with GHG emissions</p>	<p>The EIR found that the Original Project would result in significant and unavoidable impacts related to greenhouse gases. As shown in Table 4.6-1 of the EIR, the Original Project would directly generate approximately 22,919 metric tons/year of CO₂e. As shown in Appendix A, the Modified Project would generate approximately 5,977 tons/year of CO₂e, which is a significant reduction when compared to the Original Project.</p>

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Environmental Issues	Filios/Dobler Annexation Project Impacts	Tracy Apartments at Grant Line Road Project Impacts
<p><u>Hazards and Hazardous Materials</u> <i>a through h) Would the project create a significant hazard to the public or the environment through potential exposure to hazardous materials, wildland fires, or incidents associated with airplane facilities and uses, or conflict with implementation of plan adopted to address emergencies?</i></p>	<p>would be significant and unavoidable.</p> <p>The EIR identifies that the proposed Project has the potential to accidentally expose construction workers and occupants and visitors of the proposed commercial and office buildings to hazardous materials such as asbestos containing materials (ACMs) or lead-based paints (LBPs) from existing onsite uses, pesticide residues in the soil from past agricultural activities, and residual crude oil and Bunker C soil contamination caused by offsite oil pipeline leaks and diesel vapor intrusion from potentially contaminated groundwater. In addition, the Project may subject people or structures to a significant risk of loss, injury or death as a result of exposure to wildland fires. Compliance with all applicable federal and state laws related to the use, storage, disposal and transportation of hazardous materials would reduce the likelihood and severity of accidents during transit, resulting in a less than significant impact. In addition, the Project would not interfere with the implementation of emergency response plans and less than significant impacts would occur in this regard. All potentially significant impacts would be reduced to a less than significant level with implementation of mitigation measures identified in Section 4.10 (Hazards and Hazardous Materials).</p>	<p>The Modified Project would not result in any new potential impacts related to greenhouse gases and would not increase the significance of any potential impacts related to greenhouse gases.</p> <p>The EIR found that the approved project would result in less than significant Hazards and Hazardous Materials impacts when mitigation measures are implemented.</p> <p>The proposed modifications to the Filios/Dobler Annexation and Development Project are not substantial changes to the originally anticipated project relating to Hazards and Hazardous Materials. The Modified Project would not result in changes to development patterns and does not designate any new sites for development or result in any substantial changes to the construction methods, location, or footprint of development that would change the potential for the development to be exposed to increased risk from Hazards and Hazardous Materials. Therefore, the Modified Project would not result in changes to development patterns or potential development that would create significant hazards associated with hazardous materials, wildland fires, airplane-related impacts, or conflicts with emergency response plans. The Modified Project would not result in any new potential impacts to Hazardous Materials and would not increase the significance of any impacts to Hazardous Materials. Mitigation measures identified in Section 4.10 (Hazards and Hazardous Materials) for the originally anticipated (approved) project would be sufficient in addressing the requirements for the Modified Project.</p>
<p><u>Hydrology and Water Quality</u> <i>a through j) Would the project result in adverse environmental effects</i></p>	<p>The EIR identifies that construction and operation of the proposed Project could degrade water quality within the Project area. However, future development on the Project site would be required to comply with state and local water quality</p>	<p>The EIR found that the approved project would result in less than significant Hydrology and Water Quality impacts when mitigation measures are implemented.</p> <p>The proposed modifications to the Filios/Dobler Annexation and Development Project are not substantial changes to the</p>

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Environmental Issues	Filios/Dobler Annexation Project Impacts	Tracy Apartments at Grant Line Road Project Impacts
<p><i>associated with water quality, waste discharge, drainage patterns, groundwater supplies, runoff, flood hazards, or other hydrological hazards?</i></p>	<p>regulations, resulting in a less than significant impact in this regard. The proposed Project would increase the demand for groundwater, but it would not substantially deplete groundwater supplies, as the City has sufficient water supplies, including, groundwater sources, to serve the Project and the City's other existing and projected future water demands. In addition, the relatively large size of the groundwater basin in the Tracy area, the impervious surface and area added as a result of the Project would not adversely affect the recharge capabilities of the local groundwater basin. Thus, the Project would have less than significant impacts relating to groundwater supply and recharge. Future development would involve vegetation removal, grading and the construction of buildings, parking lots, sidewalks and driveways, which would alter existing drainage patterns and increase the potential for erosion and/or siltation. The Project would increase the amount of impervious surface onsite, which would change existing absorption rates, drainage patterns and the amount of stormwater runoff and could result in potential flooding on and offsite or exceed the capacity of existing or planning stormwater drainage systems. A combination of mitigation identified in Section 4.11 (Hydrology and Water Quality) and compliance with state and local policies and regulations would reduce these potentially significant impacts to less than significant.</p>	<p>originally anticipated project relating to Hydrology and Water Quality. The Modified Project would not result in changes to development patterns and does not designate any new sites for development or result in any substantial changes to the construction methods, location, or footprint of development that would change the potential for development to increase the significance of impacts, or risks related to Hydrology and water quality. The Project site is not located within the 100-year floodplain, and as such, the Modified Project would not place housing within the 100-year floodplain. The Modified Project would increase the demand for groundwater, but it would not substantially deplete groundwater supplies, as the City has sufficient water supplies, including, groundwater sources, to serve the Project and the City's other existing and projected future water demands. In addition, the relatively large size of the groundwater basin in the Tracy area, the impervious surface and area added as a result of the Project would not adversely affect the recharge capabilities of the local groundwater basin. Hydrology and Water Quality mitigation measures identified in Section 4.11 (Hydrology and Water Quality) for the originally anticipated (approved) project would be sufficient in addressing the requirements for the Modified Project.</p>
<p><i>Land Use and Planning a through c) Would the project physically divide an established community or conflict with any applicable land use plan, policy or</i></p>	<p>The EIR identifies the proposed Project could conflict with San Joaquin County Multi-Species Habitat Conservation Plan (SJMSCP) resulting in a potentially significant impact. Mitigation identified in Section 4.1 (Land Use Planning) requiring compensation for loss of habitat and its conversion to urban use, would</p>	<p>The EIR found that the approved project could result in a potentially significant land use impact due to the conflict with the SJMSCP. However mitigation measures identified in Section 4.1 (Land Use Planning) would reduce this impact to less than significant.</p>

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<p><i>regulation adopted for purpose of avoiding or mitigating an environmental effect?</i></p>	<p>fulfill the mitigation requirements of the SJMSCP and reduce impacts to less than significant.</p>	<p>The proposed modifications to the Filiros/Dobler Annexation and Development Project are not substantial changes to the originally anticipated project relating to Land Use Planning.</p> <p>The Modified Project would not result in changes to development patterns and does not designate any new sites for development or result in any substantial changes to the construction methods, location, or footprint of development that would change the potential for development to increase the significance of impacts related to Land Use Planning.</p> <p>Although the Modified Project may increase residential development in the City, the Modified Project will be required to comply with applicable land use policies and the requirements of the SJMSCP to avoid or mitigate environmental effects. Furthermore, the Modified Project would not physically divide an established community, nor would it conflict with the City's current General Plan Land Use regulations, or the Growth Management Ordinance. The Modified Project includes an amendment to the I-205 Corridor Specific Plan from General Commercial to High Density Residential, and requires approval of a development plan for the Project. A General Plan Amendment is not required because high-density residential development (12.1 to 25 units per acre) is allowed in the Commercial General Plan designation. The proposed change in use from commercial to high-density residential would result in a less intense land use development pattern, generating fewer vehicle trips. The Modified Project would not result in land use impacts to adjacent uses, as described in greater detail throughout this Addendum. The Modified Project supports the underlying goals of the City's General Plan to promote a compact land use pattern on lands designated for urban development. The Modified Project also supports the City's General Plan goals of providing for a range of housing types and housing options within the City.</p> <p>Therefore, the Modified Project would not result in any new</p>

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<p><u>Mineral Resources</u> <i>a and b) Would the project result in the loss of availability of a known mineral resource or recovery site?</i></p>	<p>The EIR does not identify any significant impacts to mineral resources.</p>	<p>The EIR does not identify any significant impacts to mineral resources.</p> <p>The proposed modifications to the Filios/Dobler Annexation and Development Project are not substantial changes to the originally anticipated project relating to Mineral Resources. Resources of this type are site-specific, and the Modified Project does not designate any new sites for development and would not result in any changes to the location or footprint of development. Therefore, the Modified Project would not result in any further potential impacts related to Mineral Resources, and would not increase the significance of any impacts to Mineral Resources.</p>
<p><u>Noise</u> <i>a through f) Would the project result in noise levels in excess of standards, a substantial temporary, periodic, or permanent increase in ambient noise levels, or exposure to excessive noise associated with an airport or airstrip?</i></p>	<p>The EIR identifies that project construction would result in potentially significant temporary increases in noise. However, vibration associated with construction would be less than significant. During operation, noise from mechanical equipment and loading docks could exceed City standards for sensitive uses (resulting in a potentially significant impact). Parking lot and landscape maintenance noise was determined to be less than significant. Traffic noise from the Project would not exceed City standards in the near term or long term. Railroad noise would also not exceed City standards and related impacts would be less than significant. A combination of mitigation identified in section 4.12 (Noise) and compliance with City policies and regulations would reduce the Project's potential noise impacts to less than significant.</p>	<p>The EIR found that the Original Project could result in potentially significant noise impacts associated with construction activities.</p> <p>The Filios/Dobler Annexation EIR previously examined potential noise impacts associated with increased traffic noise at off-site receptors, noise associated with proposed commercial uses (loading docks, truck circulation, commercial HVAC, parking lots, landscape maintenance, etc.), and construction noise/vibration at existing sensitive receptors. Each of these impacts were found to be less than significant.</p> <p>An analysis of potential noise impacts associated with the Modified Project is contained in Appendix B. The Modified Project is predicted to generate less traffic than previously analyzed. Therefore, the increase in traffic noise associated with the Modified Project would be less than previously analyzed and no additional analysis is required.</p> <p>On-site noise generated by the Modified Project would be less</p>

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		<p>than previously analyzed as no loading docks, truck circulation, or commercial HVAC equipment would be required for the Modified Project. Additionally, less traffic circulation would occur on this project site versus the previously analyzed land use. Therefore, no additional analysis on-site noise generation is required.</p> <p>It is expected that construction activities associated with the Modified Project would be less than or equal to the previously-analyzed project.</p> <p>The City of Tracy General Plan Noise Element establishes exterior and interior noise level limits for residential project. Policy 6 establishes a specific limit of 65 dB L_{dn} for outdoor activity areas of multi-family residential uses. However, if the primary noise source is train pass-bys, then the standard for outdoor noise levels in multi-family residential is increased to 70 dB L_{dn} (Policy 9). An interior noise level standard of 45 dB L_{dn} is also established for all residential uses under Policy 5.</p> <p>To quantify existing ambient noise levels in the vicinity of the project site, j.c. Brennan & Associates, Inc. staff conducted continuous 24-hour noise level measurements at two locations. See Figure 2 in Appendix B for noise measurement locations. The noise level measurements were conducted July 8-9, 2014. The noise level measurements were conducted to determine the existing traffic noise levels on the project site. Table 2 in Appendix B shows a summary of the noise measurement results.</p> <p>Railroad Noise Levels</p> <p>Based upon field observations the adjacent UPRR line does not appear to be in operation. Additionally, the Mountain House Master Plan EIR (San Joaquin County, July 2008) indicates that the line is a standby route with no contemplated use for freight movement. However, the Master Plan indicates that there has been discussion of using the line for commuter passenger service.</p>

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		<p>Because future use of the rail line could occur, the potential for noise impacts exists. However, it is difficult to provide an accurate assessment of railroad noise impacts without knowing the level of activity that could occur on the rail line.</p> <p>Assuming that 10 trains occurred daily and that each train generated a sound exposure level (SEL) of 100 dB at a distance of 100 feet from the railroad centerline, the day-night average (L_{dn}) noise level can be calculated using the following equation.</p> $L_{dn} = SEL + 10 \log N_{eq} - 49.4 \text{ dB, where:}$ <p>SEL is the measured SEL of the train event (100 dB at a distance of 100 feet), N_{eq} is the sum of the daytime (7 a.m. to 10 p.m.) train events plus 10 times the number of nighttime (10 p.m. to 7 a.m.) train events, and 49.4 is ten times the logarithm of the number of seconds per day. Assuming an even distribution of trains between daytime and nighttime hours, the L_{dn} would be 67 dB at 100 feet. The nearest proposed apartment building facades would be located approximately 130 feet from the railroad centerline. Table 4 in Appendix B shows the predicted railroad noise levels at the project site. The Modified Project includes plans to construct garage units and a six-foot masonry wall along the southern boundary of the site. These intervening structures would act as a noise attenuation feature, and would reduce exposure to train noise within the Project site.</p> <p>Traffic Noise Levels</p> <p>On July 8, 2014 j.c. brennan & associates, Inc. conducted short-term noise level measurements and concurrent counts of traffic on Grant Line Road at the project site. The purpose of the short-term traffic noise level measurement is to determine the accuracy of the FHWA model in describing the existing noise environment on the project site, while accounting for existing site conditions such as intervening structures, actual travel speeds, and roadway grade. Noise</p>

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<p>Population/Housing a through c) Would the project induce substantial population growth or displace substantial numbers</p>	<p>The EIR does not identify any significant impacts to population and housing. During preparation of the Initial Study and Notice of Preparation (NOP) Population and Housing was not</p>	<p>measurement results were compared to the FHWA model results by entering the observed traffic volume, speed, and distance as inputs to the FHWA model. Noise measurement sites are shown on Figure 2 in Appendix B. Table 4 in Appendix B shows the predicted traffic noise levels at exterior and interior areas of the project. Based upon the Appendix B Table 4 data, exterior noise levels are predicted to comply with the City of Tracy 65 dB L_{dn} exterior noise level standard, at common outdoor recreation areas, without additional noise control measures. Interior noise levels at the residential uses along Grant Line Road may exceed the City's 45 dB L_{dn} interior noise level standard as shown by the Appendix B Table 4 data. In order to achieve compliance with the City's interior noise level standard, additional noise control measures would be required. Specifically, all facades shall have windows and doors installed which have a minimum sound transmission class (STC) rating of 35. This includes all facades parallel or perpendicular to Grant Line Road. Windows or doors facing away from Grant Lane Road would not require this upgrade. As an alternative to the blanket STC 35 requirement, a detailed assessment of interior noise levels and required noise control measures could be prepared when building plans are available for the proposed apartment units. This analysis would need to be conducted by a qualified acoustic engineer and would need to outline the specific measures required to comply with the 45 dB L_{dn} interior noise level standard. Additionally, mechanical ventilation shall be installed in all residential uses to allow residents to keep doors and windows closed, as desired for acoustical isolation.</p>
<p>Population/Housing a through c) Would the project induce substantial population growth or displace substantial numbers</p>	<p>The EIR does not identify any significant impacts to population and housing. No housing units were proposed under the Original Project. The Modified Project would construct up to 489 residential units. Based on a standard household size of 3.27 individuals</p>	<p>The EIR does not identify any significant impacts to population and housing. No housing units were proposed under the Original Project. The Modified Project would construct up to 489 residential units. Based on a standard household size of 3.27 individuals</p>

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<p><i>of housing or people?</i></p>	<p>identified to be relevant to the Scope of the EIR (due to the Project not including residential dwellings).</p>	<p>as provided in the City of Tracy Draft Housing Element 2009-2014, the Modified Project has the potential to add up to 1,599 residents to the City. The Modified Project is currently in an area designated as a Secondary Residential Growth Area by the Tracy General Plan.</p> <p>The Residential Growth Management Ordinance (GMO) and Growth Management Ordinance Guidelines (GMO Guidelines) regulate the rate and timing of new residential development, encouraging a balance of housing types, encouraging growth that maximizes use of existing and future public services and infrastructure, and encouraging diverse housing opportunities. The GMO and GMO Guidelines establish priority and eligibility requirements for new residential development. The growth expected from the Modified Project is within the parameters of the City's Growth Management Ordinance Residential Growth Allotment. As such, the Modified Project would not induce substantial population growth beyond the levels permitted by the Tracy GMO.</p> <p>There are currently three single-family residences and their associated accessory structures, as well as a welding shop located along the Grant Line Road frontage of the Project site. Implementation of the Modified Project would likely result in the removal of these three existing houses. However, these houses would be replaced by up to 489 multi-family housing units. Therefore, the Modified Project will not displace substantial amounts of housing or people.</p>
<p>Public Services <i>a through e) Would the project have an effect upon, or generate a need fire protection, police services, parks, schools, or other public facilities?</i></p>	<p>The EIR identifies that the Tracy Fire Department (TFD) would be able to serve the proposed Project with established response times after Station 96 is relocated to 1800 W. Grant Line Road. This relocation project is scheduled to be completed in fiscal year 2012/2013 (CIP#71061). According to the Tracy Police Department, the proposed Project would result in the need for approximately 0.5 full time equivalent (FTE) of additional police officer.</p>	<p>The EIR found that the approved project would result in less than significant Public Services impacts when mitigation measures are implemented.</p> <p>Public Services</p> <p>The proposed modifications to the Filiros/Dobler Annexation and Development Project are not substantial changes to the originally anticipated project relating to Public Services. The Modified Project does not designate any new sites for</p>

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Environmental Issues	Filios/Dobler Annexation Project Impacts	Tracy Apartments at Grant Line Road Project Impacts
	<p>The Project would be subject to the public facilities impact fees to offset its impacts on police and fire facilities and, therefore, impacts would be less than significant. Moreover, the Project's impacts on schools would be less than significant with payment of school impact fees.</p>	<p>development and would not result in any changes to the location or footprint of development.</p> <p>While the Modified Project may increase future residential development in the City, there is no evidence that this will result in a substantial increase in public service needs related to police, fire or other public facilities. Furthermore, the Project would be subject to the public facilities impact fees to offset its impacts on police and fire, and other public facilities and services.</p> <p>Implementation of the proposed project would not adversely impact existing fire and emergency services within the City, and would not require the construction of new fire protection facilities.</p> <p>In order to provide adequate fire protection and suppression services to the project site, the Tracy Fire Department must have access to adequate onsite hydrants with adequate fire-flow pressure available to meet the needs of fire suppression units. The final site plans and development specifications developed for the Modified Project will indicate the location and design specifications of the fire hydrants that will be required within the Project site.</p> <p>It is not anticipated that implementation of the Modified Project would result in significant new demand for police services. Project implementation would not require the construction of new police facilities to serve the project site, nor would it result in impacts to the existing response times and existing police protection service levels.</p> <p>Implementation of the Modified Project would result in population growth within the City of Tracy, which would increase enrollment at schools within the Tracy Unified School District. Under the provisions of SB 50, a project's impacts on school facilities are fully mitigated via the payment of the requisite new school construction fees established pursuant to Government Code Section 65995.</p>

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Environmental Issues	Filiros/Dobler Annexation Project Impacts	Tracy Apartments at Grant Line Road Project Impacts
<p><u>Recreation</u> <i>a and b) Would the project result in substantial physical deterioration of recreational facilities or require construction or expansion of recreational facilities that may have an adverse physical effect on the environment?</i></p>	<p>The EIR does not identify any significant impacts associated with park and recreation facilities. During Initial Study and NOP, Recreation was not identified to be relevant in the Scope of this EIR. (due to the Project not including residential dwellings)</p>	<p>Payment of the applicable impact fees by the project applicant, and ongoing revenues that would come from taxes, would ensure that project impacts to school services are less than significant. Therefore, impacts relating to Public Services would be less than significant. The Modified Project would not result in any new potential impacts to Public Services, and would not increase the significance of any impacts to Public Services. Compliance with City Requirements and mitigation identifies in Section 4.13 for the originally anticipated (approved) project would be sufficient in addressing the requirements for the Modified Project.</p>
		<p>The Modified Project would increase demand for parks and recreational facilities within the City of Tracy, and would increase the use of the City's existing parks and recreation system. As described in the Tracy General Plan, the City maintains 48 mini-parks, 15 neighborhood parks, and eight community parks, providing approximately 256 acres at 71 sites. The City is also in the process of constructing the Holly Sugar Sports Park at the northern edge of the City, which will provide an additional 166 acres of sports parks, 86 acres of passive recreation area, and a 46-acre future expansion area for additional park facilities. The City strives to maintain a standard of 4 acres of park land for every 1,000 persons. In order to maintain this standard, the City requires new development projects to either include land dedicated for park uses, or to pay in-lieu fees towards the City's parks program. Chapter 13.12 of the Tracy Municipal Code states that, "all development projects shall be</p>

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<p>Transportation/Traffic <i>a through g) Would the project cause a substantial increase in traffic, exceed established level of service standards, result in a change in air traffic patterns, increase roadway hazards, result in inadequate emergency access or parking capacity, or conflict with adopted policies or programs supporting alternative transportation?</i></p>	<p>The EIR identifies that the addition of Project traffic to existing conditions would cause operations at the following intersections to degrade to an unacceptable level of service (LOS): Lammers Road and Grant Line Road (AM and PM); I-205 Eastbound Off-Ramp and Grant Line Road (PM peak hour and Saturday peak hour); and Access-2 and Grant Line Road (PM peak hour). Under existing conditions, the additions of Project traffic at the intersections of Access-3 and Grant Line Road could result in unsafe driving conditions. The addition of Project traffic along with near-term growth (anticipated growth at Project buildout - year 2015) would cause operations at the following intersections to degrade</p>	<p><i>required to maintain the City standard of four (4) acres of park land per 1,000 population. All development projects, as a condition of approval of any tentative parcel map or tentative subdivision map, or as a condition of approval of any building permit, shall dedicate land to the City or pay a fee in lieu thereof, or a combination of both, in order to maintain this City standard. The precise obligation of any development project to dedicate land or pay a fee pursuant to this section shall be incorporated in the implementing resolution for the park fee applicable to the development project."</i></p> <p>Rather than including land dedicated for park uses within the proposed project, the project applicant will be required to pay in-lieu fees towards the expansion of park facilities within Tracy. The payment of the project's fair share in-lieu parks fees to the City of Tracy, would ensure that this is a less than significant impact.</p> <p>As a result of the policies and requirements mentioned above, impacts resulting in the increased use of existing parks, such that substantial physical deterioration would occur or be accelerated, would be less than significant with implementation of the Modified Project.</p>
		<p>An analysis of traffic generated by the Modified Project is contained in Appendix D. The purpose of this analysis is to evaluate the conformance of Modified Project's potential traffic impacts with those described in the transportation/traffic section of the Filios-Dobler Annexation EIR. As discussed in the EIR, the Filios-Dobler service area includes the extent anticipated to be developed up to 2035. Specifically, this analysis seeks to determine whether the trip generation and distribution estimated for the project is equal to or less than the trip generation and traffic volumes assumed for the same site in the Filios-Dobler EIR. Given the assumption that the distribution and assignment of trips are the same or less than the trips generated (and analyzed) in the Filios-Dobler Annexation EIR, the CEQA mitigations</p>

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Environmental Issues	Filios/Dobler Annexation Project Impacts	Tracy Apartments at Grant Line Road Project Impacts
	<p>to an unacceptable LOS: Lammers Road and Grant Line Road; I-205 EB Off-Ramp and Grant Line Road; Access-2 and Grant Line Road; and Access-3 and Grant Line Road. Under cumulative conditions (anticipated growth at Year 2030), the proposed Project along with cumulative growth would result in unacceptable LOS at the following four intersections: Lammers Road and Grant Line Road; I-205 Eastbound Off-Ramp and Grant Line Road; Corral Hollow Road and Byron Road; and Access-2 and Grant Line Road. All impacts would be reduced to less than significant with mitigation identified in section 4.14 (Transportation/Traffic).</p>	<p>measures identified within the Filios-Dobler Annexation EIR would be assumed to adequately address the Modified Project's traffic impacts. If this assumption proves accurate, Tracy Grant Line Apartments mitigation obligations would be limited to payment of Traffic Impact Fees to offset its potential Cumulative impacts on the City road network.</p> <p>The Original Project proposed 106,700 square feet of General Office and 359,300 square feet of Shopping Center land uses resulting in an estimated 15,977 daily trips. The Modified Project would generate 11,665 daily trips, 509 AM peak hour trips (233 inbound and 276 outbound) and 1,142 PM peak hour trips (579 inbound and 563 outbound). The Modified Project's trip generation is indicated in Exhibit 2 of Appendix D.</p> <p>Exhibit 3 in Appendix D outlines the net change in trips from the previously certified Filios-Dobler Annexation EIR traffic impact analysis in comparison to the Modified Project. The Modified Project would generate 4,312 fewer trips per day, 17 trips less during the AM peak hour and 3 trips less during the PM peak hour (as compared to the analysis in the EIR). The inbound and outbound peak hour travel characteristics changed in comparison to the Original Project due to the nature of the trip origins, destinations and purpose of trips associated with an apartment use. The AM inbound trips would be reduced by 141 trips, but the outbound trips increased by 124 vehicles. For the PM peak hour inbound trips would increase by 86 and the outbound trips would increase by 89 vehicles.</p> <p>A Level of Service (LOS) analysis was completed due to the change in land use designations, and the change in project trips and trip assignments during the AM and PM peak hours for both Existing/Near Term conditions and Cumulative (2035) conditions.</p> <p>Pursuant to the analysis in the previously certified Filios-Dobler EIR, various Existing Plus Project off-site</p>

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<p><i>Utilities/Service Systems a through g) Would the project exceed wastewater treatment requirements or capacity, require the construction or expansion of utility facilities that would result in a significant environmental effects, be served by a landfill with sufficient capacity, and</i></p>	<p>The EIR identifies that there is sufficient wastewater capacity, water supply and solid waste capacity to serve the proposed Project. However, the Project would need to construct new wastewater lines and possibly additional water line and/or pump station and water reservoir improvements. Construction related impacts associated with these improvements (short-term increases in noise and air pollutants, as well as temporary traffic delays) would be reduced to less than significant with adherence to City</p>	<p>improvements/mitigations were identified to allow for site specific project development. Similar improvements are also identified for the change in land use from Mixed-use Office and Retail to Apartments in this traffic analysis. These improvements are located at Intersection #2 Lammers Road / Grant Line Road and at Intersection #3 I-205 EB Ramps / Grant Line Road. These improvements do not create new or exacerbate previously identified LOS impacts evaluated as part of the buildout of the Filios-Dobler Annexation EIR, nor do they change the characterization or nature of mitigation measures identified in the Filios-Dobler Annexation EIR for Buildout of the Project.</p> <p>Since the Filios Dobler EIR was certified, the City has updated the <i>City of Tracy Transportation and Roadway Master Plan</i> (TMP). The TMP provides a plan for implementing roadway improvements for development through 2035 conditions. The change in land use from Mixed-use Office and Retail to Apartments and the subsequent assignment of traffic to the City roadway network does not change any of the Cumulative plus Project Improvements identified in the TMP. The project would mitigate its incremental cumulative impacts by payment of the City Traffic Impact Fees. Exhibit 4 in Appendix D shows consistency between the Tracy Grant Line Apartments analysis and the previously certified EIR Cumulative conditions analysis.</p>
<p><i>Water Supply</i></p>	<p>Water Supply</p> <p>West Yost Associates prepared a technical memorandum evaluating the ability of the City's existing water distribution system to meet required minimum pressures and flows for the Modified Project (see Appendix C). Appendix C also includes an estimate of the Modified Project's water demand. The analysis in Appendix C concludes that various improvements identified in the City's Water Master Plan would ensure adequate water pressure and fire flows for the Project and existing development in the City.</p>	<p>Water Supply</p> <p>West Yost Associates prepared a technical memorandum evaluating the ability of the City's existing water distribution system to meet required minimum pressures and flows for the Modified Project (see Appendix C). Appendix C also includes an estimate of the Modified Project's water demand. The analysis in Appendix C concludes that various improvements identified in the City's Water Master Plan would ensure adequate water pressure and fire flows for the Project and existing development in the City.</p>

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Environmental Issues	Filiros/Dobler Annexation Project Impacts	Tracy Apartments at Grant Line Road Project Impacts
<p><i>comply with applicable statutes and regulations related to solid waste?</i></p>	<p>requirements and mitigation identified in Section 4.5</p>	<p>As shown in Table 1 of Appendix C, the annual water demand for the Modified Project would be 134 acre-feet per year (af/yr). The City's 2012 Water System Master Plan assumed a demand of 48 af/yr for the Original Project. The City of Tracy obtains water from both surface water and groundwater sources. The amount of water that Tracy uses from each of its water supply sources to make up its total water use varies from year to year based on contractual agreements, annual precipitation, and City policies about how to expand, utilize, and manage its water resources. As described in the 2011 City of Tracy Urban Water Management Plan- Public Review Draft, Tracy's maximum annual water supply amounts to over 31,500 acre feet per year from its various supply sources. Future agreements may increase the City's available water supply to over 49,500 acre feet per year.</p> <p>In recent years, demand for potable water in the City of Tracy has been trending downward. The 2010 total water demand in the City was 16,603 af/yr. The addition of the project's water demand would not exceed the City's available water supply. The City's water treatment and conveyance infrastructure is adequate to serve existing demand, in addition to the demand created by the proposed project. This is a less than significant impact and no mitigation is required.</p> <p>Wastewater Generation</p> <p>The Modified Project would generate a greater amount of wastewater than the Original Project. Wastewater generated by the Project would be conveyed to the Tracy Wastewater Treatment Plant (WWTP) for treatment and disposal. The City's wastewater collection system consists of gravity sewer lines, pump stations and the WWTP. Wastewater flows toward the northern part of the City where it is treated at the WWTP and then discharged into the Old River in the southern Sacramento-San Joaquin Delta.</p> <p>The City's WWTP provides secondary-level treatment of</p>

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		<p>wastewater followed by disinfection. Treated effluent from the WWTP is conveyed to a submerged diffuser for discharge into the Old River. The WWTP has an NPDES permit for discharge into the Old River from the State Regional Water Quality Control Board. The City of Tracy currently has plans to expand and improve the existing Tracy Wastewater Treatment Plant. These plans have been evaluated in the Draft and Final EIR for the Tracy Wastewater Treatment Plant Expansion (SCH No. 2000012039). The Final EIR was completed in September of 2002 and was certified in November 2002. The City plans to expand the average dry weather flow treatment capacity of the Plant from 9.0 million gallons per day to 16.0 million gallons per day. The expansion would also result in improvements to the quality of the effluent discharged from the Plant by upgrading the facility from secondary to tertiary treatment. The expansion of the Wastewater Treatment Plant is occurring in four phases. The phase expanding the treatment capacity to 10.8 mgd was completed in 2008. The final phase of the four phases is projected to be completed in the year 2014.</p> <p>The City's WWTP currently treats approximately 9.0 mgd of wastewater. For this analysis, a unit generation factor of 176 gallons per day of wastewater per residential unit was used. Therefore, the Modified Project would generate up to 87,296 gallons per day of wastewater, or 0.0087 mgd of wastewater. The addition of 0.0087mgd of wastewater would not exceed the treatment capacity of the City's WWTP. No improvements or expansions to the existing WWTP are required, and the addition of project-generated wastewater would not result in any RWQCB violations related to effluent treatment or discharge. Implementation of the Modified Project would have a less than significant impact and no mitigation is required.</p> <p>Solid Waste</p> <p>The City of Tracy has an exclusive franchise agreement with</p>

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		<p>Tracy Disposal Service for solid waste collection and disposal and recycling collection. Solid waste is collected and taken to the 40-acre Tracy Material Recovery Facility (MRF) and Transfer Station on South MacArthur Drive before being sent to the Foothill Sanitary landfill, 48 miles northeast of Tracy, off of Shelton Road east of Linden, California. The MRF is operated by Tracy Material Recovery and Solid Waste Transfer, Inc., and has capacity of approximately 1,000 tons per day, but averages approximately 350 tons per day, of which 85 percent is generated in Tracy. Approximately 175,000 tons of solid waste is generated in Tracy each year, of which approximately 27 percent is residential garbage.</p> <p>The approximately 800-acre Foothill landfill, owned by San Joaquin County, is the primary disposal facility accepting the City's solid waste. The Foothill landfill receives approximately 810 tons per day. The landfill is permitted to accept up to 1,500 tons per day, and has a permitted capacity of 51 million tons, of which approximately 45 million tons of capacity remains. It is estimated that the Foothill landfill will have the capacity to accept solid waste from the City of Tracy until 2054.</p> <p>The proposed project would not generate significant volumes of solid waste, beyond levels normally found in residential developments. The proposed project would not generate hazardous waste or waste other than common household solid waste. As described above, there is adequate landfill capacity to serve the proposed project.</p>
<p><u>Cumulative Impact:</u> <u>Aesthetics</u></p>	<p align="center">Cumulative Impacts</p> <p>Future development at the Project site and under the City's General Plan could potentially result in the alteration of the existing rural and natural landscape in the region, if not mitigated. Individual projects proposed Within the Project area must be designed consistent with policies established in the City's General Plan, 1-205 Corridor Specific Plan and</p>	<p>The proposed modifications to the Filios/Dobler Annexation and Development Project are not substantial changes to the originally anticipated project relating to Aesthetics. The Modified Project does not designate any new sites for development and would not result in any changes to the location or footprint of future development. Therefore, the</p>

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	<p>Municipal Code. In addition, project specific mitigation would reduce visual impacts on a project-by-project basis.</p> <p>Implementation of the proposed Project would not result in impacts to existing views/scenic vistas and scenic highways; therefore, the Project would not contribute to cumulative impacts with regard to views/vistas and scenic highways.</p> <p>The Project would result in a less than significant impact with regard to light and glare with implementation of recommended Mitigation Measures 4.3-2a and 4.3-2b identified in Section 4.3 (Aesthetics). Although the City General Plan EIR identifies light and glare impacts associated with General Plan build-out to be potentially significant, the General Plan EIR identifies a mitigation measure to reduce these impacts to less than significant. Therefore, with implementation of the Project-specific Mitigation Measures 4.3-2a and 4.3-213, the Project would not cumulatively contribute to light and glare impacts.</p> <p>The City General Plan EIR identified significant and unavoidable character/quality impacts with build-out of the General Plan, due to the amount of development permitted Within the City limits and Sphere of Influence. Therefore, as development occurs Within the Project site and Within the City limits and Sphere of Influence, residents and visitors in the area would notice the visual effects of urbanization. The significance of these visual and aesthetic changes is difficult to determine, as aesthetic value is subjective, potential impacts are site-specific, and cumulative project details are</p>	<p>Modified Project would not result in significant changes to development or development patterns and would not contribute more significantly to cumulative impacts. Because of the site specific nature of aesthetics, future visual impacts will need to be assessed on a project by project basis. As development occurs within the Project site, the City limits and Sphere of Influence, residents and visitors in the area could notice the visual effects of future development, but the significance of these aesthetic changes is difficult to determine, as aesthetic value is subjective, and potential future impacts are site-specific.</p>

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<p><u>Cumulative Impact: Agricultural and Forest Resources</u></p>	<p>unknown at this time.</p> <p>The City General Plan EIR analyzed the long-term development of the City and found that implementation of the City General Plan would result in the permanent conversion of farmland to non-agricultural use. The City General Plan includes several policies and actions under Objective OSC-2.1 stating that the City will work at a regional level to control the conversion of agricultural uses.</p> <p>The Project proposes the development of a mix of commercial and office uses, which would permanently convert Prime Farmland to non-agricultural use. Other projects within the County and state undertaken in agricultural areas would also convert Important Farmland to non-agricultural use. While local and regional programs and regulations would help reduce the cumulative loss of agricultural land throughout the state, the proposed Project in combination with other development projects undertaken in agricultural areas would result in a net permanent loss of agricultural land to non-agricultural use, which would be significant and unavoidable. The proposed Project's contribution would be cumulatively considerable, as there is no way to recreate the agricultural land it would convert to non-agricultural use.</p>	<p>Although the Modified Project's contribution would be considerable to the reduction of Agricultural Resources, as there is no way to recreate the agricultural land it would convert to non-agricultural use, the Modified Project would be no more severe than the previously approved Project because the Modified Project does not designate any new sites for development and would not result in any changes to the location or intensity of future development patterns. Therefore, there would be no new cumulative impact associated with the Modified Project.</p> <p>Furthermore, The City's General Plan includes several policies and actions under Objective OSC-2.1 stating that the City will work at a regional level to control the conversion of agricultural uses, and Objective OSC-2.2 to minimize conflicts between agricultural and urban uses, stating that: Development projects shall have buffer zones, such as roads, setbacks and other physical boundaries, between agricultural uses and urban development. With these policies in place, the cumulative future effects of the Modified Project would be no more significant than the previously approved project, and all future projects would have to adhere to the same City Standards of development stated above.</p>
<p><u>Cumulative Impact: Air Quality</u></p>	<p>Impacts from area and mobile source emissions would be significant and unavoidable. Since construction emissions from future development projects within the Project area cannot be mitigated to a less than significant level, operation of those projects would exceed San Joaquin Valley Air Pollution Control District (SJVAPCD) thresholds, and the construction and operation of 466,000 square feet of commercial and office uses would have similar significant and unavoidable impacts. Thus,</p>	<p>The Modified Project would result in significantly reduced air quality emissions when compared to the Original Project. As such, cumulative impacts related to air quality would be reduced under the Modified Project.</p>

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	<p>cumulative impacts would be considered significant and unavoidable.</p> <p>The proposed Project would be consistent with the City General Plan vision for UR 2 land use designation. The General Plan EIR analyzed the long-term development of the City and found that build-out under the City General Plan is projected to lead to substantial increases in vehicle miles traveled and contribute to existing air quality issues in the Basin. These air quality impacts associated with increases in regional traffic are anticipated to occur after 2030, constituting a cumulatively significant impact. As a result, the proposed Project would have significant air quality impact at both the Project and the cumulative level.</p>	
<p><u>Cumulative Impact: Biological Resources</u></p>	<p>Foreseeable future actions within the vicinity of the Project site would result in considerable disturbance to special—status Wildlife and plants, their habitats, and other sensitive biological resources. However, the incremental effect of the proposed Project, when combined with the effects created by other past and reasonably foreseeable projects, would not be cumulatively considerable or significant because the Project applicant would obtain regulatory approvals and implement the mitigation measures previously described to address direct and indirect effects of the Project. The Project would also fully comply With the terms and requirements of the San Joaquin Multi-Species Habitat Conservation and Open Space Plan (SJMSCP).</p>	<p>The incremental effect of the Modified Project, when combined with the effects created by other past and foreseeable future projects, would not be cumulatively considerable or significant because the Modified Project would be required to obtain regulatory approvals and implement the mitigation measures to address direct and indirect effects of the Modified Project in the same way as described in the previously approved project. The Modified Project would also similarly comply with the terms and requirements of the San Joaquin Multi-Species Habitat Conservation and Open Space Plan (SJMSCP), thus no new cumulative impacts would occur, and would be no more severe in nature than the Approved project.</p>
<p><u>Cumulative Impact: Cultural Resources</u></p>	<p>The proposed Project has the potential to damage or destroy potentially significant unknown cultural resources because their subsurface presence cannot be conclusively ruled out. Mitigation Measure 4.8-1</p>	<p>Similar to the approved project, the Modified Project has the potential to damage potentially significant unidentified cultural resources because the presence of these resources cannot be fully determined. The Modified Project would be</p>

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	<p>identified in Section 4.8 (Cultural Resources) would help to protect potentially significant unknown cultural resources from damage, destruction, or information loss as a result of Project construction. Therefore, implementation of this mitigation measure, along with implementation of similar mitigation measures by other projects in City's Planning Area and western San Joaquin County, would prevent the Project from cumulatively contributing to the damage or destruction of cultural resources.</p>	<p>similar in nature to the approved project and would not significantly change future development patterns, or methods of construction. Furthermore, because of the site-specific nature of Cultural Resources, future projects would have to be assessed of a site by site basis. Therefore, cumulative impacts would be no more severe than the approved project, and mitigation identified for the approved project in section 4.8 would be sufficient in addressing the Modified Project, and no new cumulative impacts would be expected.</p>
<p><u>Cumulative Impact: Geology and Soils</u></p>	<p>Future development would not result in significant effects associated with seismic or soil hazards with adherence to applicable state and local regulations, codes and requirements and implementation of Mitigation Measure 4.9-1 identified in Section 4.9 (Geology and Soils). Other individual development projects would be reviewed for seismic safety and would be required to comply with local regulations, codes and requirements. Moreover, none of the cumulative projects would reasonably be expected to be affected by the exact same seismic or soil impacts as the proposed Project due to the unique characteristics of each project and site. Therefore, the proposed Project's geology and soils impacts would not be cumulatively considerable</p>	<p>Future development associated with the Modified Project would not result in significant effects associated with seismic or soil hazards with adherence to applicable state and local regulations, codes and requirements and implementation of Mitigation Measures identified in Section 4.9 (Geology and Soils). Therefore, there will be no new impacts associated with the Modified Project. Furthermore, other individual development projects would be reviewed for seismic safety and would be required to comply with local regulations, codes and requirements. Therefore, none of the cumulative projects would reasonably be expected to be affected by seismic or soil impacts as the Modified Project due to the unique characteristics of each project and the site-specific nature of geology and soils. Therefore, future impacts would not be cumulatively considerable for the Modified Project.</p>
<p><u>Cumulative Impact: Greenhouse Gases</u></p>	<p>Although the proposed Project would be consistent with the City's General Plan and Sustainability Action Plan and would incorporate relevant measures within the Sustainability Action Plan, Project GHG emissions would not meet SIVAPCD criteria and impacts would be significant and unavoidable. Project generated GHGs in combination with GHG emissions from other known and reasonably foreseeable projects would result in a much greater</p>	<p>The Modified Project would result in significantly reduced GHG emissions when compared to the Original Project. As such, cumulative impacts related to greenhouse gases would be reduced under the Modified Project.</p>

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	<p>amount of GHG emissions.</p> <p>It is generally the case that an individual project of the size and nature of the proposed Project is of insufficient magnitude by itself to influence climate change or result in a substantial contribution to the global GHG inventory.1 GHG impacts are recognized as exclusively cumulative impacts; there are no non-cumulative GHG emission impacts from a climate change perspective.2 However, because the Project's impacts associated with GHG emissions would be significant and unavoidable, the Project's cumulative-related GHG emissions would also be significant and unavoidable.</p>	
<p><u>Cumulative Impact: Hazards and Hazardous Materials</u></p>	<p>The incremental effect of the proposed Project, when combined with the effects created by other past, present and reasonably foreseeable projects, would not be cumulatively considerable or significant because the Project would be required to comply with federal, state and local regulations, which would ensure that potential contamination or exposure to hazardous materials would be avoided or minimized on a case-by-case basis. Impacts in this regard are less than significant. With compliance of applicable federal, state and local regulations and implementation of recommended mitigation measures.</p>	<p>Because the Modified Project has similar development methods, the incremental effect of the Modified Project, when combined with the effects created by other past, present and reasonably foreseeable projects, would not be cumulatively significant because all current and future projects would be required to comply with federal, state and local regulations, which would ensure that potential contamination or exposure to hazardous materials would be avoided or minimized on a project by project basis. Impacts in this regard are less than significant with compliance of applicable federal, state and local regulations and implementation of recommended mitigation measures. The Modified Project would not result in new or increased cumulative impacts related to hazards and hazardous materials.</p>
<p><u>Cumulative Impact: Hydrology and Water Quality</u></p>	<p>Future development of the Project site would not result in any significant impacts with the implementation of mitigation measures. In addition, future development within the Project vicinity would be guided by the Tracy General Plan, and associated planning and environmental documents. Each project would be subject to the City planning</p>	<p>The Modified Project does not designate any new sites for development and would not result in any changes to the location or footprint of development. Similarly to the approved project, future development would not result in any significant impacts with a combination of mitigation measures identified in Section 4.11 (Hydrology and Water Quality) and compliance with state and local policies and</p>

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Environmental Issues	Filios/Dobler Annexation Project Impacts	Tracy Apartments at Grant Line Road Project Impacts
	<p>process. Impacts on hydrology and water quality would not be cumulatively considerable.</p>	<p>regulations would reduce these potentially cumulatively significant impacts to less than significant. In addition, future development within the Project vicinity would be guided by the City General Plan, and associated planning and environmental documents required of projects. Therefore, no cumulative impacts relative to Hydrology and Water Quality are expected with implementation of the Modified Project.</p>
<p><u>Cumulative Impact: Land Use and Planning</u></p>	<p>The City General Plan EIR analyzed the long-term development of the City and found that no significant impacts relative to land use and planning would occur with implementation of the City General Plan. The proposed Project would generally be consistent with the City General Plan vision for UR 2. However, the proposed Project would increase the amount of commercial and office use compared to the projections of the City General Plan and City General Plan EIR. Thus, the proposed Project would potentially allow more commercial and job generating uses compared to the projections of the City General Plan and City General Plan EIR. Nonetheless, these changes would not cause substantial disruption within the established community or conflicts with adopted plans and policies related to avoidance or mitigation of environmental effects. Therefore, no cumulative impacts relative to land use and planning are expected with implementation of the proposed Project.</p>	<p>The City's General Plan EIR analyzed the long-term development of the City and found that no significant impacts relative to land use and planning would occur with implementation of the City General Plan. The Modified Project site is identified in the current General Plan as a Secondary Residential Growth Area. The increases of residential units stemming from the Modified Project comply with the City's General Plan and residential growth allotment provided by the City's Growth Management Ordinance. Furthermore, the Modified Project would not require a General Plan amendment because under the current commercial zoning designation high density residential is an allowable use. However, the I-205 Specific Plan would require an amendment from general commercial to high density residential. Even so, these changes would not cause substantial disruption within an established community, or conflict with adopted plans and policies related to avoidance or mitigation of environmental effects. Therefore, no cumulative impacts relative to land use and planning are expected with implementation of the Modified Project.</p>
<p><u>Cumulative Impact: Mineral Resources</u></p>	<p>The EIR does not identify any significant impacts to</p>	<p>The California Geological Survey (CGS) has designated mineral deposits in the Tracy Planning Area as regionally</p>

EIR ADDENDUM – TRACY APARTMENTS AT GRANT LINE ROAD PROJECT, FILIOS/DOBLER ANNEXATION EIR

TABLE 1: COMPARISON OF APPROVED PROJECT IMPACTS AND MODIFIED PROJECT IMPACTS

Environmental Issues	Filiros/Dobler Annexation Project Impacts	Tracy Apartments at Grant Line Road Project Impacts
	<p>mineral resources.</p>	<p>significant. Local aggregate resources being important for minimizing construction costs, especially in a rapidly growing area such as Tracy. Furthermore, the State Division of Mines and Geology has identified Tracy as having potentially significant mineral deposits. In order to protect aggregate land and mitigate conflicts between mining activities and urban uses, the General Plan designates lands with production quality mineral reserves as "Aggregate" in the southern portion of Tracy. Presently, there are five aggregate extraction sites operating within the Tracy Planning Area. Although there are important mineral resources located in the planning area, these areas lie to the southern side of the City, and are not identified near the Modified Project site. The policies in the proposed General Plan would minimize potential land use conflicts between aggregate resource activities and other uses, and in general ensure that new development would not impact the future availability of mineral resources or mineral resource recovery sites. Therefore, no cumulative impacts relative mineral resources are expected with implementation of the Modified Project.</p>
<p><u>Cumulative Impact: Noise</u></p>	<p>The proposed Project would introduce the use of stationary equipment that would increase noise levels within the area. Based on the long-term stationary noise analysis, impacts would be less than significant. Because noise dissipates as it travels away from its source, noise impacts from onsite stationary sources would be limited to each of the respective sites and their vicinities. Future development proposals within the City would also require separate discretionary approval and CEQA assessment, which would address potential noise impacts and identify necessary attenuation measures, Where appropriate. Therefore, in conjunction with cumulative projects, the proposed Project would not have the potential to result in</p>	<p>The Modified Project would not generate noise levels in excess of those addressed in the EIR for the Original Project. As such, the Modified Project would not result in an increase in cumulative noise levels within the City of Tracy.</p>

TABLE 1: COMPARISON OF APPROVED PROJECT IMPACTS AND MODIFIED PROJECT IMPACTS

Environmental Issues	Filiros/Dobler Annexation Project Impacts	Tracy Apartments at Grant Line Road Project Impacts
	<p>cumulatively significant stationary noise impacts.</p> <p>A significant cumulative mobile noise impact would result only if both the combined and incremental effects criteria have been exceeded. Noise by definition is a localized phenomenon and drastically reduces as distance from the source increases. Consequently, only proposed projects and growth anticipated to occur in the general vicinity of the Project area would contribute to cumulative noise impacts. The proposed Project would not exceed either the combined or incremental effects criteria. Thus, the proposed Project would not result in long-term mobile noise impacts based on Project generated traffic or cumulative and incremental noise levels. Therefore, less than significant impacts would occur in this regard.</p>	
<p><u>Cumulative Impact: Population/Housing</u></p>	<p>No housing is proposed as part of this Project and, therefore, Project implementation would not directly induce population growth. As described above, projects that do not directly induce population growth still have the potential to result in indirect population growth through the creation of jobs or the extension of infrastructure into areas that were not previously served. It is possible that future employees of the proposed commercial and office uses would be hired from within the City or from other locations. Given that the majority of jobs would be in the retail sector, it is expected that most employees would already live in the City and not move there to work at the proposed commercial uses. Furthermore, retail development responds to residential growth rather than causes it.</p> <p>Moreover, the City has a residential Growth Management Ordinance (GMO), which strives to</p>	<p>Although the Modified Project may increase residential development in the City, there is no evidence that this would substantially increase overall population growth in the City. The project would construct up to 489 residential units. Based on a standard household size of 3.27 individuals as provided in the City of Tracy Draft Housing Element 2009-2014, the Modified Project has the maximum potential to add approximately 1,599 residents. Although the Modified Project could generate a greater number of residents than the previously approved Project, the population, increase would be insignificant as the population increase represents only a 1.7% increase over the City's overall current population of approximately 84,691 (assuming that all residents in the Modified Project are new to the City and no internal-migration has occurred within the City). Furthermore, population projects for 2020 show a 52.2% growth rate by 2020. Making 1.7% growth a less than significant Cumulative impact. Moreover, the Modified Project is currently in an area designated as a Secondary Residential Growth Area as</p>

EIR ADDENDUM – TRACY APARTMENTS AT GRANT LINE ROAD PROJECT, FILIOS/DOBLER ANNEXATION EIR

TABLE 1: COMPARISON OF APPROVED PROJECT IMPACTS AND MODIFIED PROJECT IMPACTS

Environmental Issues	Filiros/Dobler Annexation Project Impacts	Tracy Apartments at Grant Line Road Project Impacts
	<p>achieve a steady and orderly growth rate and allow for the adequate provision of services and community facilities. The GMO limits the number of new residential building permits to an average of 600 housing units per year for market rate housing, with a maximum of 750 units in any single year. There are exceptions for affordable housing. For these reasons, the Project would not have any significant influence on indirect residential growth.</p>	<p>identified the current General Plan. The growth expected from the Modified Project is within the parameters of the City's Growth Management Ordinance's Growth Allotment which allows 600-750 units per year. Because all future projects will have to comply with the City's General Plan, and Growth Management Ordinance, no cumulative impacts relative Population and Housing are expected with implementation of the Modified Project.</p>
<p><u>Cumulative Impact: Public Services</u> <u>Utilities/Service Systems</u></p>	<p>Future development of the Project site would not result in any significant impacts to public services, utilities and service systems with the implementation of mitigation measures. Future development Within the Project vicinity would be guided by the City and County General Plans, and associated planning and environmental documents. Each project would be subject to the City and/or County planning process. As part of this planning process, the payment of appropriate fees (including development impact fees) by all development projects would be required to mitigate any effects on public services, utilities and service systems and minimize cumulative impacts on a project-by-project basis. Furthermore, the City and County Fire Departments and Police/ Sheriff Departments would be involved in the development review process for all projects in the City and County, and would continue to provide input into the review of new projects. Future development would be also required to comply with all federal, state and local regulations and ordinances protecting utility services, including</p>	<p>The City's Growth Management Ordinance is intended to achieve a steady and orderly growth that allows for the adequate provision of services and facilities. To support this, the General Plan outlines policies to ensure the provision of adequate public services needed to provide a safe environment in Tracy (Goal PF-2, Objective PF-2.1) stating that the City will maintain adequate staffing, Objective LU-1.3 would ensure that public facilities are accessible and distributed evenly and efficiently throughout the City, and Objective LU-1.4 that ensures residential development is directed in a way to maximize the use of existing public services and infrastructure. Since some level of service staffing increases would be needed over time there may be a need for new or expanded police and fire facilities in the City. The proposed General Plan does not specifically identify potential expansion locations since it would depend on the location of new developments. However, Objective PF-2.2 P3 states that sub-stations shall be constructed in new development areas in order to meet the City's response time requirements. Furthermore the General Plan identifies that each project would be subject to the City and/or County planning process. As part of this planning process, the</p>

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Environmental Issues	Filios/Dobler Annexation Project Impacts	Tracy Apartments at Grant Line Road Project Impacts
	<p>complying with all water conservation measures and waste minimization efforts in accordance with City and County requirements. Therefore, the incremental impact associated with the proposed Project would not contribute to cumulative long-term impacts on public services, utilities and service systems and, therefore, would not be cumulatively considerable.</p>	<p>payment of appropriate fees (including development impact fees for police, fire, schools, and infrastructure improvements) by all development projects would be required to mitigate any effects on public services. Because all future projects will have to comply with the City's General Plan, and local and state requirements, no cumulative impacts relative Public Services and Utilities are expected with implementation of the Modified Project.</p>
<p><u>Cumulative Impact:</u> <u>Recreation</u></p>	<p>The EIR does not identify any significant impacts associated with park and recreation facilities.</p>	<p>Because the Modified Project will potentially add more residents to the City, there is a potential that the project could increase the use of existing recreation facilities, and or require expansion of recreation facilities. However, using the City's adopted requirement of 4 acres of parkland per 1,000 residents and the estimated population increase of 34,930 residents through 2025, there would be a need for 133 additional acres of parkland to serve development projected during the 20-year planning horizon of the General Plan. The Modified Project complies with the current General Plan and the growth allotment under the Growth Management ordinance. Furthermore, as part of approval of a final tract or parcel map, the California Quimby Act allows a city to require dedication of land, the payment of in-lieu fees or a combination of both to be used for the provision of parks and recreational purposes. Cities can require land or in-lieu fees for a minimum of 3 acres per 1,000 residents, with the possibility of increasing the requirement to a maximum of 5 acres per 1,000 residents if the City already provides more than 3 acres per 1,000 residents. Tracy's current Park Dedication Ordinance requires a dedication of 4 acres per 1,000 people for all new development projects. Also, the City of Tracy adopted a Parks Master Plan, which identifies existing park facilities, analyzes the demand for future parks, provides standards for new park facilities and identifies goals, policies and actions for the provision of park and</p>

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Environmental Issues	Filios/Dobler Annexation Project Impacts	Tracy Apartments at Grant Line Road Project Impacts
<p><u>Cumulative Impact: Transportation/Traffic</u></p>	<p>The proposed Project along with cumulative growth would result in unacceptable operations at four intersections: Lammers Road/Grant Line Road; 1-205 Eastbound Off-Ramp/Grant Line Road; Corral Hollow Road/Byron Road; and Access-Z/Grant Line Road. Mitigation Measures 4.14-10a through 4.1410c identified in Section 4.14 (Transportation/Traffic) would reduce this cumulative impact to less than significant. Although the proposed Project would add traffic to 1-205, impacts would be less than significant under cumulative conditions.</p>	<p>recreation facilities and services.</p> <p>As a result of the policies and requirements mentioned above, impacts resulting in the increased use of existing parks, such that substantial physical deterioration would occur or be accelerated, would be less-than cumulatively significant with implementation of the Modified Project.</p>
		<p>Since the Filios Dobler EIR was certified, the City has updated the <i>City of Tracy Transportation and Roadway Master Plan</i> (TMP). The TMP provides a plan for implementing roadway improvements for development through 2035 conditions. The change in land use from Mixed-use Office and Retail to Apartments and the subsequent assignment of traffic to the City roadway network does not change any of the Cumulative plus Project Improvements identified in the TMP. The project would mitigate its incremental cumulative impacts by payment of the City Traffic Impact Fees. Exhibit 4 in Appendix D shows consistency between the Tracy Grant Line Apartments analysis and the previously certified EIR Cumulative conditions analysis.</p>

REFERENCES

- BAAQMD, 2014. Air Quality Standards and Attainment Status. Site:
http://hank.baaqmd.gov/pln/air_quality/ambient_air_quality.htm
- City of Tracy, 2009-2014. Draft Housing Element. October 2011.
- City of Tracy, 2005. City of Tracy General Plan Update Draft EIR. Prepared by Design Community & Environment. October 4, 2005.
- City of Tracy, 2010. General Plan Final Supplemental EIR. Prepared by Design Community & Environment. December 1, 2010.
- City of Tracy, 2011. General Plan Final Supplemental EIR Addendum. Prepared by Design Community & Environment. February 1, 2011.
- City of Tracy, 2011. City of Tracy General Plan. Prepared by Design Community & Environment. February 1, 2011.
- City of Tracy, 1999. I-205 Corridor Specific Plan. Prepared by Harris & Associates. July 6, 1999.
- City of Tracy, 2012. Resolution 2012-214. Growth Management Ordinance. October 16, 2012
- San Joaquin County, 2010. San Joaquin County General Plan Volume II. July 4, 2010.

Tracy Apartments at Grant Line Road Conditions of Approval
Application Number D14-0011
August 27, 2014

These Conditions of Approval shall apply to the real property described as the Tracy Apartments at Grant Line Road, Planned Unit Development (PUD) Preliminary and Final Development Plan (Application Number D14-0011). The approximately 20.04-acre subject property is located on the south side of Grant Line Road, north of the Union Pacific Railroad tracks, east of Byron Road and approximately 600 feet west of Lammers Road, (Assessor's Parcel Numbers 209-270-30 and 31).

A. The following definitions shall apply to these Conditions of Approval:

1. "Applicant" means any person, or other legal entity, defined as a "Developer".
2. "City Engineer" means the City Engineer of the City of Tracy, or any other duly licensed engineer designated by the City Manager, or the Development Services Director, or the City Engineer to perform the duties set forth herein.
3. "City Regulations" means all written laws, rules and policies established by the City, including those set forth in the City of Tracy General Plan, the Tracy Municipal Code, ordinances, resolutions, policies, procedures, and the City's Design documents (the Streets and Utilities Standard Plans, Design Standards, Parks and Streetscape Standard Plans, Standard Specifications, and Manual of Storm Water Quality Control Standards for New Development and Redevelopment, and Relevant Public Facilities Master Plans).
4. "Conditions of Approval" shall mean the conditions of approval applicable to the real property described as the Tracy Apartments at Grant Line Road, Planned Unit Development (PUD) Preliminary and Final Development Plan (Application Number D14-0011). The approximately 20.04-acre subject property is located on the south side of Grant Line Road, north of the Union Pacific Railroad tracks, east of Byron Road and approximately 600 feet west of Lammers Road, (Assessor's Parcel Numbers 209-270-30 and 31).
5. "Development Services Director" means the Development Services Director of the City of Tracy, or any other person designated by the City Manager or the Development Services Director to perform the duties set forth herein.
6. "Project" means the real property consisting of approximately 20.04 acres proposed for the Tracy Apartments at Grant Line Road located on the south side of Grant Line Road, north of the Union Pacific Railroad tracks, east of Byron Road and approximately 600 feet west of Lammers Road, (Assessor's Parcel Numbers 209-270-30 and 31).
7. "Property" means the real property generally located on the south side of Grant Line Road, north of the Union Pacific Railroad tracks, east of Byron Road and approximately 600 feet west of Lammers Road, (Assessor's Parcel Numbers 209-

270-30 and 31).

8. "Subdivider" means any person, or other legal entity, who applies to the City to divide or cause to be divided real property within the Project boundaries, or who applies to the City to develop or improve any portion of the real property within the Project boundaries. "Subdivider" also means Developer. The term "Developer" shall include all successors in interest.

B. Planning Division Conditions of Approval:

1. The Developer shall comply with all laws (federal, state, and local) related to the development of real property within the Project, including, but not limited to: the Planning and Zoning Law (Government Code sections 65000, *et seq.*), the Subdivision Map Act (Government Code sections 66410, *et seq.*), the California Environmental Quality Act (Public Resources Code sections 21000, *et seq.*, "CEQA"), and the Guidelines for California Environmental Quality Act (California Administrative Code, title 14, sections 15000, *et seq.*, "CEQA Guidelines").
2. Unless specifically modified by these Conditions of Approval, the Project shall comply with all City Regulations.
3. Unless specifically modified by these Conditions of Approval, the Developer shall comply with all mitigation measures identified in the General Plan Environmental Impact Report, dated February 1, 2011 and the previously certified EIR (State Clearinghouse Number 2010072043) for the Filios/Dobler Annexation and Development Project, certified by the City on November 11, 2011.
4. Pursuant to Government Code section 66020, including section 66020(d)(1), the City HEREBY NOTIFIES the Developer that the 90-day approval period (in which the Developer may protest the imposition of any fees, dedications, reservations, or other exactions imposed on this Project by these Conditions of Approval) has begun on the date of the conditional approval of this Project. If the Developer fails to file a protest within this 90-day period, complying with all of the requirements of Government Code section 66020, the Developer will be legally barred from later challenging any such fees, dedications, reservations or other exactions.
5. Except as otherwise modified herein, all construction shall be consistent with the plans received by the Development Services Department on August 15, 2014.
6. Prior to the issuance of a building permit, the applicant shall provide a detailed landscape and irrigation plan consistent with City landscape and irrigation standards, including, but not limited to Tracy Municipal Code Section 10.08.3560, the City's Design Goals and Standards, and the applicable Department of Water Resources Model Efficient Landscape Ordinance on private property, and the Parks and Parkways Design Manual for public property, to the satisfaction of the Development Services Director. Said landscape plans shall include documentation which demonstrates there is no less than 20 percent of the parking area in landscaping, and 40 percent canopy tree coverage at tree maturity in accordance with City Regulations. Newly planted, on-site

trees shall be a minimum size of 24-inch box and shrubs shall be a minimum size of five gallons.

7. Where landscape planters are parallel and adjacent to vehicular parking spaces, the planter areas shall incorporate a 12-inch wide concrete curb along their perimeter that is adjacent to the parking space in order to allow access to vehicles without stepping into landscape planters.
8. Prior to the issuance of a building permit, an Agreement for Maintenance of Landscape and Irrigation Improvements shall be executed and financial security submitted to the Development Services Department. The Agreement shall ensure maintenance of the on-site landscape and irrigation improvements for a period of two years. Said security shall be equal to the actual material and labor costs for installation of the on-site landscape and irrigation improvements, or \$2.50 per square foot of on-site landscape area.
9. No roof mounted equipment, including, but not limited to, HVAC units, fans, antennas, and dishes whether proposed as part of this application (excluding renewable energy systems, such as photovoltaic solar), potential future equipment, or any portion thereof, shall be visible from Grant Line Road, Byron Road, or any other public right-of-way. All roof-mounted equipment shall be contained within the roof well or screened from view from the public rights-of-way by the roof of the building, to the satisfaction of the Development Services Director.
10. All vents, gutters, downspouts, flashing, electrical conduit, gas meters, electrical panels and doors, and other wall-mounted or building-attached utilities shall be painted to match the color of the adjacent surface or otherwise designed in harmony with the building exterior to the satisfaction of the Development Services Director.
11. Prior to final inspection or certificate of occupancy, all exterior and parking area lighting shall be directed downward or shielded, to prevent glare or spray of light into the public rights-of-way, to the satisfaction of the Development Services Director.
12. Prior to the issuance of a building permit, bicycle parking spaces shall be provided in accordance with Tracy Municipal Code Section 10.08.3510 to the satisfaction of the Development Services Director.
13. All PG&E transformers, phone company boxes, Fire Department connections, backflow preventers, irrigation controllers, and other on-site utilities, shall be vaulted or screened from view from any public right-of-way, behind structures or landscaping, to the satisfaction of the Development Services Director.
14. Prior to the issuance of a building permit, a lot line adjustment or other instrument shall be approved by the City and recorded to effectively merge the two lots of the Tracy Apartments at Grant Line Road Project site into one lot or relocate the property lines to conform with Tracy zoning and building codes.
15. Prior to the installation of any signs, the applicant shall submit a sign permit application

and receive approval from the Development Services Director in accordance with City Regulations. The proposed entryway monument sign shall be located and built consistent with City standards, and shall not create safety issues for pedestrians, bicycles and vehicles.

16. The Developer shall construct trash, trash compactor and recycling enclosures in substantial conformance with the project plans received by the Development Services Department on August 15, 2014 and with the following: the walls shall be of masonry construction, at least six feet in height (or the height of the bin or compactor being enclosed, whichever is higher), include solid metal doors, a solid roof, and an interior perimeter concrete curb.
17. Prior to the issuance of a building permit, the developer shall design a recycling program consistent with State Assembly Bill 341, to the satisfaction of the Public Works Director. The program shall include enclosures with adequate space for both refuse and recycling and shall be incorporated with the trash and recycling enclosures described in Planning Division Condition of Approval Number 17, above. Each enclosure shall have signs that clearly indicate refuse and recycling locations as well as prohibition of scavenging. The program shall include recycling options or elements at the pool areas and other common areas for the tenants.
18. Prior to final inspection for any residential unit of the project, the Developer shall construct a six-foot tall masonry wall (as measured from the taller finished grade on either side of the wall) along the project's east property line, adjacent to the commercially zoned property, as shown on the project site plan received by the Development Services Department August 15, 2014. The wall shall be designed with materials and colors compatible with the on-site building exterior to the satisfaction of the Development Services Director.
19. Prior to the issuance of a building permit, the developer shall document compliance with the City of Tracy Manual of Stormwater Quality Control Standards for New Development and Redevelopment (Manual) to the satisfaction of the Public Works Director, which includes the requirement for Site Design Control Measures, Source Control Measures and Treatment Control Measures under the guidelines in a project Stormwater Quality Control Plan (SWQCP). Compliance with the Manual includes, but is not limited to, addressing outdoor storage areas, loading and unloading areas, trash enclosures, parking areas, any wash areas and maintenance areas. The SWQCP must conform to the content and format requirements indicated in Appendix D of the Manual and must be approved by the Public Works Director prior to issuance of grading or building permits.
20. The project shall comply with all applicable provisions of the San Joaquin County Multi-Species Habitat Conservation and Open Space Plan, including Incidental Take Minimization Measures applicable at the time of permit and a pre-construction survey prior to ground disturbance, to the satisfaction of San Joaquin Council of Governments.
21. The developer shall design the carports, garages, clubhouses and related restroom buildings in substantial conformance with the design received by the Development Services Department on August 15, 2014 to the satisfaction of the Development

Services Director.

22. The developer shall design and construct all buildings with fire sprinklers in accordance with City Regulations to the satisfaction of the Chief Building Official.
23. The project's on-site, usable open space areas contain proposed improvements such as a "pool," "tot lot" and other improvements. Recognizing that such amenities may change from time to time over the life of the project, all usable open space area improvements shall be designed and improved consistent with City standards to the satisfaction of the Development Services Director.
24. The "metal fence" and gates identified adjacent to Grant Line Road, if constructed, shall be designed and constructed in accordance with City standards and, while providing site security, shall be of a decorative nature, such as wrought iron or tube steel construction, such that spaces in the fence elements result in the fence being predominantly "see through" and which does not create a visual barrier. The color, material, and other design elements of the fence shall be compatible with the on-site building architecture, and the height shall be the minimum necessary to provide reasonable security but not over 72 inches tall, to the mutual satisfaction of the Developer and the Development Services Director. Gates at project entries shall be designed to provide reasonable access by emergency vehicles and located such that vehicle stacking or queuing does not occur in the public right-of-way, to the satisfaction of the Development Services Director.
25. All exterior building colors shall be consistent with City standards and obtain approval by the Development Services Director prior to issuance of a building permit for the project.
26. Prior to issuance of a building permit, the developer shall demonstrate to the Development Services Director, compliance with San Joaquin Valley Air Pollution Control District Rule 9510 (Indirect Source Review), including payment of all applicable fees, to the satisfaction of the Air Pollution Control District.
27. A temporary pedestrian and bicycle pathway shall be constructed along the south side of Grant Line Road between the project site easterly boundary and the existing Tracy Marketplace development to the east if a permanent connection is not otherwise constructed prior to occupancy of the first unit in the project. The pathway shall be constructed within the Grant Line Road right-of-way. Plans for the pathway shall be submitted to and approved by the City Engineer.
28. The Developer shall provide for emergency vehicle access to the project, including acceptable access through the entryway gates, subject to approval by the City Fire Marshall and Police Chief. Plans for access shall be provided prior to issuance of a building permit. The plans shall identify proposed method of gate power. Plans shall be submitted prior to issuance of the project building permit.
29. Improvement Plans and Building Permit plans shall demonstrate the opportunity for a shared driveway at Grant Line Road with the Maibes property (Assessor's Parcel Numbers 209-270-10, 11), to the satisfaction of the Development Services Director.

Prior to the issuance of a building permit, the developer shall record an instrument (after receiving approval of such instrument by the Development Services Director) identifying the size, location, and assurance of the shared driveway at Grant Line Road if required by the City for development or use of the adjacent Maibes property.

30. Prior to the issuance of the first building permit, the developer shall do one of the following, which shall be subject to City approval:
- a. The Applicant shall enter into an agreement with the City or provide sufficient evidence, to the satisfaction of the City, which stipulates the following: (1) the Project shall annex into the Tracy Consolidated Landscape Maintenance District (TCLMD) prior to issuance of a building permit; (2) when property annexes into the TCLMD, the owners of the property will be assessed for assessment district costs related to maintenance, operation, repair and replacement of public landscaping, public walls and any special amenities as described in the TCLMD; (3) the items to be maintained include but are limited to the following: ground cover, turf, shrubs, trees, irrigation systems, drainage and electrical systems, masonry walls and other fencing, entryway monuments or other ornamental structures, furniture, recreation equipment, hardscape and any associated appurtenances within medians, parkways, dedicated easements, channel-ways, parks or open space areas; (4) prior to issuance of a building permit, the developer shall deposit a first year's assessment equivalent to the Maintenance District's first twelve months of estimated costs as determined by the City's Public Works Director; and (5) the developer shall be responsible for all costs associated with the annexation into the TCLMD; or
 - b. The Applicant shall enter into an agreement with the City or provide sufficient evidence, to the satisfaction of the City, which stipulates that prior to issuance of a building permit, the Applicant will form a Community Facilities District (CFD), Home Owner's Association (HOA) or establish another lawful funding mechanism that is reasonably acceptable to the City for the funding or performing the ongoing maintenance costs related to the maintenance, operation, repair and replacement of public landscaping, public walls and any public amenities included in the Project. The items to be maintained include but are limited to the following: ground cover, turf, shrubs, trees, irrigation systems, drainage and electrical systems, masonry walls and other fencing, entryway monuments or other ornamental structures, furniture, recreation equipment, hardscape and any associated appurtenances within medians, parkways, dedicated easements, channel-ways, parks or open space areas. Formation of the CFD shall include, but not be limited to, affirmative votes and the recordation of a Notice of Special Tax Lien. Upon successful formation, the parcels will be subject to the maximum special tax rates as outlined in the Rate and Method of Apportionment; or
 - c. The Applicant shall enter into an agreement with the City or provide sufficient evidence, to the satisfaction of the City, which stipulates that prior to issuance of a building permit, the Applicant will fund a fiscal impact study to be conducted and approved by the City to determine the long term costs related to the maintenance, operation, repair and replacement of public landscaping, public walls and any public amenities included in the Project, and deposit with the City an amount necessary to fund the full costs in perpetuity as identified by the approved study. The items to be

maintained include but are limited to the following: ground cover, turf, shrubs, trees, irrigation systems, drainage and electrical systems, masonry walls and other fencing, entryway monuments or other ornamental structures, furniture, recreation equipment, hardscape and any associated appurtenances within medians, parkways, dedicated easements, channel-ways, parks or open space areas.

31. Prior to the issuance of the first building permit, the developer shall demonstrate compliance with Tracy Unified School District's Comprehensive School Facilities Capital Improvement and Finance Plan, consistent with State law, as evidenced in the District's School Facilities Needs Analysis, and obtain certificate of compliance from the District for each new residential building permit.
32. Fences/walls on the perimeter of the 20.04-acre site and trash enclosure/compactor screen walls indicated on the plans received by the Development Services Department on August 15, 2014 are approved with this project. Additionally, temporary fences may be constructed at phase boundaries if the entire project is not constructed in one phase. Any such temporary fence shall be removed prior to occupancy of subsequent, adjacent phases, ensuring that all phases function as a single, cohesive project, including unimpeded through-circulation for project residents and emergency responders. If fences or other barriers are proposed to divide the project into two or more separated projects, the developer shall first obtain Development Services Director approval based on the following criteria:
 - a. Project boundaries are designed with curb(s), fencing, landscaping, vehicle turnarounds, or other improvements consistent with City standards and approved, on-site improvements of the project;
 - b. The proposal is consistent with City Police Department security and Fire Department emergency vehicle access and circulation standards;
 - c. The proposal undergoes City Engineer review of fronting or nearby public right-of-way improvements (such as left-turn movements into the project from Grant Line Road) to ensure compliance with City standards; and
 - d. Appropriate easements, maintenance agreements, or other coordination documentation is executed or recorded regarding shared utilities, access, common improvements, lift station, etc.

The Development Services Director may approve the interior fences and may impose reasonable design or improvement requirements to achieve City standards. The costs of any consultant services required by the Development Services Director to undergo City review of the proposal shall be paid by the developer.

33. The final landscape plan shall include trees to be located within the stormwater quality planter along the south property line to enhance the appearance of the project from Byron Road. Tree species, spacing, and locations shall be compatible and consistent with operational requirements of the stormwater quality planters to the satisfaction of the Development Services Director. This may include use of either smaller accent trees planted in clusters or larger canopy trees which can be more evenly spaced along the railway frontage.

34. The developer shall be responsible for complying with the City's interior noise level standard. This shall include use of additional noise control measures for the first row of buildings constructed along Grant Line Road. Specifically, all facades shall have windows and doors installed which have a minimum sound transmission class (STC) rating of 35. This includes all facades parallel or perpendicular to Grant Line Road. As an alternative to this requirement, a detailed assessment of interior noise levels and required noise control measures could be prepared when building plans are available for the proposed apartment units. This analysis shall be conducted by a qualified acoustic engineer and shall outline the specific measures required to comply with the 45 dB Ldn interior noise level standard. Final design plans shall be subject to City review and approval prior to issuance of building permits.
35. Mechanical ventilation shall be installed in all residential uses to allow residents to keep doors and windows closed, as desired for acoustical isolation from traffic and future railroad noise.
36. Prior to the issuance of the first building permit, on-site parking area lighting shall be designed to achieve the City's standard of a minimum one foot candle throughout the entire parking area.

C. Engineering Division Conditions of Approval

C.1. Grading Permit

The City will not accept grading permit application for the Project until the Developer has provided all relevant documents related to said grading permit required by the applicable City Regulations and these Conditions of Approval, to the satisfaction of the City Engineer, including, but not limited to, the following:

- C.1.1 Grading and Drainage Plans prepared on a 24" x 36" size polyester film (mylar). Grading and Drainage Plans shall be prepared under the supervision of, and stamped and signed by a Registered Civil Engineer.
- C.1.2 Payment of the applicable Grading Permit fees which include grading plan checking and inspection fees, and other applicable fees as required by these Conditions of Approval.
- C.1.3 Three (3) sets of the Storm Water Pollution Prevention Plan (SWPPP) for the Project with a copy of the Notice of Intent (NOI) submitted to the State Water Quality Control Board (SWQCB) and any relevant documentation or written approvals from the SWQCB, including the Wastewater Discharge Identification Number (WDID#).
 - C.1.3.1 After the completion of the Project, the Developer is responsible for filing the Notice of Termination (NOT) required by SWQCB. The Developer shall provide the City with a copy of the completed Notice of Termination.
 - C.1.3.2 The cost of preparing the SWPPP, NOI and NOT, including the filing fee of the NOI and NOT, shall be paid by the Developer.
 - C.1.3.3 The Developer shall comply with all the requirements of the SWPPP and applicable Best Management Practices (BMPs) and the applicable provisions of the City's Storm Water Management Program.
- C.1.1. Two (2) sets of the Project's Geo-technical Report signed and stamped by a licensed Geo-technical Engineer licensed to practice in the State of California, as required in Condition C.3.1(a), below. The technical report must include

- relevant information related to soil types and characteristics, soil bearing capacity, percolation rate, and elevation of the highest observed groundwater level.
- C.1.2. A copy of the Approved Fugitive Dust and Emissions Control Plan that meets San Joaquin Valley Air Pollution Control District (SJVAPCD) as required in Mitigation Measure 4.5-1 and 4.5-2 of the Mitigation Monitoring and Reporting Program of the Addendum to the Filios/Dobler Annexation & Development Project Environmental Impact Report (Filios/Dobler EIR).
 - C.1.3. Documentation of any necessary authorizations from Regional Water Quality Control Board (RWQCB) as required in the applicable mitigation measures identified in the Filios/Dobler EIR.
 - C.1.4. Two (2) sets of Hydrologic and Storm Drainage Calculations for the design of the on-site storm drainage system and for determining the size of the project's storm drainage connection, as required in Conditions C.3.1(e) and (g), below.
- C.2. Encroachment Permit - No applications for encroachment permit will be accepted by the City as complete until the Developer provides all relevant documents related to said encroachment permit required by the applicable City Regulations and these Conditions of Approval, to the satisfaction of the City Engineer, including, but not limited to, the following:
- C.2.1. Improvement Plans prepared on a 24" x 36" size 4-mil thick polyester film (mylar) and these Conditions of Approval. Improvement Plans shall be prepared under the supervision of, and stamped and signed by a Registered Civil, Traffic, Electrical, Mechanical Engineer, and Registered Landscape Architect for the relevant work.
 - C.2.2. Two (2) sets of structural calculations signed and stamped by a Structural Engineer licensed in the State of California, as required in Condition C.3.1 (b), below.
 - C.2.3. Signed and stamped Engineer's Estimate that summarizes the cost of constructing all the public improvements shown on the Improvement Plans.
 - C.2.4. Signed and notarized Offsite Improvement Agreement (OIA) and Improvement Security, to guarantee completion of the identified public improvements that are necessary to serve the Project as required by these Conditions of Approval. The form and amount of Improvement Security shall be in accordance with Section 12.36.080 of the Tracy Municipal Code (TMC), and the OIA. The Developer's obligations in the OIA shall be deemed to be satisfied upon City Council's acceptance of the public improvements and release of the Improvement Security.
 - C.2.5. Signed and notarized Deferred Improvement Agreement (DIA) and Improvement Security, to allow deferment of completion of improvements as required by these Conditions of Approval. The form and amount of Improvement Security shall be in accordance with the DIA and Section 12.36.080 of the TMC. The Developer's obligations in the DIA shall be deemed to be satisfied upon the release of the Improvement Security.
 - C.2.6. Check payment for the applicable of engineering review fees which include plan checking, permit and agreement processing, testing, construction inspection, and other applicable fees as required by these Conditions of Approval. The engineering review fees will be calculated based on the fee rate adopted by the City Council on April 15, 2014, per Resolution 2014-059.

- C.2.7. Traffic Control Plan signed and stamped by a Registered Civil Engineer or Traffic Engineer licensed in the State of California, as required in Condition C.3.3, below.
- C.2.8. Signed and notarized Pipeline Crossing Agreement with UPRR, for the installation, use, repair, and maintenance of the Project's permanent water connection from Byron Road through the UPRR right-of-way, as required in Condition 3.2(i), below.
- C.3. Improvement Plans - Improvement Plans shall contain the design, construction details and specifications of public improvements that is/are necessary to serve the Project and as . The Improvement Plans shall be drawn on a 24" x 36" size 4-mil thick polyester film (mylar) and shall be prepared under the supervision of, and stamped and signed by a Registered Civil, Traffic, Electrical, Mechanical Engineer, and Registered Landscape Architect for the relevant work. The Improvement Plans shall be completed to comply with City Regulations, these Conditions of Approval, and the following requirements:
- C.3.1 Grading and Storm Drainage Plans
- Site Grading
- a) Include all proposed erosion control methods and construction details to be employed and specify materials to be used. All grading work shall be performed and completed in accordance with the recommendation(s) of the Project's Geo-technical Engineer. A copy of the Project's Geo-technical Report must be submitted with the Grading and Storm Drainage Plans.
 - b) When the grade differential between the Project Site and adjacent property(s) exceeds 12 inches, a reinforced or masonry block, or engineered retaining wall is required for retaining soil. The Grading Plan shall show construction detail(s) of the retaining wall or masonry wall. The entire retaining wall and footing shall be constructed within the Project Site. A structural calculation shall be submitted with the Grading and Storm Drainage Plans.
 - c) An engineered fill may be accepted as a substitute of a retaining wall, if the grade differential is less than 2 feet and subject to approval by the City Engineer. The Grading and Storm Drainage Plans must show the extent of the slope easement(s). The Developer shall be responsible for obtaining permission from owner(s) of the adjacent and affected property(s). The slope easement must be recorded, prior to the issuance of the final building certificate of occupancy.
 - d) Site grading shall be designed such that the Project's storm water can surface drain directly to a public street that has a functional storm drainage system with adequate capacity to drain storm water from the Project Site, in the event that the on-site storm drainage system fails or it is clogged. The storm drainage release point is recommended to be at least 0.70 foot lower than the building finish floor elevation and shall be improved to the satisfaction of the City Engineer.
- Storm Drainage
- e) The Developer shall design and install the Project's permanent drainage connection(s) to the City's existing storm facility located on Grant Line Road/ Lammers Road per City Regulations and as approved by the City Engineer. Storm drainage calculations for the sizing of the on-site storm drainage

system and the Project's permanent storm drainage connection must be submitted with the Improvement Plans.

- f) The Project's permanent storm drainage connection(s) shall be designed and constructed to drain by gravity to the City's Westside Storm Drainage Outfall System (Detention Basin 10/11 or DB 10/11). The Project's permanent storm drainage connection(s) shall include the installation of a storm drain pipeline (with adequate capacity) on Grant Line Road from the Property to the existing reinforced concrete junction structure located on Grant Line Road and Lammers Road, and shall be completed by the Developer, prior to any paving work within the Property.
- g) The design and construction details of the Project's permanent storm drainage connection shall meet City Regulations and shall comply with the applicable requirements of the City's Storm Water Quality Control Standards and storm water regulations that were adopted by the City Council in 2008 and any subsequent amendments. The design and construction details of the Project's permanent storm drainage connection shall be incorporated in the Offsite Improvement Plans.
- h) Prior to the final inspection of the first building to be constructed on the Property, the Developer shall submit a signed and notarized Stormwater Treatment Facilities Maintenance Agreement (STFMA) as a guarantee for the performance of Developer's responsibility towards the repair and maintenance of on-site storm water treatment facilities. Calculations related to the design and sizing of on-site storm water treatment facilities must be submitted with the STFMA and the Grading and Storm Drainage Plans.

C.3.2 Offsite Improvement Plans
Sanitary Sewer

- a) Temporary Sewer Connection - The City will allow the Developer to install a private sewer connection to the existing Hansen Sewer Collection System near the southeast corner of the Property as a temporary sewer connection in the interim condition until such time that a permanent sanitary sewer facility for the Project on Lammers Road ("Westside Catchment Sewer System") is constructed and becomes available for connection by the approved users. It is the Developer's responsibility to design and install a gravity sewer pipeline from the Property to the Westside Catchment Sewer System on the location approved by the City.

The Developer shall design and construct all on-site sewer improvements such that the Project's permanent sewer connection shall function as a gravity sewer line. A drop sewer manhole will be required at the point of connection with the Hansen Sewer Collection System. The Developer shall obtain Encroachment Permit, and pay plan checking and engineering inspection fees, prior to starting work within the City's sewer easement. The Developer is responsible for repairing and maintaining the temporary sewer connection up to the drop sewer manhole.

The City will be responsible for repairing and maintaining the drop sewer manhole. Prior to the issuance of the certificate of occupancy of the first building within the Property, the Developer shall dedicate to the City, a 10 foot wide permanent sanitary sewer easement from Grant Line Road to the

point of connection with Hansen Sewer Collection System, for City's access to the drop sewer manhole. All requirements relating to access and maintenance by the Public Works Department shall be incorporated into the design of on-site improvements. The Grant of Easement must be filed at the Office of the San Joaquin County Recorder, prior to the issuance of Encroachment Permit. The Developer is responsible for all costs associated in dedicating the necessary easement(s) to the City including the cost of preliminary title report, and preparing the easement document, legal description and plat map.

The Developer shall remove the temporary sewer connection when the permanent sewer connection is constructed. In order to guarantee completion of the Developer's obligation to remove the temporary sewer connection, the Developer shall sign an improvement agreement (Deferred Improvement Agreement), prior to the issuance of the building permit of the first building to be constructed within the Property.

- b) Permanent Sewer Connection - According to the Tracy Wastewater Master Plan (TWMP) that was adopted by the City Council on January 15, 2013, per Resolution 2013-008, the Property is within the planned service area of the Westside Catchment Sewer System (Page 5-1 of the TWMP). In the ultimate condition, the Property is to be served by the Westside Catchment Sewer System (Figure 5-1 of the TWMP). The Developer is responsible to design and construct a permanent sewer line with adequate capacity to serve the Project from the Property to the Westside Catchment Sewer System.

Option 1 (Utilize Hansen Sewer Collection System) – Per the TWMP, the Hansen Sewer Collection System is not considered as a sewer conveyance facility in the ultimate condition for this Property. The City may allow the Developer to utilize the Hansen Sewer Collection System as part of the permanent sewer connection to convey sewage from the Property to the Westside Catchment Sewer System, if the City Council approves the amendment to the TWMP, and if the method of conveying the sewage including the design and construction details of the proposed sewer connection meets the approval of the City Engineer. The City will engage the services of the City's Sewer Consultant, to assist the City Engineer in the review of the design and construction details of the proposed sewer connection. The Developer will pay for the cost of amending the TWMP including the cost of City Consultant(s) services at the time the Developer submits a written request to amend the TWMP.

Option 2 (Gravity Sewer Line on Grant Line Road) – The Project can be served by a new gravity sewer line on Grant Line Road from the Property to the Westside Catchment Sewer System. With this option, the City agreed to accept a cash payment in the amount \$260,000 (In-Lieu Fee) which represents the Project's proportional share towards the cost of designing and installing a new gravity sewer line on Grant Line Road from the Property to the Westside Catchment Sewer System. The In-Lieu Fee shall include cost of shoring and dewatering of trenches, protecting affected utilities, traffic

control, replacing asphalt concrete pavement, reconstructing curb, gutter and sidewalk, restoring pavement markings and striping, and other streets and utilities improvements that are disturbed as a result of installing the gravity sewer line on Grant Line Road. The In-Lieu Fee also includes cost of design, plan checking, engineering inspection, and testing. Developer may request establishment of a Benefit District for reimbursement of fair share costs by any future users of this sewer line installed by the Developer. The Developer shall deliver the In-Lieu Fee to the City at the time of issuance of the building permit for the first building to be constructed within the Property. If the Developer pursues this option, there are certain improvements that are to be designed and constructed by the Developer as part of the on-site improvements. The improvements include a gravity sewer line from the Project to Grant Line Road and a new sewer manhole on Grant Line Road. The new sewer manhole on Grant Line Road shall be located on the travel lane on the westbound Grant Line Road.

- c) Prior to the issuance of Grading Permit for the Project, Developer shall submit improvement plans and secure approval of plans from the City's Building Division, for the design of on-site sewer improvements and the Project's temporary sewer connection.

Water Distribution System

- d) Water Distribution Mains on Grant Line Road and Byron Road - The City's Water Consultant (West Yost & Associates) has completed the technical evaluation of the ability of the City's existing water distribution system to meet required minimum pressures and flows for the proposed Project. The results of that analysis are included in the Technical Memorandum dated August 4, 2014 entitled "Hydraulic Evaluation of Grant Line Road Apartments" (Technical Report). The Developer shall comply with all the recommendations in the Technical Report. All water connections that are bigger than 2 inches in diameter shall be Ductile Iron Pipe (DIP).

The Developer shall complete the design and installation of the 16-inch diameter DIP on Grant Line Road and 12-inch diameter DIP on Byron Road and under the Union Pacific Railroad Company (UPRR) property (Offsite Water Line Improvements) and the Project's permanent water connections as described in the Technical Report, prior to final inspection of the first building to be constructed within the Property.

In order to guarantee completion of the Offsite Water Line Improvements, the Developer shall enter into an improvement agreement (Offsite Improvement Agreement or OIA) and post improvement security in the amounts and form in accordance with section 12.36.080 of the TMC, and as required by these Conditions of Approval. The Developer shall submit the signed and notarized OIA with the necessary improvement security, prior to the issuance of the Grading Permit. If the Developer completes the installation of the Offsite Water Line Improvements, the Developer will be entitled to fee credit in accordance with the OIA and Title 13 of the TMC. Fee credits shall apply to

the program portion of the Offsite Water Line Improvements. The amount of fee credit shall be determined during the review of improvement plans. All costs associated with the installation of the Project's permanent water connection(s) as identified in the Technical Report including the cost of removing and replacing asphalt concrete pavement, pavement marking and striping such as crosswalk lines and lane line markings, replacing traffic detecting loops, conduits, and wires, relocating existing utilities that may be in conflict with the water connection(s), and other improvements shall be paid by the Developer.

If water main shut down is necessary, the City will allow a maximum of 4 hours water supply shutdown. The Developer shall be responsible for notifying residents or business owner(s), regarding the water main shutdown. The written notice, as approved by the City Engineer, shall be delivered to the affected residents or business owner(s) at least 72 hours before the water main shutdown. Prior to starting the work described in this section, the Developer shall submit a Water Shutdown Plan and Traffic Control Plan to be used during the installation of the offsite water mains.

- e) Other Offsite Water Line Improvements – The Technical Report identified certain improvements to the existing water distribution system that are needed to correct the existing deficiencies of the water distribution system and to mitigate for the impacts of the Project (Attachment A of the Technical Report). These improvements include the following:
- Replacing the existing 12-inch diameter water main on Sixth Street with an 18-inch diameter DIP T-main.
 - Replacing the existing 12-inch diameter water main on Tracy Boulevard with an 18-inch diameter DIP T-main.
 - Upsizing of an existing 12-inch diameter pipeline located on Eleventh Street, east of Tracy Boulevard with a 16-inch diameter DIP pipe.
- The City will construct these water line upgrade improvements as part of its Water Facility Capital Improvement Program. The Developer's responsibility will be considered to have been fully satisfied upon payment of the water development impact fees specified in Condition C4.1, below.

- f) Domestic and Irrigation Water Services – The Developer shall design and install domestic and irrigation water service connection, including a remote-read master water meter (the water meter to be located within City's right-of-way) and a Reduced Pressure Type back-flow protection device in accordance with City Regulations. The City will allow two remote-read master water meters for the Project, one remote-read master water meter for each residential phase. The domestic and irrigation water service connection(s) must be completed before the final inspection of the building. Sub-metering will be allowed within private property. The City will not perform water consumption reading on sub-meters. The Developer will be responsible for relocating or reinstalling water sub-meters.

The City shall maintain water lines from the master water meter to the point of connection with the water distribution main (inclusive) only. Repair and maintenance of all on-site water lines, laterals, sub-meters, valves, fittings, fire hydrant and appurtenances shall be the responsibility of the Developer.

- g) Fire Service Line – The Developer shall design and install fire hydrants at the locations approved by the City’s Fire Safety Officer and Chief Building Official. Prior to the approval of the Improvement Plans, the Developer shall obtain written approval from the City’s Fire Safety Officer and Chief Building Official, for the design, location and construction details of the fire service connection to the Project, and for the location and spacing of fire hydrants that are to be installed to serve the Project.
- h) UPRR Water Line Crossing - The proposed water line crossing within the UPRR property will require written permission or pipeline crossing agreement and permanent utility easement from UPRR. The Developer shall submit a signed pipeline crossing agreement prior to starting work within UPRR’s right-of-way. The Developer shall pay all costs associated with obtaining written permission and a pipeline crossing agreement with UPRR and the granting of the necessary utility easements. The pipeline crossing agreement will require approval from the City Council. The Developer shall also obtain tunnel classification relative to flammable gas or vapors, and submit all required documentation to meet applicable requirements of the California Occupational Safety and Health Administration. The Developer shall provide the City the tunnel classification issued by the Cal-OSHA Department of Industrial Relations, prior to starting the work.

Street Improvements:

- i) Roadway Improvements Frontage Responsibility – Per the Citywide Roadway & Transportation Master Plan (CRTMP) that was adopted by the City Council on November 26, 2012, pursuant to Resolution 2012-240, Grant Line Road between Lammers Road and Byron Road will be a 4-lane major arterial street with a minimum right-of-way of 97 feet. According to the CRTMP (Figure 5.1 – Roadway Improvement Cross Section Responsibility per Frontage Policy), the Developer is responsible to design and construct the outside travel lane (plus shoulder) and the landscape strip behind the curb up to the property line. Any travel lane(s) or left-turn and right-turn lane(s) along the Property’s frontage or at all the access points on Grant Line Road that are provided and are necessary to meet access spacing requirements are considered to be site specific offsite improvements and they are Developer’s responsibility to design and construct without any reimbursement from the City.
- j) Right-of-Way on Grant Line Road – The Developer shall dedicate right-of-way along the entire frontage of the Property on Grant Line with variable width of 35.5 feet and approximately 45.5 feet at the bus-turn out measured from the existing right-of-way line of Grant Line Road towards the Property. The Developer shall execute a Grant Deed to convey the land in fee title and submit legal description and plat map that describes the area to be

dedicated, prior to City Council's acceptance of the public improvements. The cost of right-of-way dedication including the cost of preparing the legal description and plat map will be paid by the Developer.

The Developer will be required to offer to the City for dedication all lands that are required for extending for the proposed pedestrian and bike path improvements on Grant Line Road, if necessary. The offer of dedication for roadway right-of-way described above has to be made and filed at the Office of the San Joaquin County Recorder, prior to the acceptance of the public improvements.

The City will assume responsibility to maintain the public improvements and accept the offer of dedication for right-of-way on Grant Line Road after the City Council accepts the public improvements.

- k) Frontage Improvements on Grant Line Road – The Developer shall design and construct all roadway improvements on Grant Line Road that is necessary to provide safe and functional access(s) to the Project for each phase and at Project's build-out condition, as described by the Technical Memorandum prepared by Kimley-Horn and Associates, titled "Grant Line Apartments Phase 1, Phase 2 and Ultimate Layouts" dated August 20, 2014 (Traffic Report), and as required by these Conditions of Approval and as approved by the City Engineer. The Traffic Report is on file with the Office of the City Engineer and is available for review upon request.
- Phase 1 - The roadway improvements required to serve Phase 1 involves the widening of the south side of Grant Line Road along the frontage of Phase 1 and pavement transitions on both sides of Phase 1 on Grant Line Road and other improvements which includes but not limited to, the installation of new asphalt concrete pavement, concrete curb, gutter, sidewalk, driveway, Class II bike lane(s), handicap ramp(s), crosswalk, bus shelter with turnout and associated improvements, and parkway landscaping improvements with automatic irrigation system (Motorola Controller), storm drainage, catch basin/ drop inlet, fire hydrant, domestic, irrigation and fire services street light, traffic sign(s), pavement marking and striping along the entire frontage of Phase 1 on Grant Line Road, and other improvements such as barricade, signing, and striping that are necessary to provide a safe transition to and from a widen roadway section of Grant Line Road (Phase 1 Roadway Improvements). The Phase 1 Roadway Improvements include the installation of a traffic signal at the intersection of the main entrance to the Phase 1 development with Grant Line Road. The intersection improvements will include but not limited to, traffic detection loops, traffic loops pull boxes, conduits and wires, audible pedestrian warning, electronic sign, crosswalk, pavement legend and marking, lane marking, traffic sign, and other improvements as determined by the City Engineer that are deemed to be necessary to have a safe and functional traffic signal. Design and construction of Phase 1 Roadway Improvements shall be completed by the Developer, prior to final inspection of the first building to be constructed within the Property.

Phase 2 - The roadway improvements involved is generally the widening of the south side of Grant Line Road along the entire frontage of the Property including the required pavement transitions and other improvements which includes but not limited to, the installation of new asphalt concrete pavement, concrete curb, gutter, sidewalk, driveway, Class II bike lane(s), raised median with landscaping, handicap ramp(s), crosswalk, , and parkway landscaping improvements with automatic irrigation system (Motorola Controller), storm drainage, catch basin/ drop inlet, fire hydrant, domestic, irrigation and fire services street light, traffic sign(s), pavement marking and striping along the entire frontage of the Property on Grant Line Road, and other improvements such as temporary asphalt concrete paving, barricade and guardrail, signing, and striping that are necessary to provide a safe transition to and from a widen roadway section of Grant Line Road (Phase 2 Roadway Improvements). The Phase 2 Roadway Improvements include the construction of the one main entrance and one driveway on Grant Line Road. Design and construction of Phase 2 Roadway Improvements shall be completed by the Developer, prior to final inspection of the first building to be constructed within the Property.

If the Developer completes the construction of roadway improvements on Grant Line Road as described in these Conditions of Approval, the Developer may be entitled to reimbursement for the program portion of the roadway improvements on Grant Line Road in accordance with the OIA and Title 13 of the Tracy Municipal Code. The amount of reimbursement shall be determined during the review of improvement plans.

- l) Option To Build One Major Entrance – The City may allow the Developer to build one major entrance to the Property on Grant Line Road provided the Developer agrees to comply with the following requirements:
- 1) Provide a revised site plan that meets City requirements on on-site traffic circulation, private landscaping, parking, building setback(s), access/driveway spacing, and others, to the satisfaction of the Development Services Director and the City Engineer.
 - 2) Pay for the cost of peer review by City's Traffic Consultant on the design of all traffic-related improvements on Grant Line Road. This payment is in addition to the standard engineering review fees (plan checking, agreement and permit processing, engineering inspection, testing, and program management fees).
 - 3) Comply with the recommendations from the City's Consultant regarding the geometric configuration of the major entrance on Grant Line Road.
 - 4) Complete the design and construction of all roadway improvements on Grant Line Road including the traffic signal per the approved construction phasing plan and improvement plans, prior to occupancy of the first building constructed within the Property.
 - 5) Enter into an improvement agreement with the City and post improvement security in the amount of approved by the City Engineer, to assure completion of all roadway improvements on Grant Line Road.
 - 6) All other requirements in these Conditions of Approval that are applicable shall remain.

- m) Offsite Improvement Agreement – Prior to starting any work on either the Phase 1 Roadway Improvements or Phase 2 Roadway Improvements, the Developer shall sign an improvement agreement (Offsite Improvement Agreement or OIA) and post improvement security in accordance with Section 12.36.080 of the TMC, to guarantee completion of the public improvements. The OIA requires approval from the City Council.

Prior to the approval of the OIA, the Developer will be required to submit Improvement Plans that contains the design, construction details and specifications of all public improvements that are required to serve the Project, prepared in a 24" x 36" size polyester film (mylar), signed and stamped by the Design Engineer, for City's approval and signature. The Developer shall also submit Technical Specifications and Cost Estimates. All engineering calculations for the design of the improvements must be submitted as part of the Improvement Plans.

The Developer will be required to pay Engineering Review Fees which include plan checking, agreement and permit processing, testing, engineering inspection, and program management fees, prior to the approval of the OIA.

- n) The Technical Memorandum prepared by Kimley-Horn and Associates, titled "Tracy Grant Line Apartments TIA Consistency Memorandum" dated August 19, 2014 and the Memorandum dated August 21, 2014 titled "Grant Line Apartments: Apartments Project Only Conditions of Approval" from Kimley Horn and Associates (Traffic Impact Report) identified the Project's traffic impacts that are to be mitigated by the Developer and the recommended mitigations are summarized below. The Traffic Impact Report is on file with the Office of the City Engineer and is available for review upon request.

Grant Line Road/ Corral Hollow Road – Prior to issuance of the first building permit within the Project, the Developer shall pay \$28,080 for the fair share cost of the Project towards lengthening the northbound left-turn lanes and modifying (or shortening) of the bay taper on Corral Hollow Road, to provide additional left-turn storage from Corral Hollow Road onto Grant Line Road. The City will be responsible for modifying the traffic signal timing plan.

- o) The Developer shall grant access rights to the City for the use, operation, repair, and maintenance of traffic detecting loops, wires, conduits, and pull boxes that will be located within the Property. The Developer submit a signed and notarized Grant of Easement and provide legal description and plat map that describes the easement area, for the dedication of the utility easement described above, prior to the acceptance of the public improvements. The Developer shall pay for the cost of dedicating easement and preparing the legal description and plat map.
- p) To provide pedestrian access to the Project from existing developments on Grant Line Road, the Developer is required to install an interim sidewalk on

Grant Line Road from the Property to the existing sidewalk on Grant Line Road in front of the Walmart/Costco area. The interim sidewalk shall have a structural section of 3" asphalt concrete and 8" Class II aggregate base and shall be installed within the existing right-of-way and on the south side of Grant Line Road along the Dobler Property. The design and construct details of the interim sidewalk shall be included on the Offsite Improvement Plans. Cost of designing and constructing the interim sidewalk shall be paid by the Developer without any reimbursement from the City.

- q) The construction details and specifications of the two bus shelter(s) on Grant Line Road shall be provided by the City at the time of review of Offsite Improvement Plans. The cost of the bus shelters and turnout and associated improvements are to be paid by the Developer without any reimbursement from the City.
- r) The westernmost driveway shall be designed to function as a joint driveway (future) to provide access to Maibe's property. Private Access Easement to facilitate future improvements (by Maibe's property to connect to this driveway) and access at this driveway shall be recorded with the County Recorders' Office. Details of the right-in/right-out driveway to be installed with Phase 2 Roadway Improvements, layout of the future joint access, private access easement configuration, future modifications including removal and reconstruction of improvements within the access easement will be finalized during the review of the improvement plans for Phase 2 Roadway Improvements and shall be as acceptable to the City Engineer.
- s) All roadway improvements described in these Conditions of Approval must be designed and constructed by the Developer to meet the applicable requirements of the latest edition of the California Department of Transportation Highway Design Manual (HDM) and the California Manual of Uniform Traffic Control Devices (MUTCD), all applicable City Regulations, and these Conditions of Approval, prior to final inspection of the first building to be constructed within the Property.

C.3.3 Traffic Control Plan - Prior to starting the work for the Project's permanent sewer connection on Grant Line Road (gravity sewer line from the Property to the sewer manhole on the north side of Grant Line Road), Phase 1 Roadway Improvements, Phase 2 Roadway Improvements, the Project's permanent storm drainage connection, and any work within City's right-of-way on Grant Line Road, Lammers Road, and Byron Road, the Developer shall submit a Traffic Control Plan, to show the method and type of construction signs to be used for regulating traffic at the work areas within these streets. The Traffic Control Plan shall be prepared by a Civil Engineer or Traffic Engineer licensed to practice in the State of California.

C.3.4 Joint Utility Trench Plans - All existing overhead utilities along the frontage of the Project on Grant Line Road shall be converted into an underground facility. Developer shall prepare joint trench plans in compliance with utility companies' requirements and City regulations, and obtain approval of the plans. All private utility services to serve Project such as electric, telephone and cable TV to the

building must be installed underground, and to be installed at the location approved by the respective owner(s) of the utilities.

The Developer shall submit Joint Utility Trench Plans for the installation of electric, gas, telephone and TV cable main and service lines that are necessary to be installed to serve the Project. These utilities shall be installed within the 10-foot wide Public Utility Easement (PUE) that will be offered for dedication to the City. The Developer shall coordinate, as feasible, with the respective owner(s) of the utilities for the design of these underground utilities to ensure they can be installed within the 10-foot wide PUE to the extent feasible (and except in the event, that additional space beyond the 10-foot PUE is required, as determined by the utilities owner(s)).

Pavement cuts or utility trench(s) on existing street(s) for the installation of water distribution main, storm drain, sewer line, electric, gas, cable TV, and telephone will require the application of 2" asphalt concrete overlay and replacement of pavement striping and marking that are disturbed during construction. The limits of asphalt concrete overlay shall be 25 feet from both sides of the trench, and shall extend over the entire width of the adjacent travel lane(s) if pavement excavation encroaches to the adjacent travel lane or up to the street centerline or the median curb. If the utility trench extends beyond the street centerline, the asphalt concrete overlay shall be applied over the entire width of the street (to the lip of gutter or edge of pavement, whichever applies).

- C.3.5 Irrigation and Landscaping Plans - All parkway and median landscaping improvements along the frontage of the Property on Grant Line Road shall be designed and constructed in accordance with City Regulations and completed as part of the Phase 1 Roadway Improvements or Phase 2 Roadway Improvements. Design and construction details of these improvements shall be included in the Irrigation and Landscaping Plans.
- C.4. Building Permit - No building permit will be approved by the City until the Developer demonstrates, to the satisfaction of the City Engineer, compliance with all required Conditions of Approval, including, but not limited to, the following:
- C.4.1. Payment of the Master Plan Fees for Citywide Roadway and Traffic, Water, Recycled Water, Wastewater, Storm Drainage, Public Safety, Public Facilities, and Park adopted by the City Council on January 7, 2014, per Resolution 2014-010, as required by these Conditions of Approval.
 - C.4.2. Payment of the San Joaquin County Facilities Fees as required in Chapter 13.24 of the TMC, and these Conditions of Approval.
 - C.4.3. Payment of the Agricultural Conversion or Mitigation Fee as required in Chapter 13.28 of the TMC, Mitigation Measure 4.4-1 of the Filios/Dobler EIR, and these Conditions of Approval.
 - C.4.4. Payment of the Regional Transportation Impact Fees (RTIF) as required in Chapter 13.32 of the TMC, and these Conditions of Approval.
- C.5. Acceptance of Public Improvements - Public improvements will not be accepted by the City Council until after the Developer completes construction of the relevant public improvements, and also demonstrates to the City Engineer satisfactory completion of the following:
- C.5.1. Correction of all items listed in the deficiency report prepared by the assigned Engineering Inspector relating to public improvements subject to City Council's acceptance.

- C.5.2. Certified "As-Built" Improvement Plans (or Record Drawings). Upon completion of the construction by the Developer, the City shall temporarily release the originals of the Improvement Plans to the Developer so that the Developer will be able to document revisions to show the "As Built" configuration of all improvements.
- C.5.3. Signed and notarized Grant Deed(s) with legal description(s) and plat map(s) for the offer of dedication of right-of-way for portion of Grant Line Road, as required in Condition C.3.2(k), above.
- C.5.4. Signed and notarized Grant of Easement with legal description and plat map, for the offer of dedication of access easement for the use, repair, and maintenance of traffic detecting loops at the eastern main entrance, as required in Condition C.3.2(o), above.
- C.5.5. Reasonable written permission from irrigation district or affected owner(s), if applicable, as required in Condition C.10.4, below. The cost of relocating and/or removing irrigation facilities and/or tile drains is the sole responsibility of the Developer.
- C.6. Temporary or Final Building Certificate of Occupancy - No Temporary or Final Building Certificate of Occupancy will be issued by the City until after the Developer provides reasonable documentation which demonstrates, to the satisfaction of the City Engineer, that:
 - C.6.1. The Developer has satisfied all the requirements set forth in Condition C.5, above.
 - C.6.2. The Developer has completed construction of all required public facilities for the building for which a certificate of occupancy is requested and all the improvements required in these Conditions of Approval. Unless specifically provided in these Conditions of Approval, or some other applicable City Regulations, the Developer shall use diligent and good faith efforts in taking all actions necessary to construct all public facilities required to serve the Project, and the Developer shall bear all costs related to construction of the public facilities (including all costs of design, construction, construction management, plan check, inspection, land acquisition, program implementation, and contingency).
- C.7. Improvement Security – The Developer shall provide improvement security for all public facilities, as required by the OIA, DIA, and these Conditions of Approval. The form of the improvement security may be a surety bond, letter of credit or other form in accordance with section 12.36.080 of the TMC. The amount of improvement security shall be as follows:
 - C.7.1. Faithful Performance (100% of the estimated cost of constructing the public facilities),
 - C.7.2. Labor & Materials (100% of the estimated cost of constructing the public facilities), and
 - C.7.3. Warranty (10% of the estimated cost of constructing the public facilities)
- C.8. Release of Improvement Security - Improvement Security(s) described herein shall be released to the Developer after City Council's acceptance of public improvements, and after the Developer demonstrates, to the satisfaction of the City Engineer, compliance of these Conditions of Approval, and completion of the following:
 - C.8.1. Improvement Security for Faithful Performance, Labor & Materials, and Warranty shall be released to the Developer in accordance with Section 12.36.080 of the TMC.

- C.8.2. Written request from the Developer and a copy of the recorded Notice of Completion.
- C.9. Benefit District – The Developer may make a written request to the City for the formation of a Benefit District, prior to the approval of improvement plans for the public facility(s) considered to be oversized that benefits other property(s) or development(s). Reimbursement request(s) will be processed in accordance with Chapter 12.60 of the TMC.
- C.10. Special Conditions
- C.10.1 All streets and utilities improvements within City's right-of-way shall be designed and constructed in accordance with City Regulations, and City's Design documents including the City's Facilities Master Plan for storm drainage, roadway, wastewater and water adopted by the City, or as otherwise specifically approved by the City.
- C.10.2 When street cuts are made for installation of utilities, the Developer is required to install 2 inches thick asphalt concrete overlay with reinforcing fabric at least 25 feet from all sides and for the entire length of the utility trench. A 2 inches deep grind on the existing asphalt concrete pavement will be required where the asphalt concrete overlay will be applied and shall be uniform thickness in order to maintain current pavement grades, cross and longitudinal slopes. If the utility trench extends beyond the median island, the limit of asphalt concrete overlay shall be up to the lip of existing gutter located along that side of the street.
- C.10.3 All existing on-site wells, if any, shall be abandoned or removed in accordance with the City and San Joaquin County requirements. The Developer shall be responsible for all costs associated with the abandonment or removal of the existing well(s) including the cost of permit(s) and inspection. The Developer shall submit a copy of written approval(s) or permit(s) obtained from San Joaquin County regarding the removal and abandonment of any existing well(s), prior to the issuance of the Grading Permit.
- C.10.4 The Developer shall abandon or remove all existing irrigation structures, channels and pipes, if any, as directed by the City after coordination with the irrigation district, if the facilities are no longer required for irrigation purposes. If irrigation facilities including tile drains, if any, are required to remain to serve existing adjacent agricultural uses, the Developer will design, coordinate and construct required modifications to the facilities to the satisfaction of the affected agency and the City. Written permission from irrigation district or affected owner(s) will be required to be submitted to the City prior to the issuance of the Grading Permit. The cost of relocating and/or removing irrigation facilities and/or tile drains is the sole responsibility of the Developer.
- C.10.5 Nothing contained herein shall be construed to permit any violation of relevant ordinances and regulations of the City of Tracy, or other public agency having jurisdiction. This Condition of Approval does not preclude the City from requiring pertinent revisions and additional requirements to the Grading Permit, Encroachment Permit, Building Permit, Improvement Plans, OIA, and DIA, if the City Engineer finds it necessary due to public health and safety reasons, and it is in the best interest of the City. The Developer shall bear all the cost for the inclusion, design, and implementations of such additions and requirements, without reimbursement or any payment from the City.

AGENDA ITEM 2 D

REQUEST

REPORT OF GENERAL PLAN CONSISTENCY FOR CAPITAL IMPROVEMENT PROGRAM PROJECTS FOR FISCAL YEAR 2014/2015 THROUGH FISCAL YEAR 2018/2019 - APPLICATION NUMBER DET14-001

DISCUSSION

Background

Government Code Section 65103(c) requires the City's planning agency to annually review its Capital Improvement Program (CIP) for its consistency with the City's General Plan. The City adopts a Capital Improvement Program, which is a comprehensive multi-year plan for the development of the City's capital facilities and improvements. The plan identifies all capital maintenance, facilities, and improvements needed within the next several years.

Analysis

The City's CIP is a list of proposed expenditures from construction, maintenance, and improvements to capital facilities including streets, buildings, infrastructure, parks, the airport, and other public facilities. The proposed CIP for fiscal year (FY) 2014/2015 through 2018/2019 are divided into the following categories:

- General Government and Public Safety Facilities
- Traffic Safety
- Streets and Highways
- Wastewater Improvements
- Water Improvements
- Drainage Improvements
- Airport and Transit Improvements
- Parks and Recreation Improvements
- Miscellaneous Projects

The following analysis provides a brief description of each CIP project category, the types of projects contained therein, and the description of consistency with the City's General Plan goals, policies and actions. A project is considered to be consistent with the General Plan if it furthers the Plan's objectives and policies and does not obstruct from their attainment.

General Government and Public Safety Facilities

This category includes new construction, maintenance and rehabilitation of City facilities, including new fire stations, a new animal shelter and painting the water tower. Upgrades and maintenance of existing facilities and equipment are also included. The projects described above are consistent with and implement the following objective found in the Public Facilities and Services Element of the General Plan.

Report of General Plan Consistency:

Objective PF-4.1 Support the needs of the community through the construction and maintenance of public buildings, such as City Hall, community centers, libraries and the public works facility.

Traffic Safety

This category involves maintenance and upgrading of the City's existing traffic signals as well as installation of new intersection improvements and traffic signal improvements to ensure adequate, safe, and efficient movement of traffic throughout the City. The projects described above are consistent with and implement the following objective found in the Circulation Element of the General Plan.

Report of General Plan Consistency:

Objective CIR-1.6 Maximize traffic safety for automobile, transit, bicycle users, and pedestrians.

Streets and Highways

The Streets and Highways category of projects encompasses the new Eleventh Street Bridge, new I-205 freeway interchanges, various roadway reconstructions, rehabilitations, extensions, and widenings, as well as sidewalk improvements. The projects described above are consistent with and implement the following goal found in the Circulation Element of the General Plan.

Report of General Plan Consistency:

Goal CIR-1 A roadway system that provides access and mobility for all Tracy's residents and businesses while maintaining the quality of life in the community.

Wastewater Improvements

Maintenance of and improvements to the City's wastewater infrastructure and treatment facility are included in this category. Typical projects in this category include upgrades, extensions, and replacements to wastewater treatment lines and equipment, expansion of the wastewater treatment plant, wastewater recycling lines, and wastewater discharge permit studies. The projects described above are consistent with and implement the following goal and objective found in the Public Facilities and Services Element of the General Plan.

Report of General Plan Consistency:

Goal PF-7 Meet all wastewater treatment demands and federal and State regulations.

Water Improvements

Water projects include the purchase of water supply, replacement of water lines throughout the City and installation of new water lines. The projects described above are consistent with and implement the following goal and objective found in the Public Facilities and Services Element of the General Plan.

Report of General Plan Consistency:

- Goal PF-6 Adequate supplies of water for all types of users.
Objective PF-6.2 Provide adequate water infrastructure facilities to meet current and future populations.

Drainage Improvements

This category of projects includes storm drain line replacements, installation of new storm drain lines, channel improvements and pump station upgrades. The projects described above are consistent with and implement the following objective found in the Public Facilities and Services Element of the General Plan.

Report of General Plan Consistency:

- Objective PF-8.2 Provide effective storm drainage facilities for development projects.

Airport and Transit Improvements

The maintenance and upgrades to the Tracy Airport, transit system improvements, and bus replacements are included in this category. Projects include runway repairs, transit and ParaTransit bus replacements, and fire protection water supply improvements. The projects described above are consistent with and implement the following goal and objective found in the Circulation Element of the General Plan.

Report of General Plan Consistency:

- Goal CIR-4 A balanced transportation system that encourages the use of public transit and high occupancy vehicles.
Objective CIR-1.6 Maximize traffic safety for automobile, transit, bicycle users, and pedestrians.
Objective ED-5.1 Support the City's Airport Master Plan

Parks and Recreation Improvements

This category consists of park and civic amenity construction, improvements and maintenance. This category includes park improvements, bikeway improvements, playground equipment replacement, aquatics center, etc. The projects described above are consistent with and implement the following goals found in the Open Space and Conservation Element and the Public Facilities and Services Element of the General Plan.

Report of General Plan Consistency:

- Goal OSC-4 Provision of parks, open space, and recreation facilities and services that maintain and improve the quality of life for residents.
Objective PF-4.2 Provide sufficient library service to meet the informational, cultural, and educational needs of the City of Tracy.

Miscellaneous Projects

Projects in this category include management of various development projects and planning documents. The projects described above are consistent with and implement the following goals found in the Community Character Element of the General Plan.

Report of General Plan Consistency:

Goal LU-1 A balanced and orderly pattern of growth in the City.
Goal ED-2 Support for and promotion of existing businesses.

Environmental Document

This report of consistency with the City's General Plan is exempt from CEQA, pursuant to CEQA Guidelines Section 15061, stating that CEQA only applies to projects which have the potential for causing a significant effect on the environment.

RECOMMENDATION

Staff recommends that the Planning Commission report that the Capital Improvement Program Projects are consistent with the goals, policies and actions of the City's General Plan.

MOTION

Move that the Planning Commission report that the Capital Improvement Program Projects are consistent with the goals, policies and actions of the City's General Plan.

Prepared by: Kimberly Matlock, Assistant Planner

Reviewed by: Bill Dean, Assistant Development Services Director
Victoria Dion, City Engineer

Approved by: Andrew Malik, Development Services Director

ATTACHMENT

Attachment A — Capital Improvement Projects List for FY 2014/2015 through FY 2018/2019

Summary by Functional Groups

FY14-15 CIP Adopted

Group # & Functional Groups	Group \$ Totals	Prior Years Expenditures	FY13-14 Appropriations	Total	NEW APPROPRIATIONS REQUIRED					Projects Requiring	
					FY14-15	FY15-16	FY16-17	FY17-18	FY18-19		
Group 71 - General Government & Public Safety Facilities											
9	Current Projects	20,232,608	6,351,291	13,786,317	95,000	0	0	0	0	0	2 New Funding
5	New Projects	387,200	0	0	387,200	0	0	0	0	0	2 In FY14-15
19	Future Projects	56,134,348	60,698	0	0	4,490,430	10,065,400	18,991,920	22,525,900	0	5 Projects Becoming Active in FY14-15
33	Totals	76,754,156	6,411,989	13,786,317	56,555,850	4,490,430	10,065,400	18,991,920	22,525,900	0	
Group 72 - Traffic Safety											
13	Current Projects	7,673,202	591,198	5,315,904	1,766,100	1,469,100	0	297,000	0	0	4 New Funding
3	New Projects	1,099,000	0	0	1,099,000	559,000	0	0	540,000	0	3 In FY14-15
47	Future Projects	86,613,912	689,067	0	85,924,845	0	6,978,145	11,856,200	31,744,000	35,346,500	3 Projects Becoming Active in FY14-15
63	Totals	95,386,114	1,280,265	5,315,904	88,789,945	2,028,100	6,978,145	12,153,200	32,284,000	35,346,500	
Group 73 - Streets & Highways											
21	Current Projects	230,343,965	23,166,069	27,345,496	179,832,400	40,730,500	13,360,900	11,492,000	18,510,000	95,739,000	11 New Funding
2	New Projects	1,656,710	0	0	1,656,710	928,910	0	0	727,800	0	3 In FY14-15
52	Future Projects	231,968,422	4,162,382	0	227,796,040	0	38,688,980	33,759,900	63,634,560	91,712,600	2 Projects Becoming Active in FY14-15
75	Totals	463,959,097	27,328,451	27,345,496	409,285,150	41,659,410	52,049,880	45,251,900	82,872,360	187,451,600	
Group 74 - Wastewater Improvements											
17	Current Projects	72,653,906	9,516,657	10,237,749	52,899,500	20,895,000	26,304,500	4,700,000	1,000,000	0	7 New Funding
5	New Projects	7,173,800	0	0	7,173,800	3,306,800	3,867,000	0	0	0	3 In FY14-15
15	Future Projects	180,396,650	59,920	0	180,336,730	0	2,977,580	3,572,000	21,022,930	152,764,220	5 Projects Becoming Active in FY14-15
37	Totals	260,224,356	9,576,577	10,237,749	240,410,030	24,201,800	33,149,080	8,272,000	22,022,930	152,764,220	
				F5							

Summary by Functional Groups FY14-15 CIP Adopted

Group # & Functional Groups	Group Totals	Prior Years Expenditures	FY13-14 Appropriations	Total	NEW APPROPRIATIONS REQUIRED					Projects Requiring New Funding in FY14-15	
					FY14-15 Capital Budget	FY15-16	FY16-17	FY17-18	FY18-19		
Group 75 - Water Improvements											
15	Current Projects	37,256,059	13,459,567	16,550,962	7,245,530	400,000	6,020,530	275,000	275,000	275,000	7 New Funding in FY14-15
3	New Projects	1,020,000	0	0	1,020,000	1,020,000	0	0	0	0	3 Projects Becoming Active in FY14-15
13	Future Projects	39,371,700	0	0	39,371,700	0	1,812,400	2,880,000	27,520,200	7,159,100	
31	Totals	77,647,759	13,459,567	16,550,962	47,637,230	1,420,000	7,832,930	3,155,000	27,795,200	7,434,100	
Group 76 - Drainage Improvements											
5	Current Projects	4,224,861	77,070	4,147,791	0	0	0	0	0	0	0 New Funding in FY14-15
3	New Projects	483,100	0	0	483,100	483,100	0	0	0	0	3 Projects Becoming Active in FY14-15
20	Future Projects	44,896,037	5,780,937	0	39,115,100	0	6,000,300	10,486,300	13,473,700	9,154,800	
28	Totals	49,603,998	5,858,007	4,147,791	39,598,200	483,100	6,000,300	10,486,300	13,473,700	9,154,800	
Group 77 - Airport & Transit Improvements											
13	Current Projects	12,350,216	2,222,333	7,565,883	2,562,000	0	2,562,000	0	0	0	2 New Funding in FY14-15
1	New Projects	5,603,990	0	0	5,603,990	100,000	3,482,000	2,021,990	0	0	1 Projects Becoming Active in FY14-15
26	Future Projects	57,202,600	0	0	57,202,600	0	4,702,000	1,470,000	0	51,030,600	
40	Totals	75,156,806	2,222,333	7,565,883	65,368,590	100,000	10,746,000	3,491,990	0	51,030,600	
Group 78 - Parks & Recreation Improvements											
15	Current Projects	31,560,796	15,833,114	16,661,682	-934,000	-1,909,000	975,000	0	0	0	0 New Funding in FY14-15
5	New Projects	4,744,600	0	0	4,744,600	4,744,600	0	0	0	0	5 Projects Becoming Active in FY14-15
28	Future Projects	77,535,722	162,352	0	77,373,370	0	8,056,600	15,046,600	35,729,670	18,540,500	
48	Totals	113,841,118	15,995,466	16,661,682	81,183,970	2,835,600	9,031,600	15,046,600	35,729,670	18,540,500	

Summary by Functional Groups

FY14-15 CIP Adopted

Group # & Functional Groups	Group \$ Totals	Prior Years		Total	NEW APPROPRIATIONS REQUIRED					Projects Requiring New Funding 1 In FY14-15 2 Projects Becoming Active in FY14-15
		Expenditures	FY13-14 Appropriations		FY14-15	FY15-16	FY16-17	FY17-18	FY18-19	
Group 79 - Miscellaneous Projects										
30 Current Projects	34,275,339	20,524,700	5,173,381	8,577,258	670,000	847,500	749,229	635,000	5,675,529	9 New Funding 1 In FY14-15
2 New Projects	850,000	0	0	850,000	850,000	0	0	0	0	2 Projects Becoming Active in FY14-15
4 Future Projects	2,440,000	0	0	2,440,000	0	910,000	510,000	510,000	510,000	
36 Totals	37,565,339	20,524,700	5,173,381	11,867,258	1,520,000	1,757,500	1,259,229	1,145,000	6,185,529	
TOTALS - All Groups										
138 Current Projects	450,570,962	91,741,999	106,785,165	252,043,788	62,360,600	50,070,430	17,513,229	20,420,000	101,689,529	42 New Funding 15 In FY14-15
29 New Projects	23,018,400	0	0	23,018,400	12,379,610	7,349,000	2,021,990	1,267,800	0	29 Projects Becoming Active in FY14-15
224 Future Projects	776,549,391	10,915,356	0	765,634,035	0	74,616,435	89,646,400	212,626,980	388,744,220	
391 Totals	1,250,138,743	102,657,355	106,785,165	1,040,696,223	74,730,210	132,035,865	109,181,619	234,314,780	490,433,749	

CIP Expenditures
 in FY12-13 28,622,805
 in FY11-12 32,182,933
 in FY10-11 31,818,634
 in FY09-10 30,636,673

New Appropriations
 Carryovers from FY13
 Revisions & Deferrals
 Supplementals

CIP Forecast Estimates

Estimated Expenditures in FY14 44,109,825
 Estimated Lapsed Appropriations from FY14 2,996,500
 Estimated Carryovers from FY14 into FY15 59,678,840

134,409,050 Amended Capital Budget
 with Carryovers

Summary by Funding Sources

FY14-15 CIP Adopted

by Funding Sources	Funds	Prior Years		Total	NEW APPROPRIATIONS REQUIRED					
		Expenditures	FY13-14 Appropriations		FY14-15	FY15-16	FY16-17	FY17-18	FY18-19	
General Fund	F101-General	1,077,877	100,000	560,000	0	560,000	0	0	0	0
Special Revenue Funds	F241-Transp Devel Tax	0	730,000	700,000	700,000	0	0	0	0	0
	F242-Transp Sales Tax	3,920,926	2,508,317	12,603,720	1,772,320	2,307,000	4,435,400	1,450,000	2,639,000	0
	F245-Gas Tax	1,664,789	2,661,111	21,200,700	408,700	11,128,000	2,560,500	7,238,000	-132,500	0
	F26x-Comm Dev Block Gt	225,336	137,255	411,000	211,000	50,000	50,000	50,000	50,000	0
	F271-Landscaping District	26,989	211,011	832,000	832,000	0	0	0	0	0
	Sub-Total	5,838,040	6,247,694	35,747,420	3,924,020	13,483,000	7,045,900	8,738,000	2,556,500	0
Capital Project Funds	F301-General Projects	13,667,609	14,109,600	76,470,110	2,489,800	9,953,100	10,616,450	22,158,460	31,252,300	0
	F311-Infill Parks	0	0	5,303,000	0	0	527,000	0	4,776,000	0
	F312-Infill Storm Drainage	21,005	0	6,608,400	0	184,000	6,069,600	293,800	61,000	0
	F313-Infill Arterials	150,645	-75,645	25,547,050	84,000	2,386,250	1,017,400	4,066,000	17,993,400	0
	F314-Infill Bldgs & Eqpt	0	714,600	739,400	0	10,400	120,000	0	609,000	0
	F318-CDA Agency-Projec	5,583,612	171,798	0	0	0	0	0	0	0
	F321-Plant C* - Parks	1,648,000	0	0	0	0	0	0	0	0
	F322-Plant C* - Drainage	5,540,222	621,600	1,566,700	0	415,600	1,040,900	0	109,200	0
	F323-Plant C* - Arterials	7,490,436	1,975,564	1,366,700	472,900	0	892,800	0	0	0
	F324-Plant C* - Gen Bldgs	1,949,662	1,663,121	2,900,530	0	1,111,330	860,200	0	929,000	0
	F325-Plant C* - Utilities	6,619,058	983,086	-1,053,060	405,000	-683,860	-774,200	0	0	0
	F345-RSP Prgm Mgmt	-10,997,178	4,811,602	784,670	193,000	-83,030	-795,300	380,000	1,090,200	0
	F351-NE Indus Area #1	18,792,548	12,079,937	9,734,794	340,100	8,224,300	1,170,394	0	0	0
	F352-South MacArthur Ar	1,592,957	641,547	4,857,435	0	349,600	715,935	2,772,000	1,019,900	0
	F353-I205 Area Spec Plar	7,224,541	2,346,136	15,634,000	0	5,987,000	7,628,700	493,800	1,524,500	0
	F354-Indus SP, South	2,625,853	1,608,662	25,903,190	68,000	8,833,590	12,316,000	2,721,400	1,964,200	0
	F355-Presidio Area	1,513,439	201,657	3,200,900	0	5,800	937,200	1,334,700	923,200	0
	F356-Tracy Gateway Area	2,139,852	544,760	67,469,200	1,603,800	30,143,500	2,517,500	20,302,100	12,902,300	0
	F357-NE Indus Area #2	12,772,236	3,050,294	36,436,765	633,000	4,801,865	3,498,100	22,124,700	5,378,100	0
	F3xx-Ellis Prgm Area	0	0	72,986,140	0	200,000	248,000	70,393,720	2,143,420	0
	F381-Comm Dev Ag Project	0	0	0	0	0	0	0	0	0
	F387-RSP Reserves	0	0	0	0	0	0	0	0	0
	F391-UMMP Facilities	10,285,867	829,978	1,443,059	800,000	247,000	100,000	100,000	196,059	0
	Sub-Total	88,620,354	46,278,277	357,896,183	7,089,600	72,086,445	48,706,679	147,140,680	82,871,779	0

(Continued)

Summary by Funding Sources (Continued)

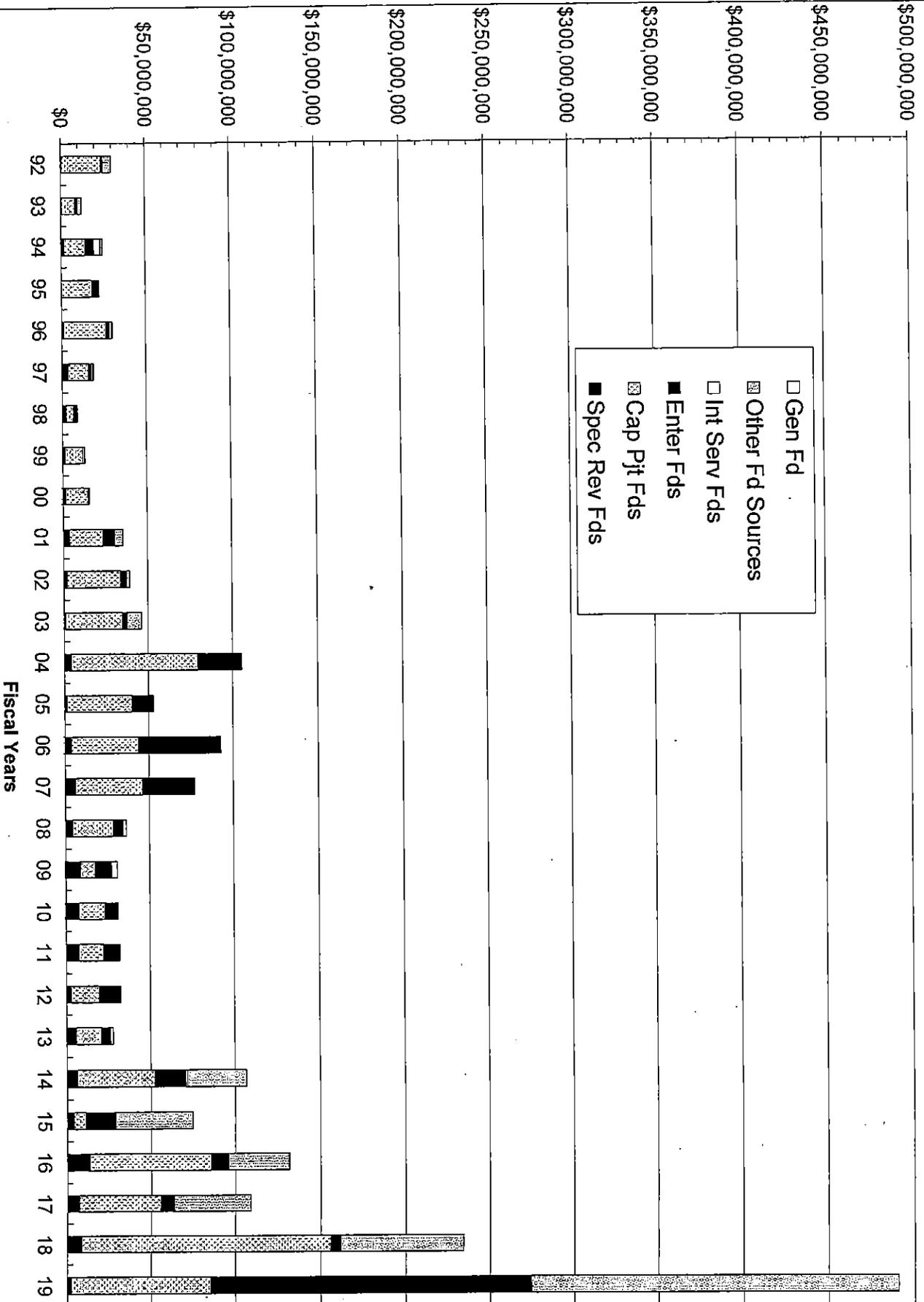
FY14-15 CIP Adopted

by Funding Sources	Funds	Prior Years		NEW APPROPRIATIONS REQUIRED					
		Expenditures	FY13-14 Appropriations	Total	FY14-15	FY15-16	FY16-17	FY17-18	FY18-19
Enterprise Funds	F513-Water	-2,145,480	8,766,538	7,720,810	1,420,000	-315,490	3,155,000	2,586,300	875,000
	Bond Issue	0	0	0	0	0	0	0	0
	State Loan or Grant	0	0	0	0	0	0	0	0
	F523-Wastewater	290,034	2,249,478	156,263,900	1,560,000	-309,920	1,721,700	2,939,500	150,352,620
	Bond Issue	0	0	14,000,000	14,000,000	0	0	0	0
	State Loan or Grant	0	0	0	0	0	0	0	0
	F541-Drainage Enterprise	14,924	228,101	1,364,000	143,000	753,000	382,000	43,000	43,000
	F563-Airport	152,666	-2,630	3,796,400	0	178,000	170,000	0	3,447,400
	FAA Grant	118,779	4,781,661	41,031,740	100,000	6,503,800	1,045,440	0	33,382,500
	State Loan or Grant	0	0	2,842,300	0	2,131,000	0	0	711,300
	F573-Transit	332,842	227,158	540,000	0	280,000	260,000	0	20,000
	Federal Grant	1,523,854	1,507,346	2,080,000	0	1,040,000	1,040,000	0	0
	State Loan or Grant	51,369	391,571	0	0	0	0	0	0
	Sub-Total	336,988	18,149,223	229,638,150	17,223,000	10,240,390	7,774,140	5,568,800	188,831,820
	Internal Service Funds	F601-Central Garage	0	0	0	0	0	0	0
F602-Central Services	0	0	0	0	0	0	0	0	
F605-Eqpt Acq	0	1,021,582	1,340,000	500,000	210,000	210,000	210,000	210,000	
Sub-Total	0	1,021,582	1,340,000	500,000	210,000	210,000	210,000	210,000	
Other Sources	Developers Contribution	7,355,628	16,170,937	66,237,190	6,000,000	1,174,540	4,320,300	16,373,500	38,368,850
Tracy Rural Fire District	0	925,000	0	0	0	0	0	0	0
Federal TEA Grants	353,702	10,035,780	61,290,290	39,792,490	3,646,500	10,106,300	7,745,000	0	
Other Federal Grants	314,619	0	1,000,000	0	1,000,000	0	0	0	
State & Local Grants	4,114,497	306,672	8,216,390	201,100	1,101,790	6,463,500	450,000	0	
Future Developments	0	7,550,000	278,771,600	0	29,533,200	23,554,800	48,088,800	177,594,800	
F834-AD 84-1 Debt	-3,418,900	0	0	0	0	0	0	0	
F835-CFD89-1 Debt	-1,937,350	0	0	0	0	0	0	0	
Sub-Total	6,782,096	34,988,389	415,515,470	45,993,590	35,456,030	45,444,900	72,657,300	215,963,650	
CIP Totals	102,667,356	106,785,165	1,040,696,223	74,730,210	132,035,865	109,181,619	234,314,780	450,433,749	

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Actual & Projected Expenditures

Tracy Capital Improvement Program by Funding Sources FY92 to FY19



Group 71 - General Government & Public Safety Facilities

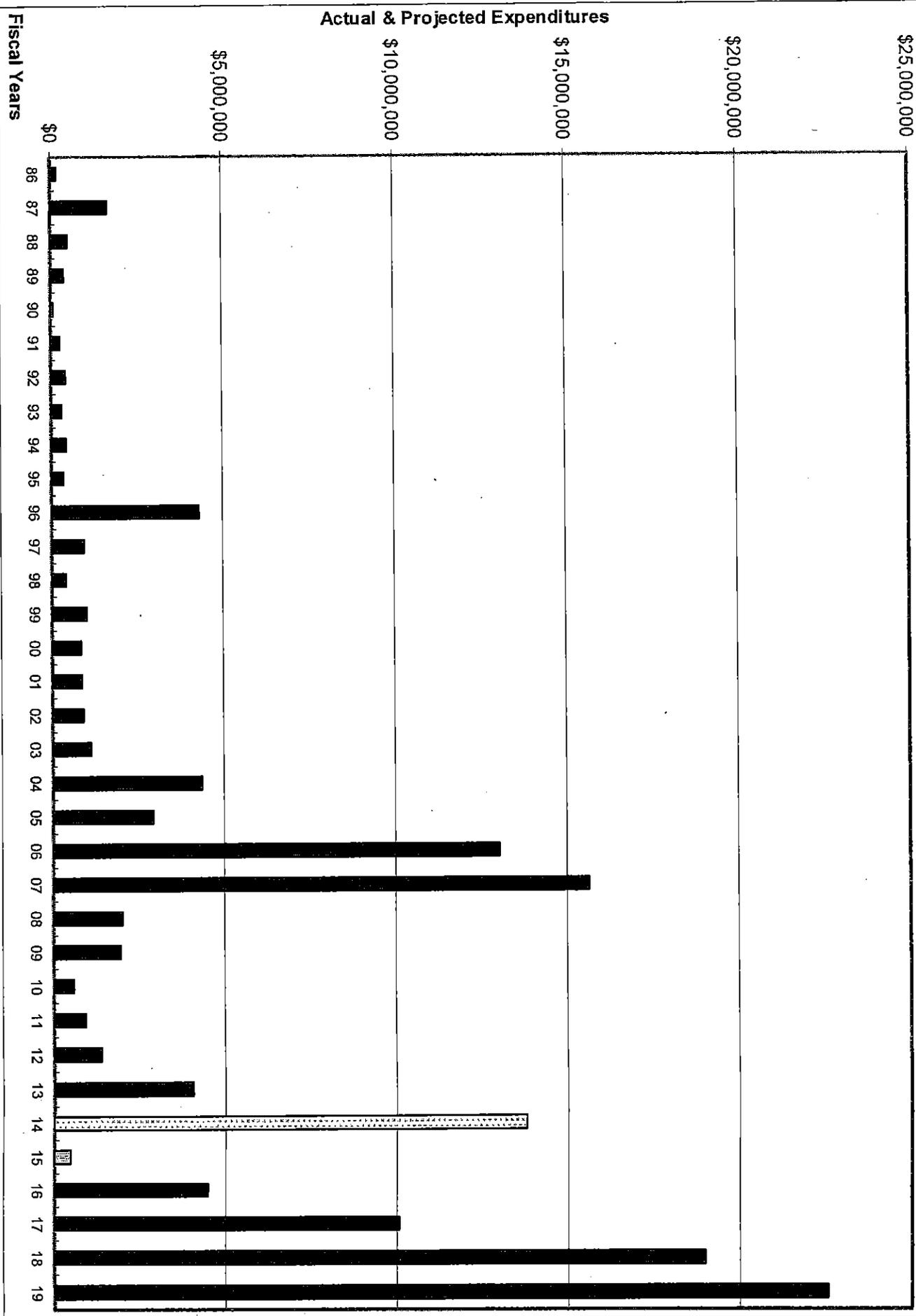
FY14-15 CIP Adopted

by Project Type	Group \$ Total	Prior Years Expenditures	FY13-14 Appropriations	Total	NEW APPROPRIATIONS REQUIRED					Projects Requiring New Funding 2 in FY14-15
					FY14-15	FY15-16	FY16-17	FY17-18	FY18-19	
9 Current Projects	20,232,608	6,351,291	13,786,317	95,000	0	0	0	0	0	2
5 New Projects	387,200	0	0	387,200	387,200	0	0	0	0	2
19 Future Projects	56,134,348	60,698	0	56,073,650	0	4,490,430	10,065,400	18,991,920	22,525,900	0
33 Totals	76,754,156	6,411,989	13,786,317	56,555,850	482,200	4,490,430	10,065,400	18,991,920	22,525,900	
by Funding Sources										
F101-General	560,000	0	0	560,000	0	560,000	0	0	0	0
F26X-Com Dev Block G	345,200	0	0	345,200	145,200	50,000	50,000	50,000	50,000	0
F301-General Projects	26,531,671	3,724,147	8,760,264	14,047,260	262,000	620,000	5,441,400	3,705,460	4,018,400	0
F314-Hill Bldgs & Eqpt	1,454,000	0	714,600	739,400	0	10,400	120,000	0	609,000	0
F324-Gen Fac - Plan "C	2,497,103	856,117	656	1,640,330	0	711,330	0	0	929,000	0
F345-RSP Area	2,373,200	0	828,000	1,545,200	75,000	0	0	380,000	1,090,200	0
F351-NE Indus Area #1	384,220	62,220	322,000	0	0	0	0	0	0	0
F352-So MacArthur Ave	452,300	143,000	0	309,300	0	93,300	0	0	216,000	0
F353-1205 Area Spec Pl	2,457,400	1,094,385	1,363,015	0	0	0	0	0	0	0
F354-HSP South Area	1,649,100	334,600	0	1,314,500	0	151,200	0	0	1,163,300	0
F355-Prestdio Area	102,700	96,900	0	5,800	0	5,800	0	0	0	0
F356-Tracy Gateway Ar	583,900	0	0	583,900	0	15,400	70,000	498,500	0	0
F357-NE Indus Area #2	331,200	60,000	271,200	0	0	0	0	0	0	0
F3XX-Ellis Prgm Area	4,219,660	0	0	4,219,660	0	0	48,000	4,171,660	0	0
F245-Gas Tax	40,620	40,620	0	0	0	0	0	0	0	0
F5X3-Water & Wastewa	1,961,300	0	0	1,961,300	0	0	0	1,961,300	0	0
F605-Equipment Acquis	601,582	0	601,582	0	0	0	0	0	0	0
Tracy Rural Fire District	925,000	0	925,000	0	0	0	0	0	0	0
Future Developments	29,284,000	0	0	29,284,000	0	2,273,000	4,336,000	8,225,000	14,450,000	0
	76,754,156	6,411,989	13,786,317	56,555,850	482,200	4,490,430	10,065,400	18,991,920	22,525,900	

CIP Expenditures

In FY12-13 >>	4,063,125	937,300	New Appropriations
In FY11-12 >>	1,396,832	13,526,557	Carryovers from FY13
In FY10-11 >>	936,940	-1,350,000	Deferrals
In FY09-10 >>	596,578	672,460	Supplementals

Tracy Capital Improvement Program
Gen Govt & Pub Safety Facilities



Group 71 - General Government & Public Safety Facilities

FY14-15 CIP Adopted

Project #	Project Title	Project \$ Total	Funding Sources	Prior Years Expenditures	FY13-14 Appropriations	Total	NEW APPROPRIATIONS REQUIRED					Anticipated Completion & Comments
							FY14-15	FY15-16	FY16-17	FY17-18	FY18-19	
CURRENT PROJECTS												
71033	Monitoring UG Tanks - 6 locations	447,295	F301-General Projects F245-Gas Tax	276,445 40,620 0	55,230 0 0	0 0 75,000	0 0 75,000	0 0 0	0 0 0	0 0 0	0 0 0	0 Jun 15 Priority B Monitoring Underway 3 Sites Completed
71054	Expansion Pub Works Facility - Phase I	2,079,720	F301-General Projects F324-Gen Fac - Plan "C" F351-NE Indus Area #1 F352-SO MacArthur AF F354-ISP South Area F355-Presidio Area F357-NE Indus Area #2	504,284 832,344 62,220 143,000 334,600 96,900 60,000	45,716 656 0 0 0 0 0	0 0 0 0 0 0 0	0 0 0 0 0 0 0	0 0 0 0 0 0 0	0 0 0 0 0 0 0	0 0 0 0 0 0 0	0 0 0 0 0 0 0	Jul 13 Work Completed See 71PP-072 for future phase II
71061	New Fire Station - Relocate Station #96 - West Grant Line Rd	4,000,000	F353-1205 Area Spec P F314-Infill Buildings F345-RSP Area	1,094,385 0 0	1,363,015 714,600 828,000	0 0 0	0 0 0	0 0 0	0 0 0	0 0 0	0 0 0	Apr 14 Contract Award Nov 12
71062	New Fire Station - Relocate #92 - Bankla East Grant Line Rd	4,155,000	F301-General Projects F351-NE Indus Area #1 F357-NE Indus Area #2 Tracy Rural Fire District	1,371,057 0 0 0	1,265,743 322,000 271,200 925,000	0 0 0 0	0 0 0 0	0 0 0 0	0 0 0 0	0 0 0 0	0 0 0 0	Apr 14 Contract Award Nov 12
71063	Police CAD/RMS Replacement	3,376,583	F301-General Projects F605-Eqpt Acquisition	1,201,906 0	1,673,095 501,582	0 0	0 0	0 0	0 0	0 0	0 0	Feb 14 Work Underway
71064	New Animal Shelter Phase I	5,395,460	F301-General Projects Grant Funding	199,520 0	5,195,940 0	0 0	0 0	0 0	0 0	0 0	0 0	Dec 14 Contract Award Apr 14
71071	Fuel Dispenser Replacement - Boyd	100,000	F605-Equipment Acquis	0	100,000	0	0	0	0	0	0	Feb 14 Work Completed
71072	Firearms Training Facility - Phase I Improvements	629,250	F301-General Projects	134,010	495,240	0	0	0	0	0	0	Jun 14 Work Underway

(Continued)

Group 71 - General Government & Public Safety Facilities

FY14-15 CIP Adopted

Project #	Project Title	Project \$ Total	Funding Sources	Prior Years Expenditures	FY13-14 Appropriations	NEW APPROPRIATIONS REQUIRED					Anticipated Completion & Comments			
						Total	FY14-15	FY15-16	FY16-17	FY17-18		FY18-19		
<u>CURRENT PROJECTS (Continued)</u>														
71075	Quarters Modifications - Fire Station #91	49,300	F301-General Projects Grant Funding	0	29,300	20,000	20,000	0	0	0	0	0	Sep 14 Added funding needed	
						<u>Approved Capital Budget</u>								
						20,000	20,000	0	0	0	0	0		
Totals						20,232,608	6,351,291	13,786,317	95,000	95,000	0	0	0	

Group 71 - General Government & Public Safety Facilities

FY14-15 CIP Adopted

Project #	Project Title	Project \$ Total	Funding Sources	Prior Years Expenditures	FY13-14 Appropriations	NEW APPROPRIATIONS REQUIRED					Anticipated Completion & Comments		
						Total	FY14-15	FY15-16	FY16-17	FY17-18		FY18-19	
NEW PROJECTS													
71076	ADA Door Modifications at Support Services Bldg	134,000	F301-General Projects F26x-Corn Dev Block G	0	0	30,000	30,000	0	0	0	0	0	Dec 15 Priority B New Facility
71077	Install Automatic Doors at City Hall	53,700	F301-General Projects F26x-Corn Dev Block G	0	0	12,500	12,500	0	0	0	0	0	Dec 15 Priority B New Facility
71078	New Radio Tower - F59	74,500	F301-General Projects Future Developments	0	0	74,500	74,500	0	0	0	0	0	Jun 15 Priority C3 New Facility
71079	Remodel of old FS#96	50,000	F301-General Projects	0	0	50,000	50,000	0	0	0	0	0	Jun 15 Priority D
71080	Repainting of Civic Center Water Tower	75,000	F301-General Projects	0	0	75,000	75,000	0	0	0	0	0	Jun 15 Priority D
Totals						387,200	387,200	0	0	0	0	0	
5 New Projects						387,200	387,200	0	0	0	0	0	

Group 71 - General Government & Public Safety Facilities

FY14-15 CIP Adopted

Project #	Project Title	Project \$ Total	Funding Sources	Prior Years Expenditures	FY13-14 Appropriations	Total	NEW APPROPRIATIONS REQUIRED					Anticipated Completion & Comments		
							FY14-15	FY15-16	FY16-17	FY17-18	FY18-19			
FUTURE PROJECTS														
71027	Retrofit Water Towers - Civic Center	400,400	F301-General Projects	0	0	400,400	0	0	0	0	0	0	Jun 17 Priority C8 Deferred to FY16-17	
71035	City Hall Vehicles - New Development	97,503	F324-Gen Fac - Plan C F352-So MacArthur Ave F354-ISP South Area F355-Residic Area	23,773	0	44,730	0	44,730	0	0	0	0	0	Jan 16 New Equipment Deferred to FY15-16
71050	Public Safety Communications Tower	1,718,925	F301-General Projects	36,925	0	1,682,000	0	170,000	1,512,000	0	0	0	0	Apr 17 Deferred to FY15-17
71052	Police Radio Repeater Tower - SMPA	18,300	F352-So MacArthur Ave Future Developments	0	0	18,300	0	18,300	0	0	0	0	0	Apr 16 Deferred to FY15-16
71PP-001	ADA Compliance - City Buildings, Future Phases	236,000	F26x-Corn Dev Block G F301-General Projects	0	0	200,000	0	50,000	50,000	50,000	50,000	9,000	9,000	Phased Annual Program Rehabilitation
71PP-003	Police Technical Facility Boyd Service Center	1,214,000	F301-General Projects	0	0	1,214,000	0	120,000	1,094,000	0	0	0	0	Jun 17 Priority C Expansion
71PP-038	New Fire Station - Tracy Hills	7,850,000	F301-General Projects Future Developments	0	0	7,850,000	0	0	0	900,000	6,950,000	0	0	Jun 18 Priority D New Facility
71PP-046	Recarpeting/Repainting Police Facility	168,000	F301-General Projects	0	0	168,000	0	168,000	0	0	0	0	0	Sep 15 Priority B4 Rehabilitation
71PP-052	Public Safety Facilities New Development Areas	2,321,720	F356-Tracy Gateway A F3xx-Ellis Prgm Area	0	0	588,500	0	0	70,000	488,500	1,705,220	0	0	Jun 18 Priority C New & Expansion
71PP-053	Police SWAT Equipment Tracy Gateway Share	15,400	F356-Tracy Gateway A	0	0	15,400	0	15,400	0	0	0	0	0	Jun 16 Priority B New Equipment
71PP-055	Haz Mat & Rescue Vehicle for Fire Department	560,000	F101-General Grant Funding	0	0	560,000	0	560,000	0	0	0	0	0	Jun 16 Priority B New Equipment

(Continued)

Group 71 - General Government & Public Safety Facilities

FY14-15 CIP Adopted

Project #	Project Title	Project \$ Total	Funding Sources	Prior Years Expenditures	FY13-14 Appropriations	NEW APPROPRIATIONS REQUIRED					Anticipated Completion & Comments					
						Total	FY14-15	FY15-16	FY16-17	FY17-18		FY18-19				
FUTURE PROJECTS (Continued)																
71PP-058	New Fire Vehicle - Ladder Truck	1,000,000	F314-Infill Buildings F324-Gen Fac - Plan "C" F352-So MacArthur Ave F354-ISP South Area	0	0	130,400	0	10,400	120,000	0	0	0	Jun 17	Priority B New Equipment		
71PP-085	Remodel Police Facility	1,516,800	F301-General Projects	0	0	1,516,800	0	153,000	1,363,800	0	0	0	Jun 17	Priority C Rehabilitation		
71PP-067	New Fire Station - Southwest Tracy	4,189,000	F301-General Projects Future Developments	0	0	0	0	0	753,000	3,436,000	0	0	Dec 17	Priority C New Facility		
71PP-068	New Fire Station - Relocate Station #97 - South Tracy	4,387,500	F301-General Projects F314-Infill Buildings F344-RSP Area F324-Gen Fac - Plan "C" F352-So MacArthur Ave F354-ISP South Area	0	0	0	0	0	0	0	0	0	0	0	Jun 19	Priority D Replacement
71PP-072	Expansion Pub Works Facility - Phase II	7,845,000	F301-General Projects F513-Water Capital F3xx-Ellis Prgm Area	0	0	3,417,260	0	0	610,000	2,807,260	0	0	0	0	Dec 18	Priority D Expansion
71PP-074	Firearms Training Facility - Future Phases Improvements	2,205,800	F301-General Projects	0	0	2,205,800	0	0	452,200	639,200	1,114,400	0	0	Jun 19	Priority C Expansion	
71PP-075	Public Safety Training Facility	17,245,000	F301-General Projects Future Developments	0	0	0	0	0	0	1,520,000	0	1,275,000	14,450,000	0	Jun 19	Priority D New Facility
71PP-079	New Animal Shelter Phase II	3,145,000	F301-General Projects Grant Funding	0	0	3,145,000	0	0	0	0	0	250,000	2,895,000	0	Jun 19	Priority D Expansion
Totals						56,134,348	60,698	0	0	56,073,650	0	4,490,430	10,065,400	18,991,920	22,525,900	

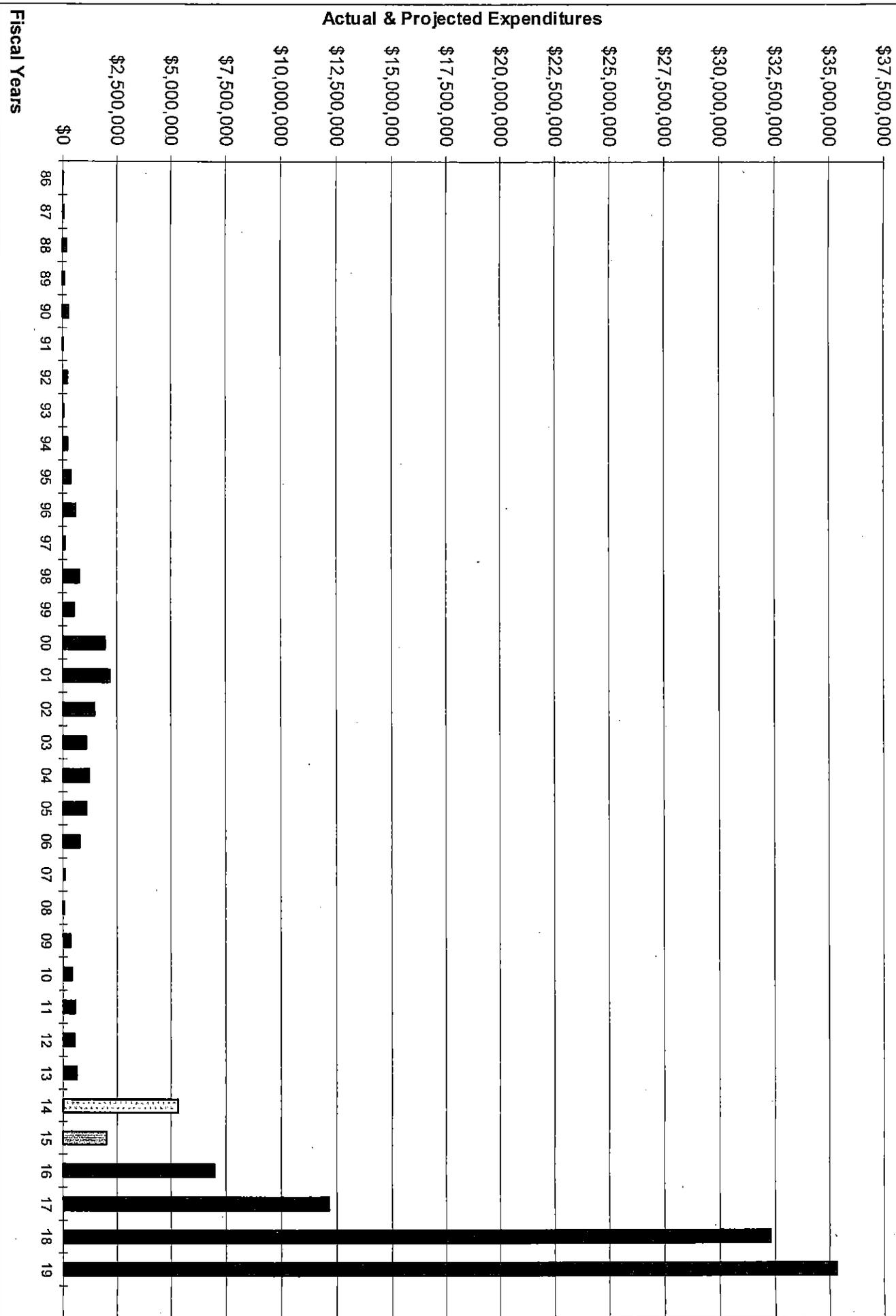
Group 72 - Traffic Safety FY14-15 CIP Adopted

by Project Type	Group \$ Total	Prior Years		Total	NEW APPROPRIATIONS REQUIRED					Projects Requiring New Funding 3 in FY14-15	
		Expenditures	FY13-14 Appropriations		FY14-15	FY15-16	FY16-17	FY17-18	FY18-19		
13 Current Projects	7,673,202	591,198	5,315,904	1,766,100	1,469,100	0	297,000	0	0	0	4 New Funding
3 New Projects	1,099,000	0	0	1,099,000	559,000	0	0	0	540,000	0	
47 Future Projects	86,613,912	689,067	0	85,924,845	0	6,978,145	11,856,200	31,744,000	35,346,500	0	
63 Totals	95,386,114	1,280,265	5,315,904	88,789,945	2,028,100	6,978,145	12,153,200	32,284,000	35,346,500	0	

by Funding Sources		Total	Approved Capital Budget	NEW APPROPRIATIONS REQUIRED					Projects Requiring New Funding 3 in FY14-15	
				FY14-15	FY15-16	FY16-17	FY17-18	FY18-19		
F301-General Projects	70,000	70,000	70,000	0	0	0	0	0	0	
F241-TDA Sales Tax	1,410,000	700,000	700,000	0	0	0	0	0	0	
F242-Transp Sales Tax	850,000	250,000	250,000	0	0	0	0	0	0	
F245-Gas Tax	5,315,562	516,605	1,139,357	3,659,600	105,000	1,745,100	1,350,500	409,000	50,000	
F313-Infill Arterials	2,839,300	0	0	2,839,300	84,000	540,000	273,900	843,200	1,098,200	
F323-Arterials Plan "C"	1,254,600	0	0	892,800	0	0	892,800	0	0	
F351-NE Indus Area #	555,000	0	0	0	0	0	0	0	0	
F352-So MacArthur Pk	1,359,000	0	0	1,359,000	0	0	50,000	1,309,000	0	
F353-1205 Area Spec F	4,781,800	100	0	4,781,700	0	686,400	2,676,000	493,800	945,500	
F354-Indus SP, South	2,333,036	10,756	0	2,220,280	68,000	1,612,280	540,000	0	0	
F355-Presidio Area	1,016,700	0	0	1,016,700	0	0	461,900	554,800	0	
F356-Tracy Gateway A	2,481,400	0	0	2,288,500	0	0	0	0	2,288,500	
F357-NE Indus Area #	14,311,000	3,035	0	13,922,565	0	1,496,965	0	12,425,600	0	
F3xx-Ellis Prgm Area	2,121,860	0	0	2,121,860	0	0	0	2,121,860	0	
Grant Funding	6,081,740	0	0	5,081,740	751,100	152,500	1,578,500	2,599,640	0	
Developer's Contributic	3,552,916	749,769	0	2,533,700	0	764,900	1,498,800	0	270,000	
Future Developments	45,052,200	0	0	45,052,200	0	0	2,830,800	11,527,100	30,694,360	
	95,386,114	1,280,265	5,315,904	88,789,945	2,028,100	6,978,145	12,153,200	32,284,000	35,346,500	

CIP Expenditures		New Appropriations	
		Carryovers from FY13	Deferrals
In FY12-13 >>	638,834	2,543,700	
In FY11-12 >>	530,814	2,942,589	
In FY10-11 >>	567,078	-239,838	
In FY09-10 >>	409,299	69,453	Supplementals

**Tracy Capital Improvement Program
Traffic Safety Projects**



Group 72 - Traffic Safety

FY14-15 CIP Adopted

Project #	Project Title	Project \$ Total	Funding Sources	Prior Years Expenditures	FY13-14 Appropriations	NEW APPROPRIATIONS REQUIRED					Anticipated Completion & Comments		
						Total	FY14-15	FY15-16	FY16-17	FY17-18		FY18-19	
CURRENT PROJECTS													
72025	Traffic Signal Grant Line & Paradise	244,000	F351-NE Indus Area #1	0	244,000	0	0	0	0	0	0	0	Nov 13 Work Completed
72056	Signal Modifications - 11th & Lammers	405,000	F356-Tracy Gateway A F245-Gas Tax	0	192,900 212,100	0	0	0	0	0	0	0	Jun 14 Priority A Upgrade
72068	Traffic Signal Lammers & West Schulte	906,940	F323-Arterials Plan "C" Developer's Contribution Grant Funding	0	361,800 289,447 0	0	0	0	0	0	0	0	Jun 15 Priority B Design Underway
72069	Intersection Improve 11th Street & Old MacArthur	4,115,862	F241-TDA Sales Tax F242-Transp Sales Tax F245-Gas Tax Grant Funding	0	710,000 400,000 620,611 1,000,000	700,000 250,000	700,000 250,000	0	0	0	0	0	Design Underway Jun 15
72072	Signal Modifications - 11th Street at East	275,000	F245-Gas Tax F242-Transp Sales Tax	7,910	67,090 200,000	0	0	0	0	0	0	0	Dec 13 Work Completed
72080	Traffic Signal Control Replacement FY12	50,000	F245-Gas Tax	1,813	48,187	0	0	0	0	0	0	0	Dec 13 Work Completed
72082	Traffic Signal Valrico & Sycamore Pkwy	420,000	F354-Indus SP, South Grant Funding	0	102,000 0	68,000 250,000	68,000 250,000	0	0	0	0	0	Jun 15 Priority B New Installation
72083	Intersection Impmts - MacArthur & Pescadero	318,000	F245-Gas Tax	1,650	19,350	297,000	0	0	297,000	0	0	0	Jun 17 Priority B Contract Award Jul 2013
72084	Fiber Optics Standards Loops - FY13	60,000	F245-Gas Tax	7,081	52,919	0	0	0	0	0	0	0	Dec 13 Work Completed
72085	Street Light Installation	132,000	F245-Gas Tax	62,547	69,453	0	0	0	0	0	0	0	Jun 14 Work Underway
72086	Traffic Calming - Various Locations - FY13 Phase	50,000	F245-Gas Tax	353	49,647	0	0	0	0	0	0	0	Jun 13 Annual Program New Installation
(Continued)						F20							

Group 72 - Traffic Safety

FY14-15 CIP Adopted

Project #	Project Title	Project \$ Total	Funding Sources	Prior Years Expenditures	FY13-14 Appropriations	NEW APPROPRIATIONS REQUIRED					Anticipated Completion & Comments	
						Total	FY14-15	FY15-16	FY16-17	FY17-18		FY18-19
CURRENT PROJECTS (Continued)												
72087	Traffic Signal - Grant Line & Chrisman	386,400	F357-NE Indus Area #2	0	385,400	0	0	0	0	0	0	Nov 13 Priority B Contract Award Apr 2013
72088	Traffic Signal Upgrade Grant Line & Chabot Court	311,000	F351-NE Indus Area #1 Developer's Contribution	0	311,000	0	0	0	0	0	0	Nov 13 Priority C Contract Award Apr 2013
Totals						1,766,100	1,469,100	0	297,000	0	0	
13	Current Projects	7,673,202		591,198	5,315,904							

Group 72 - Traffic Safety FY14-15 CIP Adopted

Project #	Project Title	Project \$ Total	Funding Sources	Prior Years Expenditures	FY13-14 Appropriations	NEW APPROPRIATIONS REQUIRED					Anticipated Completion & Comments		
						Total	FY14-15	FY15-16	FY16-17	FY17-18		FY18-19	
NEW PROJECTS													
72089	Advanced Traffic Signs Controllers - Tracy Blvd	405,000	F245-Gas Tax Grant Funding	0	0	105,000	105,000	0	0	0	0	0	Jun 15 Priority B Upgrade
72090	Traffic Signal - Tracy & Lime Road	624,000	F313-Infill Arterials	0	0	624,000	84,000	0	0	540,000	0	0	Jun 18 Priority D New Installation
72091	Downtown Up Lighting 10th Street	70,000	F301-General Projects F245-Gas Tax	0	0	70,000	70,000	0	0	0	0	0	Jun 15 Priority B Upgrade
Totals						1,099,000	559,000	0	0	540,000	0	0	
3 New Projects						1,099,000	559,000	0	0	540,000	0	0	

Group 72 - Traffic Safety

FY14-15 CIP Adopted

Project #	Project Title	Project \$ Total	Funding Sources	Prior Years Expenditures	FY13-14 Appropriations	NEW APPROPRIATIONS REQUIRED					Anticipated Completion & Comments		
						Total	FY14-15	FY15-16	FY16-17	FY17-18		FY18-19	
<u>FUTURE PROJECTS (Continued)</u>													
72PP- 028	Traffic Signal Grant Line Road & Street "A"	356,500	F353-1205 Area Spec P	0	0	356,500	0	0	0	0	0	Jun 16	Priority B New Installation
72PP- 029	Traffic Signal Naglee Road & Auto Plaza Drive	350,000	F353-1205 Area Spec P F245-Gas Tax	0	0	309,900	0	40,100	0	0	0	Jun 16	Priority B New Installation
72PP- 030	Intersection Improvement 1580 & Corral Hollow	4,376,000	F352-SO MacArthur PA F355-Presidio Area Future Developments	0	0	278,000	0	0	50,000	228,000	0	Jun 18	Priority D Expansion
72PP- 033	Traffic Signals -2.6 Collectors	959,000	F351-NE Indus Area #1 Future Developments	0	0	0	0	0	0	959,000	0	Jun 18	Priority D New Installation
72PP- 036	Traffic Centerline Strippping - various locations	60,000	F245-Gas Tax	0	0	60,000	0	0	0	0	0	Jun 16	Priority B New Installation
72PP- 042	Traffic Signal - MacArthur & Glenbrook	337,500	Developer's Contribution Grant Funding	0	0	185,000	0	152,500	0	0	0	Jun 16	Priority B New Installation
72PP- 053	Traffic Signal - Corral Hollow & Valpico	540,000	F354-Indus SP, South	0	0	540,000	0	0	540,000	0	0	Jun 17	Priority B New Installation
72PP- 054	Traffic Signal - Corral Hollow & Linne	540,000	F354-Indus SP, South	0	0	540,000	0	0	0	0	0	Jun 16	Priority B New Installation
72PP- 064	Intersection Imprints - various locations	303,200	F245-Gas Tax F313-Infill Arterials	0	0	0	0	0	0	303,200	0	Jun 18	Priority D Replacement
72PP- 071	Left Turn Traffic Signal Chrisman & Kellogs Entrance	270,000	F351-NE Indus Area #1 Developer's Contribution	0	0	0	0	0	0	0	270,000	Jun 19	Priority D New Installation
72PP- 074	Lighted Crosswalk/Flare Lowell Ave, west of Tracy	126,000	F245-Gas Tax	0	0	126,000	0	63,000	0	63,000	0	Jun 18	Biannual Program New Installation
(Continued)						F24							

Group 72 - Traffic Safety

FY14-15 CIP Adopted

Project #	Project Title	Project \$ Total	Funding Sources	Prior Years Expenditures	FY13-14 Appropriations	Total	NEW APPROPRIATIONS REQUIRED					Anticipated Completion & Comments				
							FY14-15	FY15-16	FY16-17	FY17-18	FY18-19					
FUTURE PROJECTS (Continued)																
72PP- 075	Red Light Flasher - Line & Tracy Road	240,000	F245-Gas Tax	0	0	240,000	0	240,000	0	0	0	0	0	Jun 16	Priority B New Installation	
72PP- 076	Traffic Signal Coordina Schulte Road & MacArthur Drive	240,000	F245-Gas Tax	0	0	240,000	0	120,000	0	0	120,000	0	0	Jun 18	Priority A Study & Plans	
72PP- 079	Traffic Calming - Various Locations - Future Years	100,000	F245-Gas Tax	0	0	100,000	0	50,000	50,000	0	0	0	0	Jun 17	Annual Program New Installation	
72PP- 081	Intersection Imprints - Grant Line & Lammers Roads	3,839,700	F353-1205 Area Spec P Future Developments	0	0	50,200 3,789,500	0	0	0	0	0	50,200 3,789,500	0	0	Jun 19	Priority D Upgrade
72PP- 082	Intersection Imprints - Grant Line & Naglee Roads	271,500	F353-1205 Area Spec P Future Developments	0	0	20,800 250,700	0	0	0	0	0	20,800 250,700	0	0	Jun 19	Priority D Upgrade
72PP- 083	Intersection Imprints - Naglee Road & Pavilion Pkwy	807,400	F353-1205 Area Spec P Future Developments	0	0	345,300 462,100	0	0	345,300	462,100	0	0	0	0	Jun 17	Priority C Upgrade
72PP- 084	Intersection Imprints - Grant Line & 1205 Ramps	2,538,500	F353-1205 Area Spec P Future Developments	0	0	493,800 2,044,700	0	0	0	493,800	2,044,700	0	0	0	Jun 18	Priority D Upgrade
72PP- 085	Intersection Imprints - Grant Line & Corral Hollow Rds	4,639,200	F353-1205 Area Spec P F323-Arterials Plan "C" Future Developments	0	0	2,069,400 319,200 2,250,600	0	0	2,069,400	319,200	0	0	0	0	Jun 17	Priority C Upgrade
72PP- 086	Intersection Imprints - Lammers & 11th Street	24,001,200	F353-1205 Area Spec P Future Developments	0	0	864,700 23,136,500	0	0	0	0	0	864,700	23,136,500	0	Jun 19	Priority D Upgrade
72PP- 087	Intersection Imprints - 11th Street & Corral Hollow Road	975,500	F353-1205 Area Spec P Future Developments	0	0	9,800 965,700	0	0	0	0	0	9,800	965,700	0	Jun 19	Priority D Upgrade
72PP- 090	Traffic Signal/Intersect Imprints - Chrisman Rd, south of Parac Developer's Contribution	385,000	F245-Gas Tax	0	0	0 385,000	0	0	0	0	385,000	0	0	0	Jun 17	Priority B New Installation

(Continued)

Group 72 - Traffic Safety

FY14-15 CIP Adopted

Project #	Project Title	Project \$ Total	Funding Sources	Prior Years Expenditures	FY13-14 Appropriations	NEW APPROPRIATIONS REQUIRED					Anticipated Completion & Comments	
						Total	FY14-15	FY15-16	FY16-17	FY17-18		FY18-19
FUTURE PROJECTS (Continued)												
72PP- 091	Traffic Signal/Intersecti Imprints - Chrisman & Paradise	458,500	F245-Gas Tax Developer's Contribution	0	0	0	0	0	0	0	0	Jun 17 Priority B New Installation
72PP- 092	Traffic Signal - Pescadero & Western Drwy	337,500	F245-Gas Tax Developer's Contribution	0	0	0	0	337,500	0	0	0	Jun 17 Priority B New Installation
72PP- 093	Intersection Imprints - 11th Street & MacArthur Drive	671,600	F313-Infill Arterials Future Developments	0	0	0	0	0	0	0	0	Jun 19 Priority D Upgrade
72PP- 094	Intersection Imprints - 11th Street & Lincoln Blvd	899,200	F313-Infill Arterials Future Developments	0	0	0	0	0	0	0	0	Jun 19 Priority D Upgrade
72PP- 095	Intersection Imprints - Grant Line Road & Tracy Blvd	695,300	F313-Infill Arterials Future Developments	0	0	0	0	0	0	0	0	Jun 19 Priority D Upgrade
72PP- 096	Intersection Imprints - Schulte Road & Tracy Blvd	1,394,000	F313-Infill Arterials Future Developments	0	0	0	0	0	0	0	0	Jun 19 Priority D Upgrade
72PP- 097	Intersection Imprints - Lammers & 11th Street	1,150,000	F245-Gas Tax Developer's Contribution	0	0	0	0	0	0	0	0	Jun 16 Priority B New Installation
72PP- 098	Traffic Improvements - Tracy Gateway Area	2,288,500	F356-Tracy Gateway A	0	0	0	0	0	0	0	0	Jun 19 Priority D Rehabilitation
72PP- 103	Study of Pedestrial Crossings - Arterials & Railroads	135,000	F245-Gas Tax	0	0	135,000	0	0	0	0	0	Jun 16 Priority A Study
72PP- 104	Traffic Signal Controlle Replacement - Future Years	200,000	F245-Gas Tax	0	0	50,000	50,000	50,000	50,000	0	0	Annual Contingency Replacement
72PP- 106	Replacement of Traffic Loops - Future Phases	352,000	F245-Gas Tax	0	0	176,000	0	176,000	0	0	0	Jun 18 Priority B Biannual Program
72PP- 109	Street Light Installation	82,000	F245-Gas Tax	0	0	0	82,000	0	0	0	0	Jun 17 Priority B
(Continued)						F26						

Group 72 - Traffic Safety FY14-15 CIP Adopted

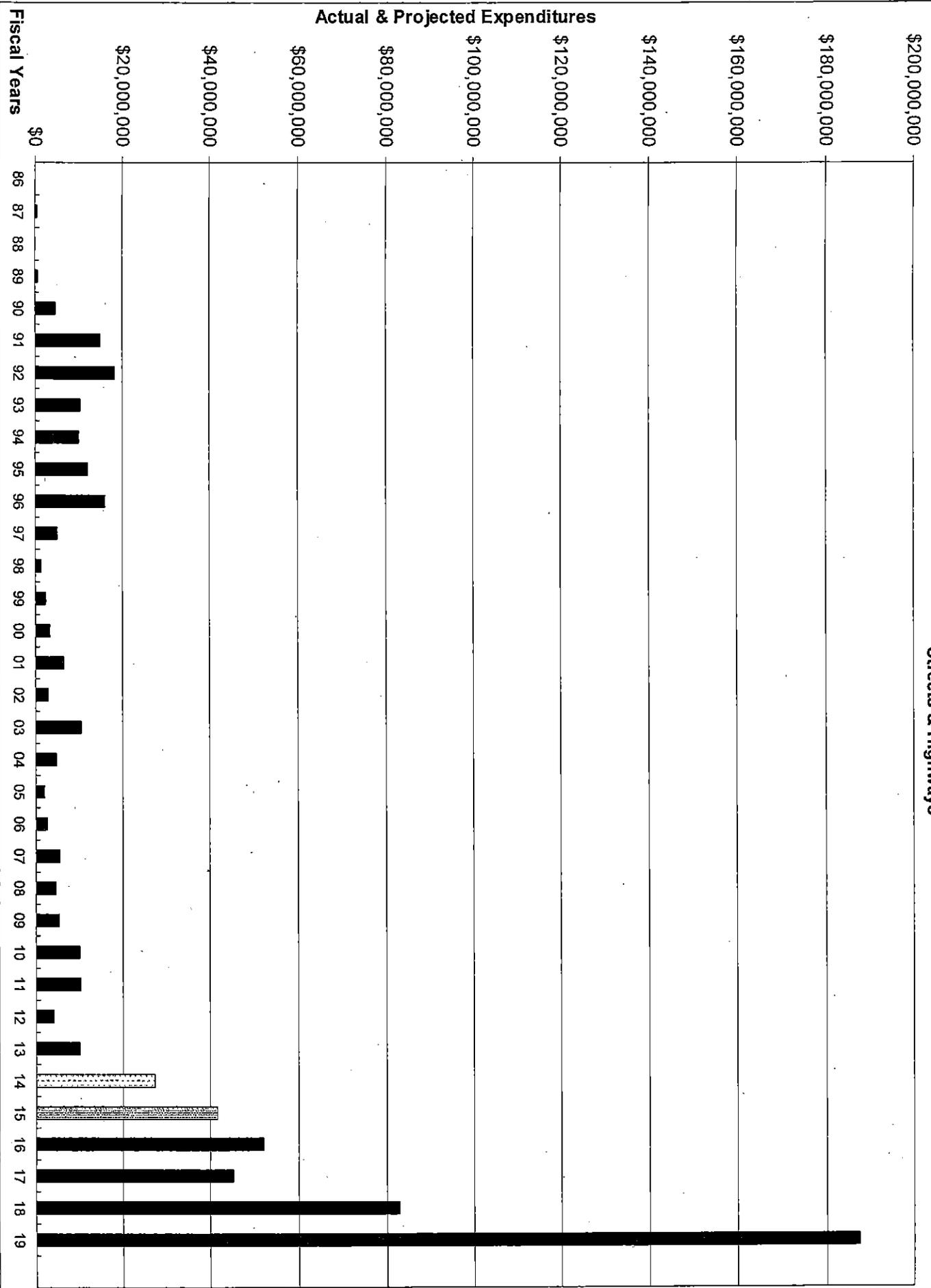
Project #	Project Title	Project \$ Total	Funding Sources	Prior Years Expenditures	FY13-14 Appropriations	Total	NEW APPROPRIATIONS REQUIRED					Anticipated Completion & Comments	
							FY14-15	FY15-16	FY16-17	FY17-18	FY18-19		
FUTURE PROJECTS (Continued)													
72PP- 110	Adaptive Traffic System Corral Hollow Road, Schulte to Mail	1,122,250	F245-Gas Tax Grant Funding	0	0	245,000	0	165,000	80,000	0	0	0	Jun 17 Priority B Upgrade
72PP- 111	Adaptive Traffic System 11th Street, Corral Hollow to MacArthur Grant Funding	911,250	F245-Gas Tax	0	0	210,000	0	146,000	64,000	0	0	0	Jun 17 Priority B Upgrade
72PP- 201	Intersection Imprints - Ellis Prgm Area	3,762,500	F3xx-Ellis Prgm Area Grant Funding	0	0	2,121,860	0	0	0	2,121,860	0	0	Jun 18 Priority C New Installation
						Approved Capital Budget							
						85,924,845	0	6,978,145	11,856,200	31,744,000	35,346,500		
Totals						86,613,912	0	6,978,145	11,856,200	31,744,000	35,346,500		

Group 73 - Streets & Highways

FY14-15 CIP Adopted

by Project Type	Group \$ Total	Pior Years				Total	NEW APPROPRIATIONS REQUIRED					Projects Requiring 11 New Funding 3 in FY14-15
		Expenditures	FY13-14 Appropriations	FY14-15	FY15-16		FY16-17	FY17-18	FY18-19			
21 Current Projects	230,343,985	23,166,069	27,345,496	179,832,400	40,730,500	13,360,900	11,492,000	18,510,000	95,739,000			
2 New Projects	1,656,710	0	0	1,656,710	928,910	0	0	727,800	0			
52 Future Projects	231,958,422	4,162,382	0	227,796,040	0	38,688,980	33,759,900	63,634,560	91,712,600			
Totals	463,959,097	27,328,451	27,345,496	409,285,150	41,659,410	52,049,880	45,251,900	82,872,360	187,451,600			
by Funding Sources												
F301-General Projects	2,400,000	0	0	2,400,000	0	800,000	800,000	800,000	0			
F241-Transp Devel Tax	0	0	0	0	0	0	0	0	0			
F242-Transp Sales Tax	17,782,963	3,920,926	1,908,317	11,953,720	1,522,320	2,107,000	4,435,400	1,250,000	2,639,000			
F245-Gas Tax	20,051,038	1,148,184	1,361,754	17,541,100	303,700	9,380,900	1,210,000	6,829,000	-182,500			
F269-Com Dev Block G	100,535	0	100,535	0	0	0	0	0	0			
F313-Hill Arterials	22,782,750	150,645	-75,645	22,707,750	0	1,846,250	743,500	3,222,800	16,895,200			
F323-Arterials Plan *C*	2,796,000	709,336	1,613,764	472,900	472,900	0	0	0	0			
F345-RSP Arterials	3,520,905	1,172,171	1,649,964	698,770	118,000	580,770	0	0	0			
F351-NE Industrial #1	15,639,788	7,512,227	8,127,561	0	0	0	0	0	0			
F352-So MacArthur Are	2,897,200	0	0	2,897,200	0	152,000	478,300	1,463,000	803,900			
F353-I205 Area Spec Pl	10,392,160	4,014,917	962,743	5,414,500	0	2,350,600	2,484,900	0	579,000			
F354-Indus SP, South	15,949,732	1,054,961	1,001,071	13,893,700	0	1,644,800	9,602,500	2,646,400	0			
F355-Prestido Area	2,062,700	0	0	2,062,700	0	0	359,600	779,900	923,200			
F356-Tracy Gateway Ar	43,040,000	1,552,970	25,660	41,461,370	0	23,451,370	0	18,010,000	0			
F357-NE Industrial #2	7,207,991	270,391	0	6,937,600	0	1,421,700	3,418,100	0	2,097,800			
F3xx-Ellis Pigm Area	5,056,000	0	0	5,056,000	0	0	0	5,056,000	0			
Developers Contribution	57,653,925	1,211,933	1,340,792	55,101,200	0	558,500	2,622,700	14,174,500	37,745,500			
Highways Grants	71,350,210	4,609,790	9,328,980	67,411,440	39,242,490	4,495,790	8,527,800	5,145,360	0			
Future Developments	163,275,200	0	0	163,275,200	0	3,260,200	10,569,100	23,495,400	125,950,500			
Totals	463,959,097	27,328,451	27,345,496	409,285,150	41,659,410	52,049,880	45,251,900	82,872,360	187,451,600			
CIP Expenditure:												
		In FY12-13 >>	10,019,175	1,920,600	New Appropriations							
		In FY11-12 >>	4,092,308	26,183,353	Carryovers from FY13							
		In FY10-11 >>	10,269,232	-1,522,100	Deferrals							
		In FY09-10 >>	10,203,979	763,643	Supplementals							

Tracy Capital Improvement Program
Streets & Highways



Group 73 - Streets & Highways

FY14-15 CIP Adopted

Project #	Project Title	Project \$ Total	Funding Sources	Prior Years Expenditures	FY13-14 Appropriations	NEW APPROPRIATIONS REQUIRED					Anticipated Completion & Comments				
						Total	FY14-15	FY15-16	FY16-17	FY17-18		FY18-19			
CURRENT PROJECTS															
73014	Widening - Corral Hollow Road - Grant Line to Mall Entry	7,777,410	F345-RSP Arterials Developer Contribution F353-I205 Area Spec P F242-Transp Sales Tax Highways Grants	421,622 739,700 2,302,310 3,064,218 791,844	-36 0 -1,292 350,888 108,156	0	0	0	0	0	0	0	Feb 13 Work Completed		
73048	Widening - Grant Line R MacArthur to City Limits	16,686,680	F351-NE Industrial #1 Developer Contribution	7,512,227 0	8,127,561 1,046,892	0	0	0	0	0	0	0	0	Nov 13 Priority B Contract Award Apr 2013	
73061	Extension - Valpico Rd. Peddlebrook to MacArthur	3,101,832	F354-Indus SP, South F313-Infill Arterials	1,054,961 0	1,071 0	2,045,800	0	300,000	1,745,800	0	0	0	0	0	Jun 17 Partial Completion Deferred to FY15-17
73063	Bridge Replacement - 11th Street Bridge	46,067,520	F242-Transp Sales Tax F245-Gas Tax Highways Grants	378,047 636,342 2,688,479	27,253 51,458 1,373,441	1,120,010	1,120,010	0	350,000	0	0	0	0	0	Sep 16 ROW Acq/Design Underw Out to Bid - Jul 14; Award Sept 14
73084	New Interchange - I205 & Lammers Road	61,523,800	F356-Tracy Gateway AI Federal TEA Grant F242-Transp Sales Tax Developer Contribution Future Development	54,340 858,613 6,910 206,063 0	25,660 5,789,187 93,127 293,900 0	18,010,000	0	0	0	18,010,000	0	0	0	0	Jun 20 Design Start Aug 13
73095	Widening - Valpico Road, Tracy to Pebblebrook	11,005,000	F242-Transp Sales Tax F313-Infill Arterials F354-Indus SP, South	160,985 0 0	339,015 0 1,000,000	100,000	0	100,000	0	0	0	0	0	0	Jun 17 Priority B Contract Award Jan 2013
73102	Widening - Corral Hollow Road, Byron to Grant Line (Phase II)	4,160,200	F353-I205 Area Spec P Future Development	335,965 0	964,035 0	2,860,200	0	2,860,200	0	0	0	0	0	0	Jun 14 - Dec 16 Contract Award Sep 2012
73103	Widening - Corral Hollow Road, 11th to Schulte	5,779,000	F323-Arterials Plan "C" F245-Gas Tax	709,336 3,888	1,613,764 188,112	0	0	0	3,263,900	0	0	0	0	0	Dec 16 Design Underway
73109	New Interchange - I205 & Paradise Road	61,289,838	F245-Gas Tax Federal TEA Grant Future Development	122,766 127,810 0	317,072 672,190 0	50,000	50,000	0	0	0	0	0	0	0	Jul 22 Priority D Prelim Planning Complete
(Continued)						F30									

Group 73 - Streets & Highways

FY14-15 CIP Adopted

Project #	Project Title	Project \$ Total	Funding Sources	Prior Years Expenditures	FY13-14 Appropriations	NEW APPROPRIATIONS REQUIRED					Anticipated Completion & Comments			
						Total	FY14-15	FY15-16	FY16-17	FY17-18		FY18-19		
CURRENT PROJECTS (Continued)														
73125	Reconstruction - Larch Road, Holly Drive to e of Tracy	1,182,000	F242-Transp Sales Tax	56,374	55,626	1,070,000	0	1,070,000	0	0	0	0	0	Jun 16 Design Underway
73126	Widening - MacArthur D Schulte to Valpico, Phase II	5,843,900	F313-Infill Arterials Federal TEA Grant RSTP Grant	150,645 143,044 0	-75,645 399,856 0	886,000 1,146,000 3,194,000	0 0 0	346,000 0 3,194,000	540,000 1,146,000 0	0 0 0	0 0 0	0 0 0	0 0 0	Jun 17 EIR Underway See 73PP-120 for Phase :
73130	Street Patch & Overlay Program - FY13 Phase	1,002,100	F242-Transp Sales Tax F245-Gas Tax RSTP Grant	94,751 1,930 0	285,249 140,070 480,100	0 0 0	0 0 0	0 0 0	0 0 0	0 0 0	0 0 0	0 0 0	0 0 0	Jun 14 Contract Award Dec 2013
73131	Pavement Management System - FY13	163,000	F245-Gas Tax	36,515	23,485	103,000	0	50,000	0	0	0	53,000	0	Dec 13 Next Phase FY15-16
73132	Sidewalk Improvements MacArthur & Schulte	235,000	F245-Gas Tax	222,423	12,577	0	0	0	0	0	0	0	0	Mar 13 Work Completed
73133	Temporary Sidewalk - Valpico, Tracy to MacArthur	170,000	F245-Gas Tax	7,443	162,557	0	0	0	0	0	0	0	0	Jun 14 Contract Award May 2013
73134	Sidewalk Repairs & Wheelchair Ramps - FY13 Phase	265,000	F242-Transp Sales Tax	159,641	105,359	0	0	0	0	0	0	0	0	Sep 13 Work Completed
73135	Construction - N-S Para Road, s of GLR, w of Christian	1,200,000	F357-NE Industrial #2 F345-RSP Arterials	0 0	0 1,200,000	0 0	0 0	0 0	0 0	0 0	0 0	0 0	0 0	Nov 13 Developer Bull
73136	Directional Signs in 120E at 2 locations	1,050,000	F245-Gas Tax F345-RSP Arterials	0 0	0 450,000	600,000	0	118,000	0	482,000	0	0	0	Jun 16 Design Underway
73137	Reconstruct Railroad Crossings - 4 locations	200,000	F242-Transp Sales Tax F245-Gas Tax	0 116,877	0 83,123	0	0	0	0	0	0	0	0	Aug 13 Work Completed
73138	Street Patch & Overlay Program - FY14 Phase	1,166,050	F242-Transp Sales Tax F245-Gas Tax RSTP Grant	0 0 0	530,000 130,000 506,050	0	0	0	0	0	0	0	0	Sep 14 Design Underway
(Continued)						F31								

Group 73 - Streets & Highways FY14-15 CIP Adopted

Project #	Project Title	Project \$ Total	Funding Sources	Prior Years Expenditures	NEW APPROPRIATIONS REQUIRED					Anticipated Completion & Comments		
					FY13-14 Appropriations	Total	FY14-15	FY15-16	FY16-17		FY17-18	FY18-19
NEW PROJECTS												
73140	Street Patch & Overlay Program - FY15 Phase	456,010	F242-Transp Sales Tax F246-Gas Tax RSTP Grant	0	402,310	402,310	0	0	0	0	0	Phased Annual Program Rehabilitation
73141	Improvements - Fabian Road, w/ of Corral Hollow	1,200,700	F323-Arterials Plan "C" Future Development	0	472,900	472,900	0	0	0	727,800	0	Jun 18 Priority C Expansion
Totals						1,656,710	928,910	0	0	727,800	0	
2	New Projects	1,656,710		0	1,656,710	928,910	0	0	0	727,800	0	

Group 73 - Streets & Highways

FY14-15 CIP Adopted

Project #	Project Title	Project \$ Total	Funding Sources	Prior Years Expenditures	FY13-14 Appropriations	Total	NEW APPROPRIATIONS REQUIRED					Anticipated Completion & Comments	
							FY14-15	FY15-16	FY16-17	FY17-18	FY18-19		
FUTURE PROJECTS													
73002	Extension - MacArthur Drive, 11th to Mt Diablo Phase I	12,195,519	F345-RSP Arterials Highways Grants F242-Transp Sales Tax	750,549	0	98,770	98,770	0	0	0	0	0	Jun 17 ROW Acq/Design Underw
73035	Widening - Grant Line Road - Naglee to Lammers	3,502,412	F35X-1205 Area Planir F353-1205 Area Spec P Developer Contribution	53,527 1,323,115 266,170	0	0	1,859,600	0	0	0	0	0	Jun 16 Partial Completion Expansion
73057	Construction - Street "C" Naglee to Corral Hollow	2,134,200	F353-1205 Area Spec P	0	0	2,134,200	0	241,000	1,893,200	0	0	0	Jun 17 Priority B New Facility
73067	Turn Lane - Grant Line Road & 1205	110,000	F245-Gas Tax	0	0	110,000	0	110,000	0	0	0	0	Jun 16 Priority B Rehabilitation
73069	Construct - Street "A", Grant Line Road to Auto Mall Drive	1,917,600	F353-1205 Area Spec P Developer Contribution	0	0	841,700	0	250,000	591,700	0	0	0	Jun 17 Priority C New Installation
73090	Extension - Chrisman R Grant Line Rd to 1205	3,985,891	F357-NE Industrial #2	270,391	0	3,715,500	0	297,400	3,418,100	0	0	0	Jun 17 Priority B Prelim Plan Completed
73092	Widening - Lammers Rc 3,000 feet south of 11th Street	10,976,000	F356-Tracy Gateway A	1,498,630	0	9,477,370	0	9,477,370	0	0	0	0	Jun 16 Priority B Expansion
73093	Widening - 11th Street, 4,500 feet west of Lammers	13,974,000	F356-Tracy Gateway A	0	0	13,974,000	0	13,974,000	0	0	0	0	Jun 16 Priority B Expansion
73PP-001b	Street Patch & Overlay Program - Future Phases	3,399,790	F242-Transp Sales Tax F245-Gas Tax RSTP Grant	0	0	2,180,000	0	530,000	540,000	550,000	140,000	560,000	Phased Annual Program Rehabilitation
73PP-007	Reconstruction - Clover Road, Lincoln to City Limits	2,016,000	F245-Gas Tax SU County Participation	0	0	1,008,000	0	0	0	1,008,000	0	0	Jun 18 Priority D Rehabilitation
73PP-010	Widening - Grant Line Road, Parker to MacArthur	0	F245-Gas Tax F313-Infill Arterials	0	0	-2,710,500	0	0	0	0	0	-2,710,500	Jun 19 Reimbursement for 73052
(Continued)						F34							

Group 73 - Streets & Highways FY14-15 CIP Adopted

Project #	Project Title	Project \$ Total	Funding Sources	Prior Years Expenditures	FY13-14 Appropriations	Total	NEW APPROPRIATIONS REQUIRED					Anticipated Completion & Comments					
							FY14-15	FY15-16	FY16-17	FY17-18	FY18-19						
<u>FUTURE PROJECTS (Continued)</u>																	
73PP- 021	Reconstruction Macarth Drive, Lime to Valpico	4,300,000	Future Development	0	0	4,300,000	0	400,000	3,900,000	0	0	0	Jun 17	Priority C Expansion			
73PP- 025	Extension - Lincoln Blvd Kavanagh to Clover	1,265,000	F245-Gas Tax Assessments	0	0	1,265,000	0	0	0	0	1,265,000	0	0	0	Jun 18	Priority D New Installation	
73PP- 028	Itchy Improvements - 1205 & Grant Line Road, Phase II	12,260,000	F245-Gas Tax Future Development	0	0	300,000	0	0	0	0	50,000	250,000	0	0	Jun 19	Priority D Expansion	
73PP- 040	Widening - Corral Hollow Road, 1205 north to City Limits	2,976,000	F352-So MacArthur Are F355-Presidio Area Future Development Developer Contribution	0	0	57,900	0	0	0	0	43,200	1,662,200	0	0	0	Jun 18	Priority C Expansion See 7314
73PP- 041	Widening - MacArthur Drive, 11th to Schulle Phases II & III	6,161,300	F352-So MacArthur Are F355-Presidio Area Future Development Developer Contribution	0	0	125,400	0	0	0	0	94,100	2,527,800	0	0	0	Jun 19	Priority D Expansion See 7302
73PP- 042	Widening - Grant Line Road, Tracy to Corral Hollow	4,990,400	F352-So MacArthur Are F355-Presidio Area Future Development Developer Contribution	0	0	155,300	0	0	0	0	116,400	2,281,500	0	0	0	Jun 18	Priority C Expansion See 7303
73PP- 043	Widening - Grant Line Road, Byron to Street A	2,927,400	F352-So MacArthur Are F355-Presidio Area Future Development Developer Contribution	0	0	86,000	0	0	0	0	65,000	1,227,100	0	0	0	Jun 18	Priority C Expansion
73PP- 044	Widening - 11th Street, MacArthur to Chrisman	8,490,500	F352-So MacArthur Are F355-Presidio Area Future Development Developer Contribution	0	0	223,700	0	0	168,200	0	3,295,800	4,476,800	0	0	0	Jun 18	Priority C Expansion

(Continued)

Group 73 - Streets & Highways

FY14-15 CIP Adopted

Project #	Project Title	Project \$ Total	Funding Sources	Prior Years Expenditures	FY13-14 Appropriations	Total	NEW APPROPRIATIONS REQUIRED					Anticipated Completion & Comments				
							FY14-15	FY15-16	FY16-17	FY17-18	FY18-19					
FUTURE PROJECTS (Continued)																
73PP-045	Widening - Central Ave, Sycamore to Tracy	1,902,800	F352-So MacArthur Ave F355-Presidio Area Future Development Developer Contribution	0 0 0 0	0	0	0	0	0	0	0	0	0	0	Jun 18 Priority C Expansion	
73PP-046	Widening - Corral Hollow Road, I580 to Old Schulte	23,081,600	F352-So MacArthur Ave F355-Presidio Area Future Development Developer Contribution	0 0 0 0	0	630,600	0	0	0	0	0	0	0	0	0	Jun 19 Priority D Expansion
73PP-047	Widening - Lammers Road, 11th south to City Limit	15,068,600	F352-So MacArthur Ave F355-Presidio Area Future Development Developer Contribution	0 0 0 0	0	425,200	0	0	0	0	0	0	0	0	0	Jun 19 Priority D Expansion
73PP-047	Widening - Schulte Roa w of Lammers, 3,200 LF	4,065,200	F352-So MacArthur Ave F355-Presidio Area Future Development Developer Contribution	0 0 0 0	0	108,000	0	0	0	0	0	0	0	0	0	Jun 18 Priority C Expansion
73PP-049	Construction - Schulte Road, Lammers to Crossroads Drive	5,553,000	F352-So MacArthur Ave F355-Presidio Area Future Development Developer Contribution	0 0 0 0	0	162,000	0	0	0	0	0	0	0	0	0	Jun 19 Priority D Expansion
73PP-050	Widening - Schulte Roa Sycamore to Crossroad Drive, Phase II	6,543,100	F352-So MacArthur Ave F355-Presidio Area Future Development Developer Contribution	0 0 0 0	0	205,000	0	0	0	0	0	0	0	0	0	Jun 19 Priority D Expansion
73PP-051	Widening - Valpico Roa Lammers to Corral Hollow	11,688,800	F352-So MacArthur Ave F355-Presidio Area Future Development Developer Contribution	0 0 0 0	0	311,500	0	0	0	0	0	0	0	0	0	Jun 19 Priority D Expansion
(Continued)						F36										

Group 73 - Streets & Highways

FY14-15 CIP Adopted

Project #	Project Title	Project \$ Total	Funding Sources	Prior Years Expenditures	FY13-14 Appropriations	Total	NEW APPROPRIATIONS REQUIRED					Anticipated Completion & Comments				
							FY14-15	FY15-16	FY16-17	FY17-18	FY18-19					
<u>FUTURE PROJECTS (Continued)</u>																
73PP 054	Aqueduct Crossings - Corral Hollow Road	4,198,600	F352-So MacArthur Ave F355-Presidio Area Future Development	0	0	254,600	0	254,600	0	0	0	0	0	Jun 18	Priority C Expansion	
73PP- 055	Widening - MacArthur D 1205 to Pescadero	1,124,300	F357-NE Industrial #2	0	0	1,124,300	0	1,124,300	0	0	0	0	0	Jun 16	Priority B Expansion	
73PP 070	Extension - Larch Road, Holly to MacArthur Dr	1,500,250	F313-Infill Arterials	0	0	1,500,250	0	1,500,250	0	0	0	0	0	Jun 16	Priority B New Installation	
73PP- 071	Widening - MacArthur D 1205, north to Arbor	1,033,000	F313-Infill Arterials F245-Gas Tax	0	0	543,000	0	0	0	0	0	0	543,000	Jun 19	Priority D Expansion	
73PP- 080	Intersection Modifier 11th & Tracy	3,570,000	F245-Gas Tax	0	0	3,570,000	0	0	0	1,780,000	1,790,000	0	0	Jun 19	Priority D Rehabilitation	
73PP- 093	Widening - Pescadero Ave, MacArthur to Paradise	2,097,800	F357-NE Industrial #2	0	0	2,097,800	0	0	0	0	2,097,800	0	0	Jun 19	Priority D Expansion	
73PP- 095	Construct - Crossroads, Greystone to Schulle	8,412,100	F352-So MacArthur Ave Developer Contribution Future Development	0	0	152,000	0	152,000	0	0	0	0	0	0	Jun 17	Priority B New Installation
73PP- 097	Widening - Linne Road, Corral Hollow to Tracy	13,641,700	F313-Infill Arterials Developer Contribution	0	0	13,641,700	0	0	0	0	13,641,700	0	0	Jun 19	Priority D Expansion	
73PP- 098b	Sidewalk, Curb, & Gutter Repairs - Future Phases	930,000	F245-Gas Tax F289-Corn Dev Block G	0	0	930,000	0	525,000	130,000	135,000	140,000	0	0		Phased Annual Program Rehabilitation	
73PP- 103	Construction- North Roadways, 1205 NW Area	579,000	F353-1205 Area Spec P	0	0	579,000	0	0	0	0	579,000	0	0	Jun 19	Priority B New Installation	
73PP- 105	Reconstruction - Bessie Ave, Carlton to Granline	1,170,000	F242-Transp Sales Tax F245-Gas Tax	0	0	1,170,000	0	1,170,000	0	0	0	0	0	Jun 16	Priority B Rehabilitation	
(Continued)						F37										

Group 73 - Streets & Highways

FY14-15 CIP Adopted

Project #	Project Title	Project \$ Total	Funding Sources	Prior Years Expenditures	FY13-14 Appropriations	NEW APPROPRIATIONS REQUIRED					Anticipated Completion & Comments			
						Total	FY14-15	FY15-16	FY16-17	FY17-18		FY18-19		
<u>FUTURE PROJECTS (Continued)</u>														
73PP-108	Construction - Larch Rd north side, east of Holly	850,000	F245-Gas Tax	0	0	850,000	0	0	0	0	0	0	Jun 16	Priority B Upgrade
73PP-109	Reconstruction MacArthur Drive, 1205 to Arbor Rd	500,000	F245-Gas Tax	0	0	500,000	0	0	0	0	0	0	Jun 16	Priority B Rehabilitation
73PP-110	Reconstruction Lammer Road, north of Redbridge Rd	111,000	F245-Gas Tax	0	0	111,000	0	0	0	0	0	0	Jun 16	Priority B Rehabilitation
73PP-111	Reconstruction 6th St, west of Tracy Blvd	200,000	F245-Gas Tax	0	0	200,000	0	0	0	0	0	0	Jun 16	Priority B Rehabilitation
73PP-112	Widening - Schulte Road west of Barcelona	1,600,000	F245-Gas Tax Developer Contribution	0	0	0	0	0	0	0	1,600,000	0	Jun 19	Priority D Expansion
73PP-113	Rehabilitation Street Shoulders - Clywilde	600,000	F245-Gas Tax	0	0	600,000	0	150,000	150,000	150,000	150,000	0		Phased Annual Program Rehabilitation
73PP-114	Install Sidewalk - Lowell Ave, southside Tracy to Chester	115,000	F245-Gas Tax	0	0	115,000	0	115,000	0	0	0	0	Jun 16	Priority B New Installation
73PP-117	Reconstruction - Larch Road, Holly Drive to WWTP	207,000	F242-Transp Sales Tax	0	0	207,000	0	207,000	0	0	0	0	Jun 16	Priority B Rehabilitation
73PP-119	Street Light Repairs - various locations	76,000	F245-Gas Tax	0	0	76,000	0	76,000	0	0	0	0	Jun 16	Priority B Replacement
73PP-120	Widening - MacArthur D Schulte to Valpico, Phase II	7,015,200	F313-Infill Arterials F354-Indus SP, South Developer Contribution	0	0	3,222,800	0	0	0	3,222,800	0	0	Jun 18	Priority D Expansion
73PP-121	Reconstruction - Corral Hollow Road, north of 1580	1,780,000	F245-Gas Tax	0	0	1,780,000	0	1,780,000	0	0	0	0	Jun 16	Priority B Rehabilitation
(Continued)						F38								

Group 73 - Streets & Highways FY14-15 CIP Adopted

Project #	Project Title	Project \$ Total	Funding Sources	Prior Years Expenditures	FY13-14 Appropriations	NEW APPROPRIATIONS REQUIRED					Anticipated Completion & Comments		
						Total	FY14-15	FY15-16	FY16-17	FY17-18		FY18-19	
FUTURE PROJECTS (Continued)													
73PP- 122	Reconstruction - Tracy Blvd, south of Line	1,871,000	F245-Gas Tax	0	0	1,871,000	0	0	0	0	0	Jun 18 Priority C Rehabilitation	
73PP- 123	Reconstruction - Arbor Road, Holly east to RR	735,000	F245-Gas Tax	0	0	735,000	0	0	0	0	0	Jun 17 Priority B Rehabilitation	
73PP- 126	11th Street Beautification Corral Hollow to MacArthur	3,591,000	F242-Transp Sales Tax F301-General Projects Highways Grants	0	0	600,000 2,400,000 591,000	0	200,000 800,000 132,000	200,000 800,000 231,000	200,000 800,000 228,000	0	0	Jun 18 Priority B Rehabilitation
73PP- 201	Street Improvements - Ellis Prgm Area	8,965,360	F3xx-Ellis Prgm Area Highways Grants	0	0	5,056,000 3,909,360	0	0	0	5,056,000 3,909,360	0	0	
Totals						227,796,040	0	38,688,980	33,759,900	63,634,560	91,712,600		
52	Future Projects	231,958,422		4,162,382	0	227,796,040	0	38,688,980	33,759,900	63,634,560	91,712,600		

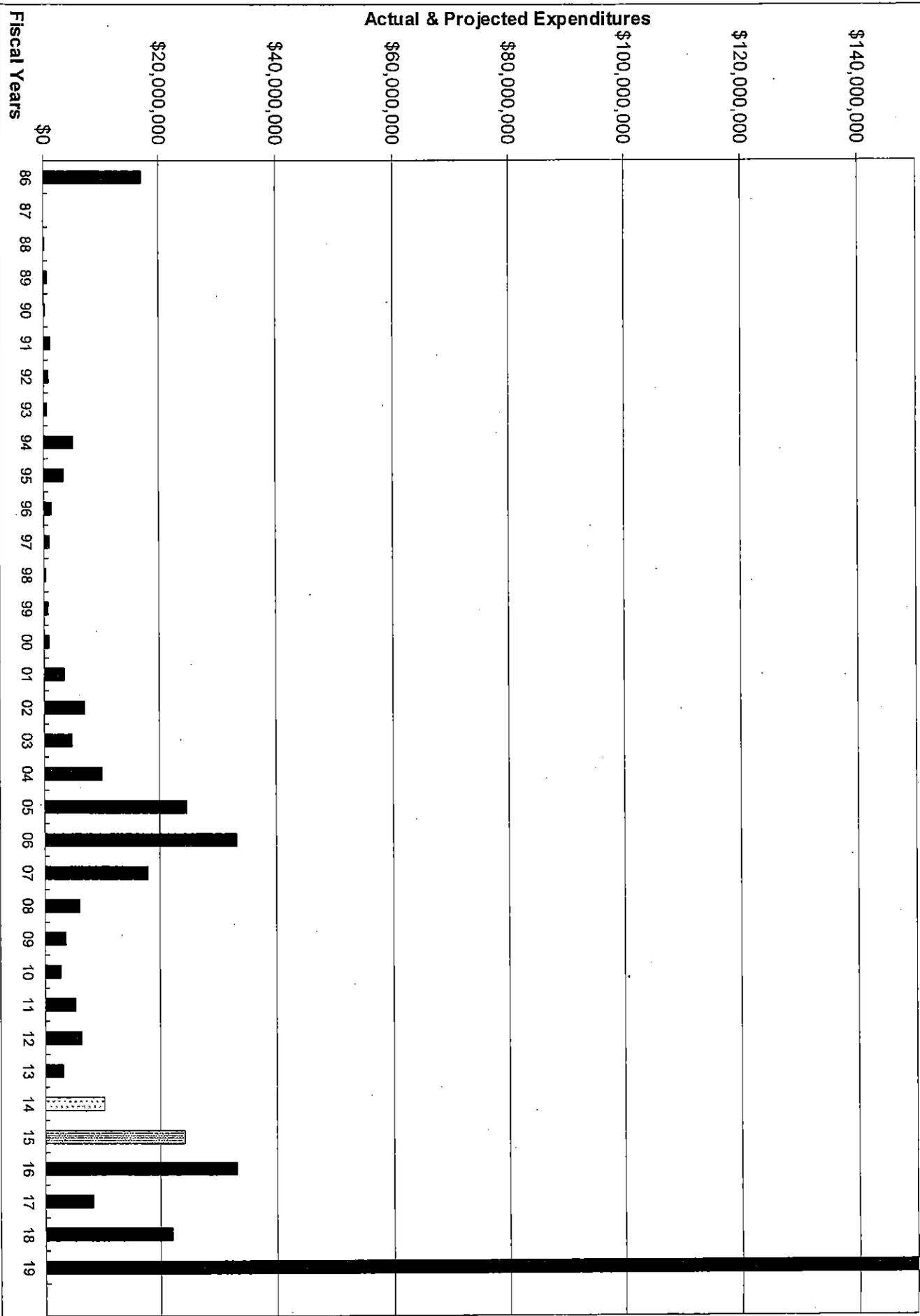
Group 74 - Wastewater Improvements FY14-15 CIP Adopted

by Project Type	Group \$ Total	Prior Years		NEW APPROPRIATIONS REQUIRED					Projects Requiring New Funding in FY14-15	
		Expenditures	FY13-14 Appropriations	Total	FY14-15	FY15-16	FY16-17	FY17-18		FY18-19
17 Current Projects	72,653,906	9,516,657	10,237,749	52,899,500	20,895,000	26,304,500	4,700,000	1,000,000	0	7 New Funding in FY14-15
5 New Projects	7,173,800	0	0	7,173,800	3,306,800	3,867,000	0	0	0	
15 Future Projects	180,396,650	59,920	0	180,336,730	0	2,977,580	3,572,000	21,022,930	152,764,220	
37 Totals	260,224,356	9,576,577	10,237,749	240,410,030	24,201,800	33,149,080	8,272,000	22,022,930	152,764,220	
by Funding Sources										
F513-Water Capital	748,534	722,624	25,910	0	0	0	0	0	0	
F523-Wastewater Caplt	171,525,592	5,566,614	2,249,478	163,709,500	1,560,000	4,932,880	3,924,500	2,939,500	150,352,620	
Debt Proceeds	14,000,000	0	0	14,000,000	14,000,000	0	0	0	0	
F325-Uilities - Plan *C*	405,000	0	0	405,000	405,000	0	0	0	0	
F351-NE Indus Area #1	0	0	0	0	0	0	0	0	0	
F352-Uilities - SMPA	0	0	0	0	0	0	0	0	0	
F353-1205 Area Spec Pl	0	0	0	0	0	0	0	0	0	
F354-ISP South Area	2,115,200	45,402	210,298	1,859,500	0	1,859,500	0	0	0	
F356-Tracy Gateway Ar	9,070,000	474,348	-14,348	8,610,000	1,603,800	553,500	2,347,500	1,693,600	2,411,600	
F357-NE Indus Area #2	7,436,200	2,767,589	2,232,411	2,436,200	633,000	1,803,200	0	0	0	
F3xx-Ellis Pym Area	15,129,530	0	0	15,129,530	0	0	0	15,129,530	0	
State & Local Grant	0	0	0	0	0	0	0	0	0	
Assessments Bonds	0	0	0	0	0	0	0	0	0	
Developer's Contributor	10,353,000	0	2,534,000	7,819,000	6,000,000	0	0	1,819,000	0	
Future Developments	29,441,300	0	3,000,000	26,441,300	0	24,000,000	2,000,000	441,300	0	
	260,224,356	9,576,577	10,237,749	240,410,030	24,201,800	33,149,080	8,272,000	22,022,930	152,764,220	

CIP Expenditure: In FY12-13 >> 3,136,735
 In FY11-12 >> 6,251,335
 In FY10-11 >> 5,250,991
 In FY09-10 >> 2,756,906

New Appropriations: 25,565,000
 Carryovers from FY13: 4,960,847
 Deferrals: -20,522,098
 Supplementals: 234,000

Tracy Capital Improvement Program
Wastewater Improvements



Group 74 - Wastewater Improvements

FY14-15 CIP Adopted

Project #	Project Title	Project \$ Total	Funding Sources	Prior Years Expenditures	FY13-14 Appropriations	Total	NEW APPROPRIATIONS REQUIRED					Anticipated Completion & Comments	
							FY14-15	FY15-16	FY16-17	FY17-18	FY18-19		
CURRENT PROJECTS (Continued)													
74101	Security Cameras for - WW Treatment Plant	30,000	F523-Wastewater Capit	0	30,000	0	0	0	0	0	0	0	Nov 13 Priority A New Equipment
74102	Laboratory Information Management System	32,000	F523-Wastewater Capit	14,587	17,413	0	0	0	0	0	0	0	Jan 13 Priority A Work Completed
74103	WW Lines Replacement Bessie Ave, Emerson to Grant Line Rd	960,000	F523-Wastewater Capit	0	90,000	870,000	870,000	0	0	0	0	0	Jun 15 Priority B Replacement
74104	WW Lines Replacement Program - FY14 Phase	265,000	F523-Wastewater Capit	0	265,000	0	0	0	0	0	0	0	Oct 14 Design Underway
74105	WWT Plant Replacement Program - FY14 Phase	240,000	F523-Wastewater Capit	0	240,000	0	0	0	0	0	0	0	Annual Contingency Replacement
74106	Pump Station Replacement Larch Road PS	1,920,000	F523-Wastewater Capit	0	500,000	1,420,000	0	420,000	0	1,000,000	0	0	Jun 18 Priority D5 Rehabilitation
74107	Wastewater Treatment Plant Expansion - Phase 2	29,000,000	F523-Wastewater Capit Future Developments	0	3,000,000	0	0	24,000,000	2,000,000	0	0	0	Jul 17 Priority B1 Expansion
74110	WWS Improvements - Tracy Hills Area	1,414,000	F523-Wastewater Capit Developer's Contribution	0	1,414,000	0	0	0	0	0	0	0	Design Underway
Totals						52,899,500	20,895,000	26,304,500	4,700,000	1,000,000	0	0	
17	Current Projects	72,653,906		9,516,657	10,237,749								

Group 74 - Wastewater Improvements

FY14-15 CIP Adopted

Project #	Project Title	Project \$ Total	Funding Sources	Prior Years Expenditures	FY13-14 Appropriations	NEW APPROPRIATIONS REQUIRED					Anticipated Completion & Comments	
						Total	FY14-15	FY15-16	FY16-17	FY17-18		FY18-19
NEW PROJECTS												
74111	WW Lines Replacement Program - FY15 Phase	255,000	F523-Wastewater Capl	0	0	255,000	255,000	0	0	0	0	0 Annual Contingency Replacement
74112	Fiber Optic Lines from Civic Center to WWTP	250,000	F523-Wastewater Capl	0	0	250,000	250,000	0	0	0	0	0 Annual Contingency Replacement
74113	Force Main Expansion - Larch Road	2,008,800	F325-Utilities - Plan "C" F356-Tracy Gateway Ar	0	0	405,000 1,603,800	405,000 1,603,800	0	0	0	0	0 Jun 16 Priority B2 Expansion
74114	WWCS Improvements - NE Industrial Area #2 - Phase 2	4,500,000	F357-NE Indus Area #2 Future Developments	0	0	2,436,200 2,063,800	633,000 0	1,803,200	0	0	0	0 Nov 16 Priority D2 New
74115	Wastewater Discharge Permit Studies - FY15 Phase	160,000	F523-Wastewater Capl	0	0	160,000	160,000	0	0	0	0	0 Jun 15 Priority B4 Study
Totals						7,173,800	3,306,800	3,867,000	0	0	0	0
5 New Projects						7,173,800	3,306,800	3,867,000	0	0	0	0

Group 74 - Wastewater Improvements FY14-15 CIP Adopted

Project #	Project Title	Project \$ Total	Funding Sources	Prior Years Expenditures	FY13-14 Appropriations	Total	NEW APPROPRIATIONS REQUIRED					Anticipated Completion & Comments		
							Approved Capital Budget	FY14-15	FY15-16	FY16-17	FY17-18		FY18-19	
FUTURE PROJECTS														
74064	Reclaimed Water Pipe - 11th Street, west of Lammers	1,893,600	F356-Tracy Gateway A	0	0	1,893,600	0	200,000	1,693,600	0	0	0	Jun 18 Priority D New	
74069	WW Lines Extension - to Chrisman Site	1,819,000	F523-Wastewater Capital Developer's Contribution	59,920	0	-59,920 1,819,000	0	1,759,080	0	-1,819,000	0	0	Dec 13 Design Underway	
74PP- 001b	WW Lines Replacement Program - Future Phases	1,090,000	F523-Wastewater Capital	0	0	1,090,000	0	265,000	270,000	275,000	280,000	0	Annual Contingency Replacement	
74PP- 049	Wastewater Treatment Plant Expansion - Phase 3	24,228,320	F523-Wastewater Capital Future Developments	0	0	10,228,320 14,000,000	0	0	0	0	10,228,320	14,000,000	Oct 21 Priority D8 Expansion	
74PP- 054	WWTP Plant Replacement Program - Future Phases	1,020,000	F523-Wastewater Capital	0	0	1,020,000	0	240,000	250,000	260,000	270,000	0	Annual Contingency Replacement	
74PP- 064	Wastewater Conveyance for Tracy Gateway, Phase I	2,147,500	F356-Tracy Gateway A	0	0	2,147,500	0	0	2,147,500	0	0	0	Jun 17 Priority C1 New	
74PP- 065	Reclaimed WD System for Tracy Gateway Area	553,500	F356-Tracy Gateway A	0	0	553,500	0	553,500	0	0	0	0	Jun 16 Priority B3 New	
74PP- 067	Reclaimed Water Impacts for Tracy Gateway Area	15,866,900	F356-Tracy Gateway A Future Developments	0	0	2,411,600 13,455,300	0	0	0	0	2,411,600	13,455,300	Jun 19 Priority D7 New	
74PP- 101	Wastewater Treatment Plant Expansion - Phase 4	105,100,000	F523-Wastewater Capital Future Developments	0	0	0 105,100,000	0	0	0	0	0	105,100,000	0	Jun 25 Priority D9 Expansion
74PP- 105	Wastewater Recycling Pipeline, Phase II	1,500,000	F523-Wastewater Capital	0	0	1,500,000	0	0	0	0	1,500,000	0	Jun 19 Priority D6 New	
74PP- 108	Wastewater Discharge Permit Studies - Future Phases	645,000	F523-Wastewater Capital	0	0	645,000	0	160,000	270,000	215,000	0	0	Jun 19 Priority B4 Study	
74PP- 111	Outfall Pipeline Rehab MacArthur Drive, North of WWTP	1,220,000	F523-Wastewater Capital	0	0	1,220,000	0	0	220,000	0	1,000,000	0	Jun 19 Priority D4 Replacement	

(Continued)

Group 74 - Wastewater Improvements

FY14-15 CIP Adopted

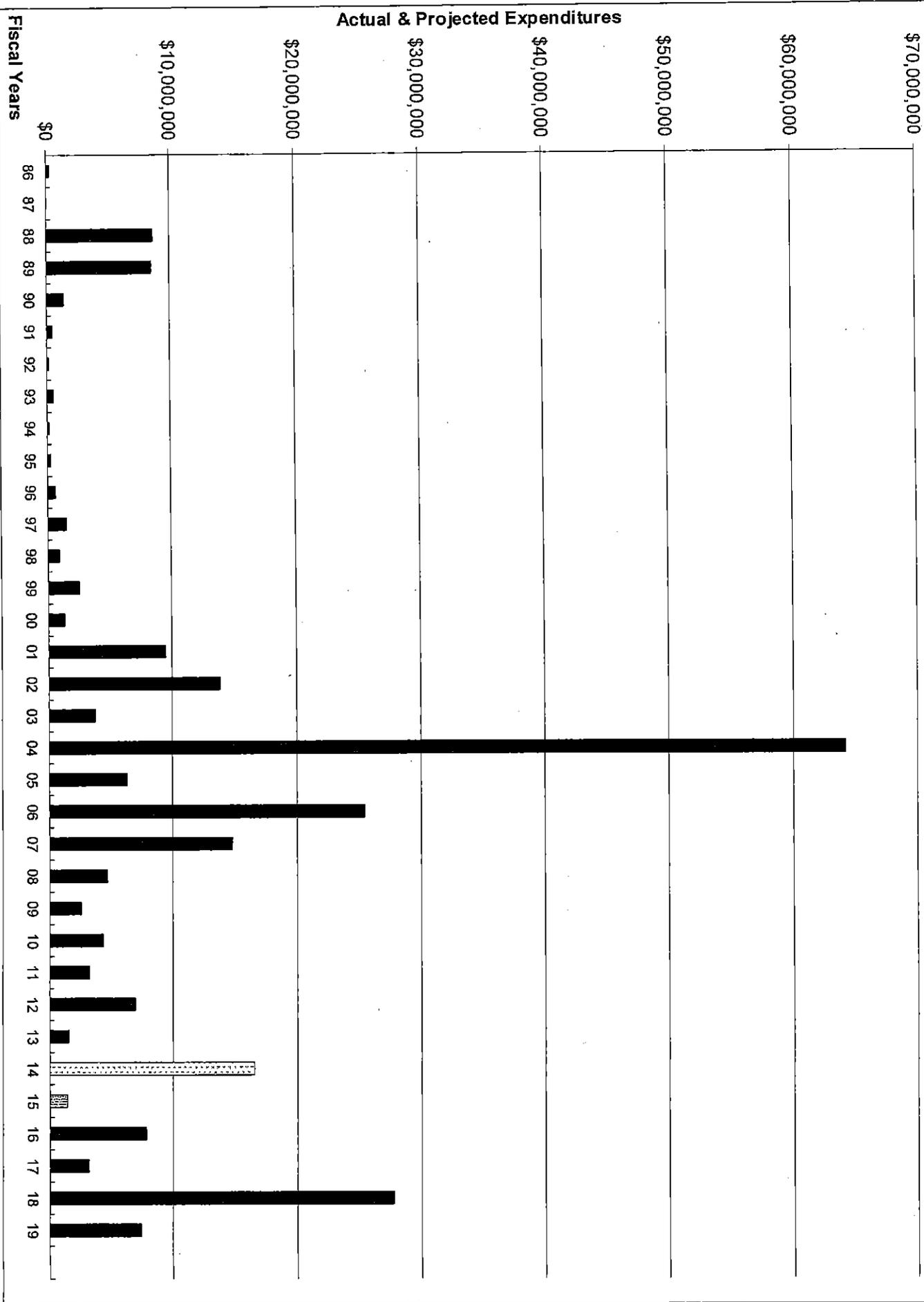
Project #	Project Title	Project \$ Total	Funding Sources	Prior Years Expenditures	FY13-14 Appropriations	Total	NEW APPROPRIATIONS REQUIRED					Anticipated Completion & Comments	
							FY14-15	FY15-16	FY16-17	FY17-18	FY18-19		
<u>FUTURE PROJECTS (Continued)</u>													
74PP- 112	Pump Station Upgrade Corral Hollow near Larch Road	5,287,500	F523-Wastewater Capit Future Developments	0	0	5,287,500	0	0	0	0	0	0	Jun 19 Priority D3 Expansion
74PP- 113	WW Upgrades - MacArthur Dr and Pacheco	2,895,800	F523-Wastewater Capit Future Developments	0	0	2,454,500 441,300	0	0	214,500	0	2,240,000 441,300	0	Jun 18 Priority D3 Expansion
74PP- 201	Wastewater Imprints Corral Hollow & Larch Roads	15,129,530	F3xx-Ellis Prgm Area Future Developments	0	0	15,129,530 0	0	0	0	0	15,129,530 0	0	Jun 19 New
<u>Totals</u>						180,336,730	0	2,977,580	3,572,000	21,022,930	152,764,220		
15	Future Projects	180,336,650		59,920	0	180,336,730	0	2,977,580	3,572,000	21,022,930	152,764,220		

Group 75 - Water Improvements FY14-15 CIP Adopted

by Project Type	Group \$ Total	Prior Years Expenditures	FY13-14 Appropriations	Total	NEW APPROPRIATIONS REQUIRED					Projects Requiring New Funding in FY14-15
					FY14-15	FY15-16	FY16-17	FY17-18	FY18-19	
15 Current Projects	37,256,069	13,459,567	16,550,962	7,245,530	400,000	6,020,530	275,000	275,000	275,000	7
3 New Projects	1,020,000	0	0	1,020,000	1,020,000	0	0	0	0	3
13 Future Projects	39,371,700	0	0	39,371,700	0	1,812,400	2,880,000	27,520,200	7,159,100	
31 Totals	77,647,769	13,459,567	16,550,962	47,637,230	1,420,000	7,832,930	3,155,000	27,795,200	7,434,100	

by Funding Sources		F101-General	F511-Water Operating	F513-Water Capital	State Grant or Loan	F325-Utilities Plan C*	F351-NE Indus Area #1	F352-So MacArthur PA	F353-1205 Area Spec Pl	F354-ISP South Area	F356-Tracy Gateway Ar	F357-NE Indus Area #2	F3xx-Ellis Pigm Area	F387-RSP Reserves	Assessments	Developers Contribution	Future Developments	CIP Expenditures	in FY12-13 >>	in FY11-12 >>	in FY10-11 >>	in FY09-10 >>	New Appropriations Carryovers from FY13	Deferrals	Supplementals			
		0	2,317,072	25,293,862	0	527,784	2,929,089	464,200	0	766,550	11,897,102	0	22,770,200	0	0	1,731,900	8,950,000	77,647,759	1,554,515	6,955,445	3,232,274	4,395,398	9,016,000	723,062	0	6,811,900		
		0	667,072	10,052,824	0	228,558	2,457,541	0	0	53,572	0	0	0	0	0	0	0	13,459,567	16,550,962	13,459,567	16,550,962	47,637,230	1,420,000	7,832,930	3,155,000	27,795,200	7,434,100	
		0	275,000	8,465,628	0	983,086	135,448	409,900	0	0	0	0	0	0	0	1,731,900	4,550,000	47,637,230	1,420,000	7,832,930	3,155,000	27,795,200	7,434,100	0	0	0	0	
		0	1,375,000	6,775,410	0	-683,860	336,100	54,300	0	766,550	11,843,530	0	22,770,200	0	0	0	4,400,000	47,637,230	1,420,000	7,832,930	3,155,000	27,795,200	7,434,100	0	0	0	0	0
		0	275,000	1,145,000	0	0	0	0	0	0	0	0	0	0	0	0	0	1,420,000	1,420,000	1,420,000	1,420,000	1,420,000	1,420,000	0	0	0	0	0
		0	275,000	1,800,410	0	-683,860	336,100	54,300	0	766,550	5,284,430	0	0	0	0	0	0	7,832,930	3,155,000	7,832,930	3,155,000	27,795,200	7,434,100	0	0	0	0	0
		0	275,000	2,880,000	0	0	0	0	0	0	0	0	0	0	0	0	0	3,155,000	3,155,000	3,155,000	3,155,000	3,155,000	3,155,000	0	0	0	0	0
		0	275,000	350,000	0	0	0	0	0	0	0	0	0	0	0	0	0	27,795,200	27,795,200	27,795,200	27,795,200	27,795,200	27,795,200	0	0	0	0	0
		0	275,000	600,000	0	0	0	0	0	0	0	0	0	0	0	0	0	7,434,100	7,434,100	7,434,100	7,434,100	7,434,100	7,434,100	0	0	0	0	0

Tracy Capital Improvement Program
Water Improvements



Group 75 - Water Improvements

FY14-15 CIP Adopted

Project #	Project Title	Project \$ Total	Funding Sources	Prior Years Expenditures	FY13-14 Appropriations	Total	NEW APPROPRIATIONS REQUIRED					Anticipated Completion & Comments			
							FY14-15	FY15-16	FY16-17	FY17-18	FY18-19				
CURRENT PROJECTS															
75046	Water Distribution System - NE Industrial Area	2,929,089	F351-NE Indus Area #1	2,457,541	135,448	336,100	0	336,100	0	0	0	0	0	0	Jun 16 Phase 1 Completed Phase 2 Deferred
75061	Water Supply Purchase from WSID & BCID	11,397,496	F513-Water Capital	8,897,496	2,500,000	0	0	0	0	0	0	0	0	0	Feb 14 75% Purchased Feb 04
75076	Well Rehabilitation - Lincoln Park & Production #1	634,900	F513-Water Capital	533,511	101,389	0	0	0	0	0	0	0	0	0	Apr 13 Work Completed
75078	Aquifer Storage & Recovery Program	1,000,000	F513-Water Capital State Grant	399,923	400,077	200,000	100,000	100,000	0	0	0	0	0	0	Jun 16 Work Underway
75085	Water Distribution Syste Tracy Gateway Area	5,338,002	F356-Tracy Gateway Al	53,572	0	5,284,430	0	5,284,430	0	0	0	0	0	0	Jun 16 Developer to Build Design Underway
75108	Water Lines -MacArthur Drive, Linne to Valpico	1,562,600	F513-Water Capital F325-Willies PlanC* F352-So MacArthur PA F354-ISP South Area	0 171,724 0 0	0 980,976 409,900 0	113,400 -859,950 0 746,550	0 0 0 0	113,400 -859,950 0 746,550	0 0 0 0	0 0 0 0	0 0 0 0	0 0 0 0	0 0 0 0	0 0 0 0	Dec 15 Design Underway Also, See 73126
75113	WDS Capacity Maint Mgmt System-Data Acquisition	125,000	F513-Water Capital	51,692	23,308	50,000	25,000	25,000	0	0	0	0	0	0	Jun 16 Work Underway
75114	Water Lines Replaceame Program - FY13 Phase	320,000	F513-Water Capital	227,036	92,964	0	0	0	0	0	0	0	0	0	Jul 13 Work Completed
75115	Security Cameras for Water Treatment Plant	30,000	F513-Water Capital	0	30,000	0	0	0	0	0	0	0	0	0	Dec 13 Work Completed
75117	Water Lines Replaceame Program - FY14 Phase	320,000	F513-Water Capital	0	320,000	0	0	0	0	0	0	0	0	0	Jun 15 Design Underway
75118	Construct New Clearwel Water Treatment Plant	4,550,000	F513-Water Capital Future Developments	0	0	4,550,000	0	0	0	0	0	0	0	0	Jun 15 Priority B1 Replacement

(Continued)

Group 75 - Water Improvements

FY14-15 CIP Adopted

Project #	Project Title	Project \$ Total	Funding Sources	Prior Years Expenditures	FY13-14 Appropriations	NEW APPROPRIATIONS REQUIRED						Anticipated Completion & Comments	
						Total	FY14-15	FY15-16	FY16-17	FY17-18	FY18-19		
CURRENT PROJECTS (Continued)													
75119	Purchase of SSJLD Water Supply from Lathrop	5,000,000	F513-Water Capital Future Developments	0	5,000,000	-5,000,000	0	5,000,000	0	0	0	0	Sep 13 Priority B2 Purchase Completed
75991	Water Purchases for Storage with Semi-tropic WUSD	2,317,072	F511-Water Operating	667,072	275,000	1,375,000	275,000	275,000	275,000	275,000	275,000	275,000	Annual Contingency
75120	Water Imprints - Tracy Hills Area	1,414,000	F513-Water Capital Developers Contributor	0	1,414,000	0	0	0	0	0	0	0	
75121	Booster Pump Station - Water Treatment Plant	317,900	F513-Water Capital Developers Contributor	0	317,900	0	0	0	0	0	0	0	
Totals						7,245,530	400,000	6,020,530	275,000	275,000	275,000	275,000	
15	Current Projects	37,256,059		13,459,567	16,550,962								

Group 75 - Water Improvements FY14-15 CIP Adopted

Project #	Project Title	Project \$ Total	Funding Sources	Prior Years Expenditures	FY13-14 Appropriations	NEW APPROPRIATIONS REQUIRED					Anticipated Completion & Comments	
						Total	FY14-15	FY15-16	FY16-17	FY17-18		FY18-19
NEW PROJECTS												
75122	Water Lines Replaceme Program - FY15 Phase	320,000	F513-Water Capital	0	0	320,000	320,000	0	0	0	0	Annual Contingency Replacement
75123	Water Meter Replaceme Program	600,000	F513-Water Capital	0	0	600,000	600,000	0	0	0	0	Jun 15 Priority B1 Replacement
75124	Process Controllers Replacement - Water Treatment Plant	100,000	F513-Water Capital	0	0	100,000	100,000	0	0	0	0	Jun 15 Priority B1 Replacement
Totals						1,020,000	1,020,000	0	0	0	0	
3	New Projects	1,020,000		0	0	1,020,000	1,020,000	0	0	0	0	

CITY OF TRACY CAPITAL IMPROVEMENT PROGRAM FIVE YEAR PLAN -- FY14-15 through FY18-19 CIP PROJECT LISTINGS 01-Jul-14

Group 75 - Water Improvements FY14-15 CIP Adopted

Project #	Project Title	Project \$ Total	Funding Sources	Prior Years Expenditures	FY13-14 Appropriations	NEW APPROPRIATIONS REQUIRED					Anticipated Completion & Comments	
						Total	FY14-15	FY15-16	FY16-17	FY17-18		FY18-19
FUTURE PROJECTS												
75PP- 001b	Water Lines Replaceme Program - Future Phases	1,370,000	F513-Water Capital	0	0	1,370,000	0	320,000	350,000	350,000	350,000	Annual Contingency Replacement
75PP- 067	Storage & Pumping Facilities	4,400,000	F513-Water Capital Future Developments	0	0	4,400,000	0	0	0	4,400,000	0	Jun 18 Priority D1 New Facilities
75PP- 077	Water Line Replacemen 20th to 23rd Streets, bwr Holly & Bessie Avenues	2,280,000	F513-Water Capital	0	0	2,280,000	0	0	2,280,000	0	0	Jun 17 Priority C4 Replacement
75PP- 081	Water Storage Reservoi Tracy Gateway Area	2,268,000	F356-Tracy Gateway Ai	0	0	2,268,000	0	0	0	2,268,000	0	Jun 19 Priority D2 New Facilities
75PP- 082	Water Pump Stations - Tracy Gateway Area	1,620,000	F356-Tracy Gateway Ai	0	0	1,620,000	0	0	0	1,620,000	0	Jun 19 Priority D3 New Facilities
75PP- 083	Emergency Well for Tracy Gateway Area	2,671,100	F356-Tracy Gateway Ai	0	0	2,671,100	0	0	0	2,671,100	0	Jun 19 Priority D4 New Facilities
75PP- 086	Watershed Survey - 2014 Update	35,000	F513-Water Capital	0	0	35,000	0	0	0	0	0	Dec 15 Priority C2 Study
75PP- 087	Urban Water Mgmt Plan - 2014 Update	85,000	F513-Water Capital	0	0	85,000	0	0	0	0	0	Dec 15 Priority C1 Study
75PP- 094	Water Master Plan - Citywide Update	38,100	F513-Water Capital F356-Tracy Gateway Ai	0	0	14,000 24,100	0	0	0	0	0	Jun 16 Priority D5 Study
75PP- 097	Water Line Replacemen Bessie Ave - Lowell to Grant Line	1,180,000	F513-Water Capital	0	0	1,180,000	0	0	0	0	0	Jun 16 Priority C3 Replacement
75PP- 099	Conjunctive Groundwat Use Study	154,300	F513-Water Capital F352-So MacArthur PA F354-ISP South Area	0	0	80,000 54,300 20,000	0	0	0	0	0	Jun 16 Priority B3 Study

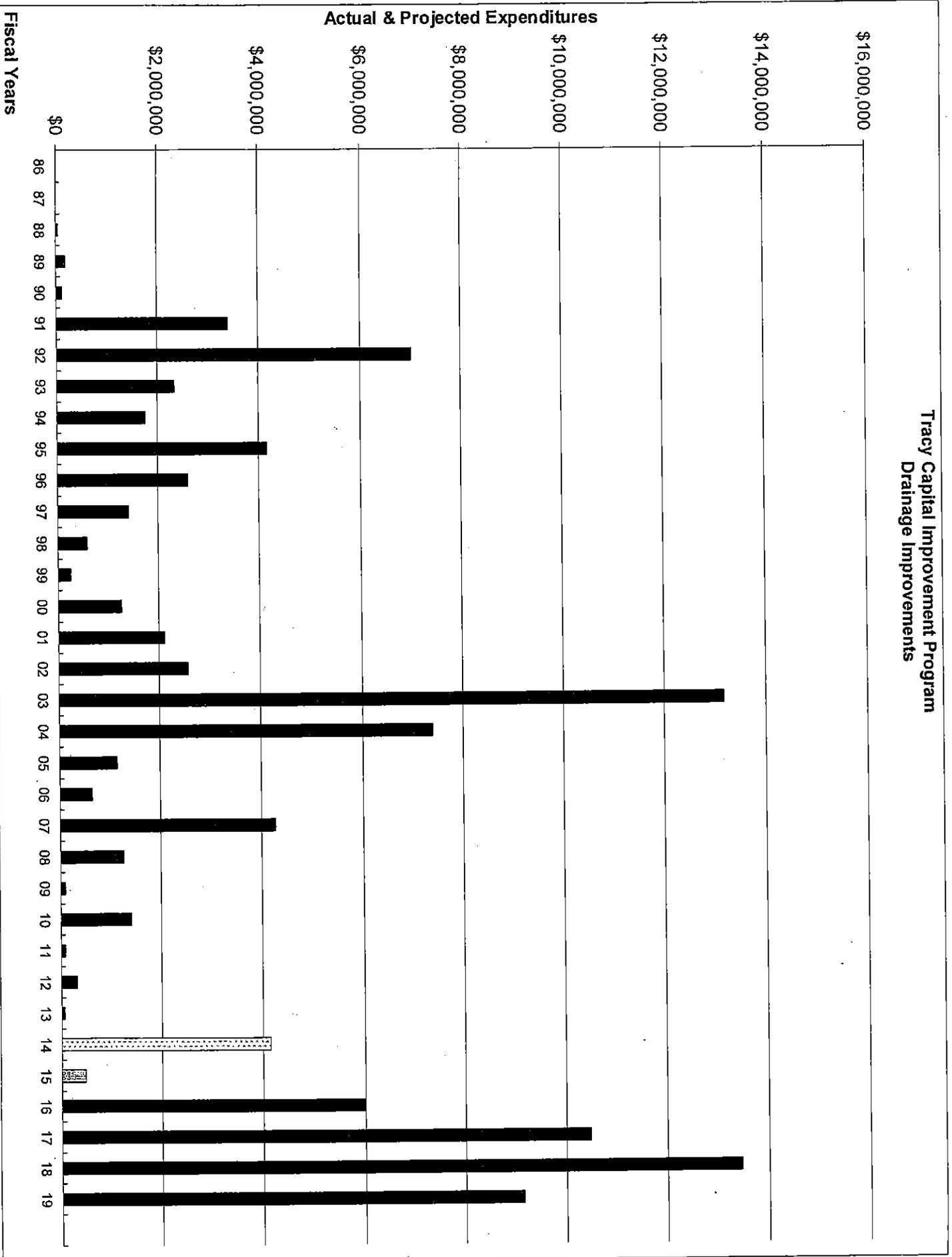
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Group 76 - Water Improvements FY14-15 CIP Adopted

Project #	Project Title	Project \$ Total	Funding Sources	Prior Years Expenditures	FY13-14 Appropriations	NEW APPROPRIATIONS REQUIRED					Anticipated Completion & Comments
						Total	FY14-15	FY15-16	FY16-17	FY17-18	
FUTURE PROJECTS (Continued)											
75PP- 105	Water Filter Replaceme Water Treatment Plant	500,000	F513-Water Capital	0	0	500,000	0	250,000	0	250,000	Jun 19 Priority B1 Replacement
75PP- 201	Water Imprnts Ellis Pigm Area	22,770,200	F3xx-Ellis Pigm Area Future Developments	0	0	22,770,200	0	0	0	22,770,200	0
Totals						39,371,700	0	1,812,400	2,880,000	27,520,200	7,159,100
13	Future Projects	39,371,700		0	0	39,371,700	0	1,812,400	2,880,000	27,520,200	7,159,100

Tracy Capital Improvement Program
Drainage Improvements



Group 76 - Drainage Improvements FY14-15 CIP Adopted

Project #	Project Title	Project \$ Total	Funding Sources	Prior Years Expenditures	FY13-14 Appropriations	NEW APPROPRIATIONS REQUIRED						Anticipated Completion & Comments		
						Total	FY14-15	FY15-16	FY16-17	FY17-18	FY18-19			
CURRENT PROJECTS														
76028	Storm Drain Line - Grant Line, w of Paradise	1,346,761	F351-NE Indus Area # Future Developments	56,150	1,290,611	0	0	0	0	0	0	0	Nov 13 Work Completed	
76036	Channel Improvements C2 Channel, NE Industrial Area	1,599,500	F351-NE Indus Area #	0	1,599,500	0	0	0	0	0	0	0	Nov 13 Work Completed	
76054	Pump Station Upgrade Larch Rd, sw corner at Tracy	200,000	F541-Drainage Enterpr F301-General Projects	14,899	185,101	0	0	0	0	0	0	0	Jun Priority A Contract Award Nov 13	
76059	Drainage Improvements South MacArthur, Phase 2	875,600	F322-Plan C Drainage F352-So MacArthur Ar Developer's Contribuit	0	621,600	0	0	0	0	0	0	0	Dec 14 Design start Jan 14	
76062	Storm Drains Replacem Program - FY14 Phase	203,000	F541-Drainage Enterpr F245-Gas Tax	0	43,000	0	0	0	0	0	0	0	Nov Priority A Rehabilitation	
Totals						0	0	0	0	0	0	0		
5 Current Projects						4,224,861	77,070	4,147,791	0	0	0	0	0	

Group 76 - Drainage Improvements

FY14-15 CIP Adopted

Project #	Project Title	Project \$ Total	Funding Sources	Prior Years Expenditures	FY13-14 Appropriations	NEW APPROPRIATIONS REQUIRED					Anticipated Completion & Comments		
						Total	FY14-15	FY15-16	FY16-17	FY17-18		FY18-19	
NEW PROJECTS													
76043	Drainage Improvements NE Industrial Area	340,100	F351-NE Indus Area #	0	0	340,100	340,100	0	0	0	0	0	Jun 15 Deferred to Future
76063	Storm Drains Replacem Program - Future Phases	43,000	F541-Drainage Enterpr	0	0	43,000	43,000	0	0	0	0	0	Jun - Priority A Rehabilitation
76065	Pump Station Rehab - Corral Hollow & Kavanaugh	100,000	F312-Infill Drainage F541-Drainage Enterpr	0	0	100,000	0	100,000	0	0	0	0	Jun - Priority B Rehabilitation
Totals						483,100	483,100	0	0	0	0	0	
New Projects						483,100	483,100	0	0	0	0	0	

Group 76 - Drainage Improvements

FY14-15 CIP Adopted

Project #	Project Title	Project \$ Total	Funding Sources	Prior Years Expenditures	FY13-14 Appropriations	Total	NEW APPROPRIATIONS REQUIRED					Anticipated Completion & Comments			
							FY14-15	FY15-16	FY16-17	FY17-18	FY18-19				
FUTURE PROJECTS															
76039	Drainage Improvements Berg Ave Area	339,025	F541-Drainage Entlpr	25	0	339,000	0	0	339,000	0	0	0	0	0	Jun 17 Deferred to Future
76045	New Detention Basin 2 ^a ISP South, Zone 2	5,236,507	F354-Indus SP, South F322-Plan C Drainage F312-Infill Drainage Developer's Contribut	703,285 839,222 0 3,694,000	0 0 0 0	1,250,760 0 0 -1,250,760	0 0 0 0	1,250,760 0 0 -1,250,760	0 0 0 0	0 0 0 0	0 0 0 0	0 0 0 0	0 0 0 0	0	Apr 07 Reimbursement Due
76PP- 001b	Storm Drains Replacem Program - Future Phases	172,000	F541-Drainage Entlpr	0	0	172,000	0	43,000	43,000	43,000	43,000	43,000	43,000	0	Annual Contingency Rehabilitation
76PP- 007	Pond Removal - 3 Locations	1,085,005	F312-Infill Drainage	21,005	0	1,064,000	0	659,800	404,200	0	0	0	0	0	Jun ' Priority B Removal
76PP- 009	Construction - West sid ^e Channel, north of Edgewood	228,200	F345-RSP Prgm Mgmt Developer Contributor	0	0	228,200	0	228,200	0	0	0	0	0	0	Jun ' Priority B Deferred to Future
76PP- 024	Detention Basin 2B Blue Zone	5,450,700	F312-Infill Drainage F322-Plan C Drainage F354-ISP South Area Future Developments	0 0 0 0	0 0 0 0	1,172,400 1,103,300 1,077,800 2,097,200	0 0 0 0	0 243,600 1,077,800 2,097,200	1,172,400 859,700	0 0 0 0	0 0 0 0	0 0 0 0	0 0 0 0	0	Jun ' Priority C New Installation
76PP- 027	Storm Drain - Sterling Park/Johnson (Yellow Zone)	172,000	F322-Plan C Drainage Developer's Contribut	0	0	172,000	0	172,000	0	0	0	0	0	0	Jun 06 Reimbursement Due
76PP- 028	Storm Drain - San Marco 42" (Yellow Zone)	181,200	F322-Plan C Drainage Developer's Contribut	0	0	181,200	0	0	181,200	0	0	0	0	0	Nov 99 Reimbursement Due
76PP- 035	Storm Drains Outfall- Eastlake 18" (Pink Zone)	170,200	F322-Plan C Drainage F312-Infill Drainage Developer's Contribut	0 0 0	0 0 0	109,200 61,000 -170,200	0 0 0	0 0 0	0 0 0	0 0 0	0 0 0	0 0 0	109,200 61,000 -170,200	0	Jan 01 Reimbursement Due
76PP- 039	Drainage Improvements South MacArthur, Phase 3	6,500,000	F352-S ^o MacArthur Ar Future Developments	0	0	6,500,000	0	0	0	0	0	0	6,500,000	0	Jun ' Priority D New Installation

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Group 76 - Drainage Improvements

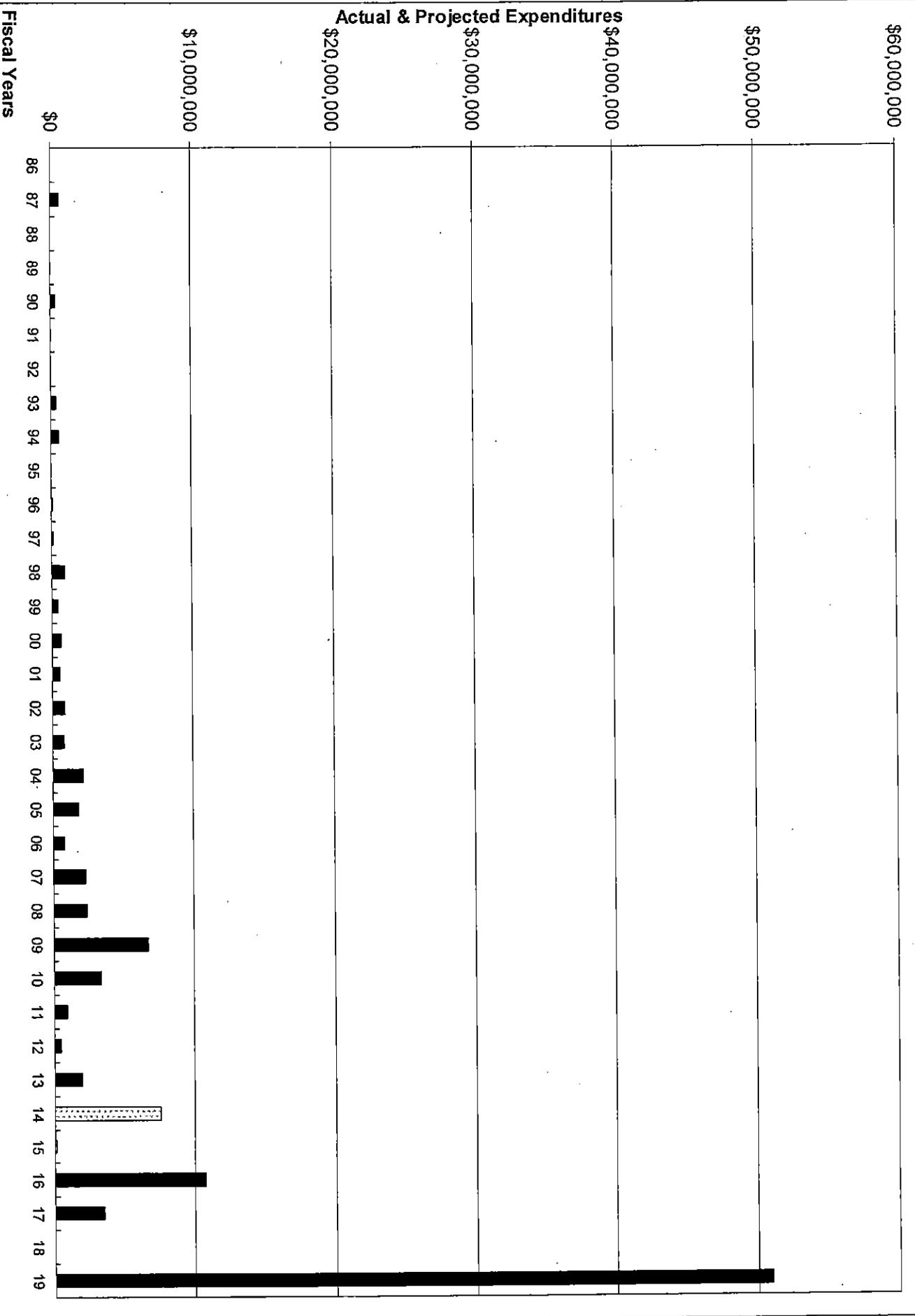
FY14-15 CIP Adopted

Project #	Project Title	Project \$ Total	Funding Sources	Prior Years Expenditures	FY13-14 Appropriations	Total	Approved FY14-15 through FY18-19					Anticipated Completion & Comments		
							FY14-15	FY15-16	FY16-17	FY17-18	FY18-19			
FUTURE PROJECTS (Continued)														
76PP- 048	Drainage Improvements ISP South, Zone 1	768,100	F354-ISP South Area	0	0	768,100	0	768,100	0	0	0	0	Jun - Priority B New Installation	
76PP- 052	Drainage Improvements Grant Line Road	3,118,000	F351-NE Indus Area #	0	0	3,118,000	0	3,118,000	0	0	0	0	Jun - Priority C New Installation	
76PP- 053	Drainage Improvements Lincoln Blvd, 11 th to Beverly Place	293,800	F312-Infill Drainage	0	0	293,800	0	0	0	293,800	0	0	Jun - Priority D Upgrade	
76PP- 051	Drainage Conveyance Tracy Gateway Area	689,100	F356-Tracy Gateway /	0	0	689,100	0	0	0	0	689,100	0	Jun - Priority D	
76PP- 064	Drainage Improvements Pescadero Avenue	11,056,900	F357-NE Indus Area # Future Developments	0	0	11,056,900	0	0	0	9,619,100	1,437,800	0	Jun - Priority D New Installation	
76PP- 065	Drainage Improvements Chrisnan Rd, Paradise to Grant Line	484,900	F357-NE Indus Area #	0	0	484,900	0	0	0	0	484,900	0	Jun - Priority B New Installation	
76PP- 068	Storm Drains Outfall- Rocha and 35	1,393,200	F312-Infill Drainage	0	0	1,393,200	0	0	1,393,200	0	0	0	Jun - Priority C New Installation	
76PP- 070	Drainage Improvements Bessie Ave, Eaton to GLR	3,329,400	F312-Infill Drainage F541-Drainage Enterpi	0	0	3,329,400	0	229,600	3,099,800	0	0	0	Jun - Priority D Upgrade	
76PP- 071	Drainage Improvements Arbor Ave, west of MacArthur Dr	710,000	F312-Infill Drainage F541-Drainage Enterpi	0	0	710,000	0	0	0	710,000	0	0	Jun - Priority B Rehabilitation	
76PP- 201	Drainage Improvements Ellis Prgm Area	3,517,800	F3xx-Ellis Prgm Area Future Developments	0	0	3,517,800	0	0	0	3,517,800	0	0	Jun 18	
Totals						44,896,037	5,780,937	0	39,115,100	0	6,000,300	10,466,300	13,473,700	9,154,800
20 Future Projects						44,896,037	5,780,937	0	39,115,100	0	6,000,300	10,466,300	13,473,700	9,154,800

Group 77 - Airport & Transit Improvements FY14-15 CIP Adopted

by Project Type	Group \$ Total	Prior Years Expenditures	FY13-14 Appropriations	NEW APPROPRIATIONS REQUIRED					Projects Requiring New Funding in FY14-15	
				Total	FY14-15	FY15-16	FY16-17	FY17-18		FY18-19
13 Current Projects	12,350,216	2,222,333	7,565,883	2,562,000	0	2,562,000	0	0	0	2
1 New Projects	5,603,990	0	0	5,603,990	100,000	3,482,000	2,021,990	0	0	0
26 Future Projects	57,202,600	0	0	57,202,600	0	4,702,000	1,470,000	0	51,030,600	0
40 Totals	75,156,806	2,222,333	7,565,883	65,368,590	100,000	10,746,000	3,491,990	0	51,030,600	
by Funding Sources										
F242-Transp Sales Tax	0	0	0	0	0	0	0	0	0	0
F301-General Projects	15,782,750	42,823	660,777	15,079,150	0	633,200	976,550	0	13,469,400	0
F381-Comm Dev Ag Proj	0	0	0	0	0	0	0	0	0	0
F563-Airport Capital	3,945,436	152,686	-2,630	3,795,400	0	178,000	170,000	0	3,447,400	0
F573-Transit Capital	1,100,000	332,842	227,158	540,000	0	260,000	260,000	0	20,000	0
FAA Grant	45,982,180	118,779	4,781,661	41,031,740	100,000	6,503,800	1,045,440	0	33,382,500	0
FTA Grant	5,111,200	1,523,854	1,507,346	2,080,000	0	1,040,000	1,040,000	0	0	0
Other Federal Grant	0	0	0	0	0	0	0	0	0	0
State Aviation Grant	730,300	0	0	730,300	0	19,000	0	0	711,300	0
State Aviation Loan	2,112,000	0	0	2,112,000	0	2,112,000	0	0	0	0
State Transit Grant	442,940	51,369	391,571	0	0	0	0	0	0	0
	75,156,806	2,222,333	7,565,883	65,368,590	100,000	10,746,000	3,491,990	0	51,030,600	
CIP Expenditures										
		In FY12-13 >>	1,942,334	4,476,000	New Appropriations					
		In FY11-12 >>	470,060	2,939,883	Carryovers from FY13					
		In FY10-11 >>	932,003	0	Deferrals					
		In FY09-10 >>	3,365,657	150,000	Supplementals					

Tracy Capital Improvement Program
 Airport & Transit Improvements



Group 77 - Airport & Transit Improvements FY14-15 CIP Adopted

Project #	Project Title	Project \$ Total	Funding Sources	Prior Years Expenditures	FY13-14 Appropriations	NEW APPROPRIATIONS REQUIRED					Anticipated Completion & Comments		
						Total	FY14-15	FY15-16	FY16-17	FY17-18		FY18-19	
CURRENT PROJECTS (Continued)													
77546	ParaTransit Bus Replacements - FY13	600,000	F573-Transit Capital FTA Grant	0	120,000	0	0	0	0	0	0	Jun 15 Priority B2 Replacement	
77547	Transit Buses Replacements - FY13	700,000	F573-Transit Capital FTA Grant	0	140,000	0	0	0	0	0	0	Jun 15 Priority B2 Replacement	
77548	Radio Replacements TRACER Buses	50,000	F573-Transit Capital FTA Grant	0	10,000	0	0	0	0	0	0	Jun 15 Priority A2 Replacement	
13 Current Projects						12,350,216	2,222,333	7,565,883	2,562,000	0	2,562,000	0	0

Group 77 - Airport & Transit Improvements

FY14-15 CIP Adopted

Project #	Project Title	Project \$ Total	Funding Sources	Prior Years Expenditures	FY13-14 Appropriations	NEW APPROPRIATIONS REQUIRED					Anticipated Completion & Comments	
						Total	FY14-15	FY15-16	FY16-17	FY17-18		FY18-19
NEW PROJECTS												
77039	Pavement Maintenance Tracy Airport, Future Phases	5,603,990	F301-General Projects F563-Airport Capital FAA Grant	0	0	1,334,750	358,200	976,550	0	0	0	Jun 17 Priority A1 Rehabilitation
				0	0	0	0	0	0	0	0	
				0	0	4,269,240	100,000	3,123,800	1,045,440	0	0	
						Approved Capital Budget						
						5,603,990	100,000	3,482,000	2,021,990	0	0	
1 New Projects						5,603,990	100,000	3,482,000	2,021,990	0	0	

Group 77 - Airport & Transit Improvements

FY14-15 CIP Adopted

Project #	Project Title	Project \$ Total	Funding Sources	Prior Years Expenditures	FY13-14 Appropriations	NEW APPROPRIATIONS REQUIRED					Anticipated Completion & Comments	
						Total	FY14-15	FY15-16	FY16-17	FY17-18		FY18-19
FUTURE PROJECTS												
77PP- 016	Aircraft Wash Facility -	99,400	F563-Airport Capital	0	0	99,400	0	0	0	0	0	Jun 19 Priority B5
77PP- 017	Helicopter Pad Airport	91,800	F563-Airport Capital FAA Grant State Aviation Grant	0 0 0	0 0 0	2,400 87,200 2,200	0 0 0	0 0 0	0 0 0	0 87,200 2,200	0	Jun 19 Priority C7 New
77PP- 018	Utilities & Drainage Improvements - Tracy Airport	2,776,000	F563-Airport Capital	0	0	2,776,000	0	0	0	2,776,000	0	Jun 19 Priority C6 Upgrade & Expansion
77PP- 025	Land Acquisition - Tracy Airport	21,849,000	F563-Airport Capital FAA Grant State Aviation Grant F301-General Projects	0 0 0 0	0 0 0 0	102,600 20,756,400 519,000 471,000	0 0 0 0	0 2,250,000 0 250,000	0 0 0 0	0 18,506,400 519,000 221,000	102,600	Jun 19 Priority C10 New - Expansion
77PP- 026	Construct FBO Facility - Main Airport Area	5,604,000	F563-Airport Capital F301-General Projects	0 0	0 0	35,000 5,569,000	0 0	0 0	0 0	0 5,569,000	35,000	Jun 19 Priority C8 New
77PP- 027	Construct FBO Facility - South Hangar Area	3,108,000	F563-Airport Capital F301-General Projects	0 0	0 0	0 3,108,000	0 0	0 0	0 0	0 3,108,000	0	Jun 19 Priority C9 New
77PP- 028	Taxiway Construction & Paving - Tracy Airport	4,808,000	F563-Airport Capital FAA Grant State Aviation Grant	0 0 0	0 0 0	125,100 4,567,500 115,400	0 0 0	0 0 0	0 0 0	0 4,567,500 115,400	125,100	Jun 19 Priority C5 New
77PP- 029	Road Upgrade - Tracy Blvd, s of Linne	2,943,000	F563-Airport Capital F301-General Projects	0 0	0 0	0 2,943,000	0 0	0 0	0 0	0 2,943,000	0	Jun 19 Priority C3 Rehabilitation
77PP- 030	Repairs FBO Building - Tracy Airport	1,337,000	F563-Airport Capital F301-General Projects	0 0	0 0	0 1,337,000	0 0	0 0	0 0	0 1,337,000	0	Jun 19 Priority C4 Replacement
77PP- 033	Airport Security Enhancements	3,112,000	F563-Airport Capital FAA Grant State Aviation Grant	0 0 0	0 0 0	80,900 2,956,400 74,700	0 0 0	0 0 0	0 0 0	0 2,956,400 74,700	80,900	Jun 19 Priority B7 New

(Continued)

Group 77 - Airport & Transit Improvements FY14-15 CIP Adopted

Project #	Project Title	Project \$ Total	Funding Sources	Prior Years Expenditures	FY13-14 Appropriations	NEW APPROPRIATIONS REQUIRED					Anticipated Completion & Comments	
						Total	FY14-15	FY15-16	FY16-17	FY17-18		FY18-19
FUTURE PROJECTS (Continued)												
77PP- 060	Sanitary Improvements Tracy Airport	291,400	F563-Airport Capital F301-General Projects	0	0	0	0	0	0	0	291,400	0 Jun 19 Priority B6 Upgrade
77PP- 067	Playground Equipment - Tracy Airport Park	100,000	F563-Airport Capital	0	0	0	0	0	0	0	100,000	0 Jun 19 Priority B8 Upgrade
77PP- 070	Airport Improvements Tracy Airport - FY13	122,000	F563-Airport Capital	0	0	122,000	0	0	0	0	0	0 Jun 16 Priority A2 Upgrade & Rehab
77PP- 071	Runway Seal Coat - Tracy Airport	310,000	F563-Airport Capital FAA Grant	0	0	10,000	300,000	0	0	0	0	0 Jun 16 Priority A5 Rehabilitation
77PP- 072	Airport Site Selection Study	250,000	F301-General Projects FAA Grant	0	0	25,000	225,000	0	0	0	0	0 Jun 16 Priority A6 Study
77PP- 073	Airport Improvements Tracy Airport - FY13	110,000	F563-Airport Capital FAA Grant	0	0	10,000	100,000	0	0	0	0	0 Jun 16 Priority A9 Rehabilitation
77PP- 074	Removal of Aligned Taxiway - Tracy Airport	110,000	F563-Airport Capital FAA Grant	0	0	10,000	100,000	0	0	0	0	0 Jun 16 Priority A9 Rehabilitation
77PP- 075	Sweeper Purchase Tracy Airport - FY13	115,000	F563-Airport Capital	0	0	0	115,000	0	0	0	0	0 Jun 16 Priority B3
77PP- 076	Upgrade AWOS & Unicom - Tracy Airport	105,000	F563-Airport Capital FAA Grant	0	0	5,000	100,000	0	0	0	5,000	0 Jun 19 Priority B2 Rehabilitation
77PP- 077	Vegetation Removal Tracy Airport	55,000	F563-Airport Capital	0	0	55,000	0	0	0	0	0	0 Dec 16 Priority A17 Rehabilitation
77PP- 078	Relocate Perimeter Fencing - Tracy Airport	111,000	F563-Airport Capital FAA Grant	0	0	11,000	100,000	0	0	0	11,000	0 Jun 19 Priority C1 Rehabilitation

(Continued)

Group 77 - Airport & Transit Improvements FY14-15 CIP Adopted

Project #	Project Title	Project \$ Total	Funding Sources	Prior Years Expenditures	FY13-14 Appropriations	NEW APPROPRIATIONS REQUIRED					Anticipated Completion & Comments	
						Total	FY14-15	FY15-16	FY16-17	FY17-18		FY18-19
FUTURE PROJECTS (Continued)												
77PP- 079	Overlay of Runways - Tracy Airport	7,100,000	F563-Airport Capital FAA Grant	0	0	100,000	0	0	0	0	100,000	Jun 19 Priority B1 Rehabilitation
77PP- 080	Install New Lights Tracy Airport	75,000	F563-Airport Capital FAA Grant	0	0	10,000	0	0	0	0	10,000	Jun 19 Priority B4 New Equipment
77PP- 562	ParaTransit Bus Replacements - Future Years	1,200,000	F573-Transit Capital FTA Grant	0	0	240,000	0	120,000	120,000	0	0	Jun 17 Priority B2 Replacement
77PP- 563	Transit Buses Replacements - Future Years	1,400,000	F573-Transit Capital FTA Grant	0	0	280,000	0	140,000	140,000	0	0	Jun 17 Priority B2 Replacement
77PP- 566	WI-FI Access on TRACER Buses	20,000	F573-Transit Capital FTA Grant	0	0	20,000	0	0	0	0	20,000	Jun 19 Priority C1 New Equipment
Totals						57,202,600	0	4,702,000	1,470,000	0	51,030,600	
26	Future Projects	57,202,600		0	0	57,202,600	0	4,702,000	1,470,000	0	51,030,600	

Group 78 - Parks & Recreation Improvements

FY14-15 CIP Adopted

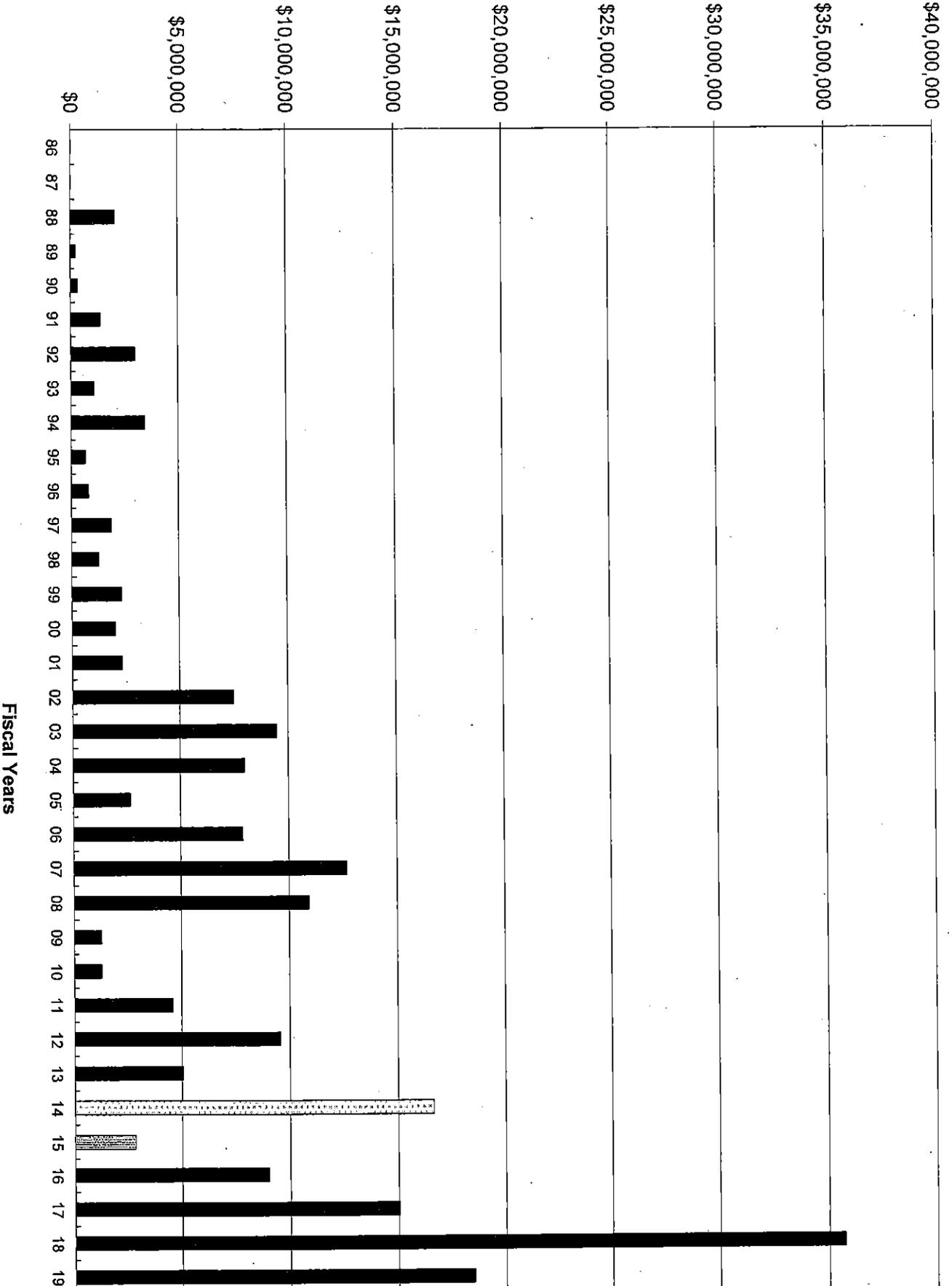
by Project Type	Group \$ Total	Prior Years Expenditures	FY13-14 Appropriations	Total	NEW APPROPRIATIONS REQUIRED					Projects Requiring New Funding in FY14-15	
					FY14-15	FY15-16	FY16-17	FY17-18	FY18-19		
15 Current Projects	31,560,796	15,833,114	16,661,682	-934,000	-1,909,000	975,000	0	0	0	0	1 New Funding in FY14-15
5 New Projects	4,744,600	0	0	4,744,600	4,744,600	0	0	0	0	0	
28 Future Projects	77,535,722	162,352	0	77,373,370	0	8,056,600	15,046,600	35,729,670	18,540,500	0	
48 Totals	113,841,118	15,995,466	16,661,682	81,183,970	2,836,600	9,031,600	15,046,600	35,729,670	18,540,500	0	

by Funding Sources	Group \$ Total	Prior Years Expenditures	FY13-14 Appropriations	Total	Approved Capital Budget	NEW APPROPRIATIONS REQUIRED					Projects Requiring New Funding in FY14-15
						FY14-15	FY15-16	FY16-17	FY17-18	FY18-19	
F242-Transp Sales Tax	400,000	0	0	400,000	0	200,000	0	200,000	0	0	
F268-Com Dev Block G	327,856	225,336	36,720	65,800	65,800	0	0	0	0	0	
F271-Landscaping Distr	1,070,000	26,989	211,011	832,000	832,000	0	0	0	0	0	
F301-General Projects	56,069,882	8,125,802	3,802,880	44,141,200	1,937,800	7,387,400	3,398,500	17,653,000	13,764,500	0	
F311-Infill Parks	5,303,000	0	0	5,303,000	0	0	527,000	0	4,776,000	0	
F318-Redev Projects	3,813,410	3,693,276	120,134	0	0	0	0	0	0	0	
F321-Parks Plan "C"	1,648,000	1,648,000	0	0	0	0	0	0	0	0	
F324-Gen Fac Plan "C"	4,016,200	1,093,535	1,692,465	1,260,200	0	400,000	860,200	0	0	0	
F345-RSP Prgm Mgmt	131,500	0	0	131,500	0	131,500	0	0	0	0	
F352-So MacArthur PA	1,157,800	878,000	138,800	141,000	0	141,000	0	0	0	0	
F353-1205 Area Spec Pl	572,500	0	0	572,500	0	0	572,500	0	0	0	
F354-ISP South Area	478,000	0	231,500	246,500	0	0	246,500	0	0	0	
F355-Presidio Area	230,400	0	114,700	115,700	0	0	115,700	0	0	0	
F3xx-Ellis Prgm Area	17,426,670	0	0	17,426,670	0	0	0	17,426,670	0	0	
F391-Kagehito Parks	457,000	0	310,000	147,000	0	147,000	0	0	0	0	
State Park Grant	100,000	0	0	100,000	0	100,000	0	0	0	0	
Other Grants	8,120,000	173,028	33,472	7,913,500	0	0	7,463,500	450,000	0	0	
Developer's Contributor	10,797,200	131,500	10,000,000	665,700	0	665,700	0	0	0	0	
Future Developments	1,721,700	0	0	1,721,700	0	0	1,721,700	0	0	0	
	113,841,118	15,995,466	16,661,682	81,183,970	2,836,600	9,031,600	15,046,600	35,729,670	18,540,500	0	

CIP Expenditures	in FY12-13 >>	in FY11-12 >>	in FY10-11 >>	in FY09-10 >>	New Appropriations	Carryovers from FY13	Deferrals	Supplementals
	5,054,442	9,583,543	4,596,373	1,265,090	11,215,200	-350,000	19,450	

Tracy Capital Improvement Program
Parks & Recreation Improvements

Actual & Projected Expenditures



Group 78 - Parks & Recreation Improvements

FY14-15 CIP Adopted

Project #	Project Title	Project \$ Total	Funding Sources	Prior Years Expenditures	FY13-14 Appropriations	Total	NEW APPROPRIATIONS REQUIRED					Anticipated Completion & Comments	
							FY14-15	FY15-16	FY16-17	FY17-18	FY18-19		
CURRENT PROJECTS (Continued)													
78143	Handball Court Refinish MacDonald Park	34,500	F301-General Projects	0	34,500	0	0	0	0	0	0	0	Dec 14 Contract Award Jun 14
78144	Restroom Replacement MacDonald Park	476,200	F301-General Projects	0	476,200	0	0	0	0	0	0	0	Dec 14 Contract Award Jun 14
78145	Facility Reservation Software	40,000	F301-General Projects Other Grant	0	20,000 20,000	0	0	0	0	0	0	0	Jun 14 Priority B Software Upgrade
78146	New Basketball Court - El Pescadero Park	65,000	F301-General Projects	0	65,000	0	0	0	0	0	0	0	Dec 14 Contract Award Jun 14
78151	Emergency Repairs - Lammersville School	85,000	F301-General Projects	0	85,000	0	0	0	0	0	0	0	Dec 14 Priority B Rehabilitation
Totals													
15	Current Projects	31,560,796		15,833,114	16,861,882	-934,000	-1,909,000	975,000	0	0	0	0	

Group 78 - Parks & Recreation Improvements

FY14-15 CIP Adopted

Project #	Project Title	Project \$ Total	Funding Sources	Prior Years Expenditures	FY13-14 Appropriations	NEW APPROPRIATIONS REQUIRED					Anticipated Completion & Comments		
						Total	FY14-15	FY15-16	FY16-17	FY17-18		FY18-19	
NEW PROJECTS													
78147	Replace Automatic Door at Tracy Library	85,600	F301-General Projects F268-Com Dev Block G	0	0	19,800	19,800	0	0	0	0	0	Jun 15 Priority B Replacement
78148	Wooden Play Structures Hoyt Park	304,000	F301-General Projects	0	0	304,000	304,000	0	0	0	0	0	Jun 15 Priority B12 Replacement
78149	Park Renovation - LMZ 07, Bailor-Hennan, Rippon	416,000	F271-Landscaping Dist	0	0	416,000	416,000	0	0	0	0	0	Jun 16 Priority B13 Rehabilitation
78150	Park Renovation - LMZ 17, Sullivan & Huck	416,000	F271-Landscaping Dist	0	0	416,000	416,000	0	0	0	0	0	Jun 16 Priority B14 Rehabilitation
78152	Pool Replacement - Dr Powers Park	3,523,000	F301-General Projects	0	0	3,523,000	3,523,000	0	0	0	0	0	Jun 16 Priority B Replacement
Totals						4,744,600	4,744,600	0	0	0	0	0	
5 New Projects						4,744,600	4,744,600	0	0	0	0	0	

Group 78 - Parks & Recreation Improvements

FY14-15 CIP Adopted

Project #	Project Title	Project \$ Total	Funding Sources	Prior Years Expenditures	FY13-14 Appropriations	Total	NEW APPROPRIATIONS REQUIRED					Anticipated Completion & Comments			
							FY14-15	FY15-16	FY16-17	FY17-18	FY18-19				
FUTURE PROJECTS															
78088	Library Facility Expans Unknown Location	4,012,100	F311-Infill Parks F324-Gen Fac Plan "C" F352-So MacArthur PA F354-ISP South Area F355-Presidio Area Future Developments	0	0	527,000	0	527,000	0	0	0	0	0	Jun 17	Priority A6 Expansion Deferred to Future
78093	Park Expansion - Tracy Press Park	131,500	F345-RSP Prgm Mgmt Developer's Contributio	0	0	131,500	0	131,500	0	0	0	0	0	Dec 05	Reimbursement Due
78124	Dog Park Site - South Tracy	310,000	F301-General Projects F391-Kagehiro Parks	0	0	163,000	0	163,000	0	0	0	0	0	Jun 16	Priority B New Facility
78PP- 002	Ballpark Renovations - Tracy Ball Park, Phase II	3,465,852	F301-General Projects State Park Grant	30,862	0	3,435,000	0	3,435,000	0	0	0	0	0	Dec 16	Priority A2 Deferred to Future
78PP- 018	Park Development - El Pescadero Park, Phase II	797,200	F311-Infill Parks Developer's Contributio	0	0	797,200	0	797,200	0	0	0	0	0	Jun 16	Rehab & Expan
78PP- 079	Park Renovation - Dr Powers Park	1,334,000	F301-General Projects State Park Grant	0	0	1,334,000	0	1,334,000	0	0	0	0	0	Jun 16	Priority A3 Rehabilitation
78PP- 086	Bikeway Improvements Future Phases	590,000	F301-General Projects Bikeway Grants	0	0	190,000	0	95,000	0	95,000	0	0	0	Jun 18	Priority A4 New & Upgrade
78PP- 108	Park Revitalization - Cily Areas - Future Phases	340,000	F301-General Projects	0	0	340,000	0	85,000	85,000	85,000	0	0	0	Annual Phased Program	Rehabilitation
78PP- 118	New Gymnasium/Mull Purpose Facility	10,788,500	F301-General Projects Grant Funding	0	0	3,600,000	0	760,000	2,840,000	0	0	0	0	Oct 17	Priority B3 New Facility
78PP- 119	Scoreboard Replaceme Tracy Sports Complex	35,000	F301-General Projects	0	0	35,000	0	35,000	0	0	0	0	0	Jun 16	Priority C5 Replacement
(Continued)						F73									

Group 78 - Parks & Recreation Improvements

FY14-15 CIP Adopted

Project #	Project Title	Project \$ Total	Funding Sources	Prior Years Expenditures	FY13-14 Appropriations	Total	NEW APPROPRIATIONS REQUIRED					Anticipated Completion & Comments	
							FY14-15	FY15-16	FY16-17	FY17-18	FY18-19		
FUTURE PROJECTS (Continued)													
78PP- 123	Neighborhood Park - Location to be Determined	4,776,000	F311-Infill Parks	0	0	4,776,000	0	0	0	0	4,776,000	Jun 19	Priority D New Facilities
78PP- 124	Bicycle Motocross Park	381,300	F301-General Projects	0	0	381,300	0	0	37,500	343,800	0	Jun 18	Priority C11 New Facility
78PP- 125	Skate Park - 2nd Location	293,700	F301-General Projects	0	0	293,700	0	0	24,500	269,200	0	Jun 18	Priority C9 New Facility
78PP- 128	Swainson Hawk Mitigation - 1205 Area, Phase II	572,500	F353-1205 Area Spec P	0	0	572,500	0	0	572,500	0	0	Jun 17	Priority C12 New Facilities
78PP- 135	Bikeway Rehabilitation - Various Locations	177,500	F301-General Projects Bikeway Grants	0	0	32,500 145,000	0	0	32,500 145,000	0	0	Jun 17	Priority C14 Rehabilitation
78PP- 136	Pool Demolition Dr Powers Park	393,500	F301-General Projects State Park Grant	0	0	393,500	0	0	0	0	393,500	0	Optional
78PP- 137	Youth Sports Facilities - Legacy Sportsfield, Phase II	16,000,000	F301-General Projects State Park Grant	0	0	16,000,000	0	0	0	16,000,000	0	Jun 18	Priority C13 New Facilities
78PP- 139	Park Renovation - Lincoln Park, Phase II	379,000	F301-General Projects	0	0	379,000	0	0	379,000	0	0	Jun 17	Priority B1 Rehabilitation
78PP- 146	Ballpark Renovations - Tracy Ball Park, Phase III	4,856,000	F301-General Projects State Park Grant	0	0	4,856,000	0	0	0	860,000	3,996,000	Jun 19	Priority B15 Rehabilitation
78PP- 147	New 50 Meter Pool Dr Powers Park - Option II	9,290,000	F301-General Projects	0	0	9,290,000	0	0	0	0	9,290,000	Jun 19	Priority D New Facilities
78PP- 149	Bikeways to Holly Legacy Sports Fields	580,000	F301-General Projects Bikeway Grants	0	0	0 580,000	0	0	0 130,000	0 450,000	0	Jun 18	Priority D New Facilities
78PP- 150	Utilities Extension - Legacy Sports Fields	330,000	F301-General Projects	0	0	330,000	0	0	330,000	0	0	Jun 16	Priority B New Facilities
(Continued)													
						F74							

Group 78 - Parks & Recreation Improvements

FY14-15 CIP Adopted

Project #	Project Title	Project \$ Total	Funding Sources	Prior Years Expenditures	FY13-14 Appropriations	Total	NEW APPROPRIATIONS REQUIRED					Anticipated Completion & Comments			
							FY14-15	FY15-16	FY16-17	FY17-18	FY18-19				
<u>FUTURE PROJECTS (Continued)</u>															
78PP- 151	Fish Pond Development Legacy Sports Fields	130,000	F301-General Projects State Park Grant	0	0	30,000	0	30,000	0	0	0	0	0	Jun 16	Priority B New Facilities
78PP- 153	Minor Improvements	145,400	F301-General Projects Other Grant	0	0	145,400	0	145,400	0	0	0	0	0	Jun 16	Priority B New Facilities
78PP- 201	Neighborhood Parks Ellis Prgm Area	11,729,450	F3xx-Ellis Prgm Area Future Developments	0	0	11,729,450	0	0	0	11,729,450	0	0	0	Jun 18	
78PP- 202	Community Park Ellis Prgm Area	3,372,010	F3xx-Ellis Prgm Area Future Developments	0	0	3,372,010	0	0	0	3,372,010	0	0	0	Jun 18	
78PP- 203	Community Center Ellis PA	1,206,810	F3xx-Ellis Prgm Area F301-General Projects	0	0	1,206,810	0	0	0	1,206,810	0	0	0	Jun 18	Priority C New Facility
78PP- 204	Library Ellis PA	1,118,400	F3xx-Ellis Prgm Area F301-General Projects	0	0	1,118,400	0	0	0	1,118,400	0	0	0	Jun 18	Priority C New Facility
Totals						77,536,722	0	8,056,600	15,046,600	35,729,670	18,540,500	0			
28	Future Projects	162,362		0	0	77,373,370	0	8,056,600	15,046,600	35,729,670	18,540,500	0			

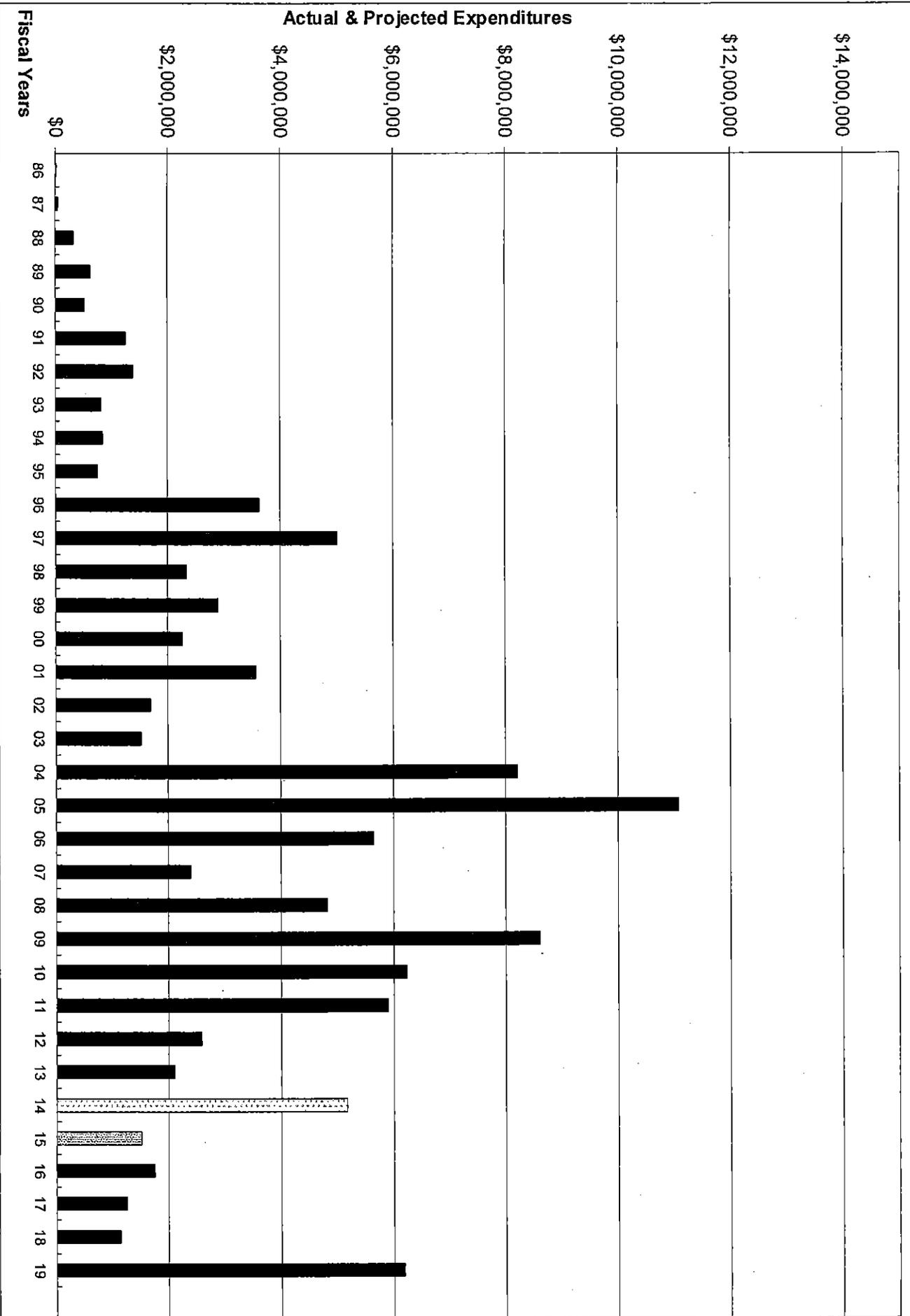
Group 79 - Miscellaneous Projects

FY14-15 CIP Adopted

Group by Project Type	Group \$ Total	Prior Years		NEW APPROPRIATIONS REQUIRED					Projects Requiring New Funding in FY13-14	
		Expenditures	FY13-14 Appropriations	Total	FY14-15	FY15-16	FY16-17	FY17-18		FY18-19
30 Current Projects	34,275,339	20,524,700	5,173,381	8,577,258	670,000	847,500	749,229	635,000	5,675,529	9 New Funding 1 in FY13-14
2 New Projects	850,000	0	0	850,000	850,000	0	0	0	0	
4 Future Projects	2,440,000	0	0	2,440,000	0	910,000	510,000	510,000	510,000	
36 Totals	37,565,339	20,524,700	5,173,381	11,867,258	1,520,000	1,757,500	1,259,229	1,145,000	6,185,529	
by Funding Sources										
F101-General	1,077,877	1,077,877	0	0	0	0	0	0	0	
F301-General Projects	3,493,016	1,774,837	985,679	732,500	220,000	512,500	0	0	0	
F345-RSP Prgm Mgmt	2,554,889	221,251	2,333,638	0	0	0	0	0	0	
F351-NE Indus Area #1	2,278,821	2,111,410	49,817	117,594	0	50,000	67,594	0	0	
F352-SMPA	383,989	242,486	44,868	96,635	0	50,000	46,635	0	0	
F353-I205 Area Spec Pl	802,217	781,839	20,378	0	0	75,000	0	0	0	
F354-ISP South	1,566,532	476,849	63,783	1,025,900	0	0	75,000	75,000	800,900	
F355-Presidio Area	177,796	90,839	86,957	0	0	0	0	0	0	
F356-Tracy Gateway	1,653,510	58,962	340,548	1,254,000	0	100,000	100,000	100,000	954,000	
F357-NE Indus Area #2	2,226,094	467,221	161,273	1,597,600	0	80,000	80,000	80,000	1,357,600	
F3xx-Ellis Prgm Area	2,743,420	0	0	2,743,420	0	200,000	200,000	200,000	2,143,420	
F318-Comm Dovel/Agri	1,942,000	1,890,336	51,664	0	0	0	0	0	0	
F391-UIMP Facilities	12,101,904	10,285,867	519,978	1,296,059	800,000	100,000	100,000	100,000	196,059	
F605-Eqpt Acq	1,760,000	0	420,000	1,340,000	500,000	210,000	210,000	210,000	210,000	
State & Local Grants	0	0	0	0	0	0	0	0	0	
Developer's Contributor	2,803,274	1,044,926	94,798	1,663,550	0	380,000	380,000	380,000	523,550	
	37,565,339	20,524,700	5,173,381	11,867,258	1,520,000	1,757,500	1,259,229	1,145,000	6,185,529	

CIP Expenditures		New Appropriations	
In FY12-13 >>	2,113,913	1,486,000	Carryovers from FY13
In FY11-12 >>	2,629,994	3,688,381	Revisions & Deferrals
In FY10-11 >>	5,900,745	0	Supplementals
In FY09-10 >>	6,233,636	0	

Tracy Capital Improvement Program
Miscellaneous Projects



Group 79 - Miscellaneous Projects

FY14-15 CIP Adopted

Project #	Project Title	Project \$ Total	Funding Sources	Prior Years Expenditures	FY13-14 Appropriations	Total	NEW APPROPRIATIONS REQUIRED					Anticipated Completion & Comments	
							FY14-15	FY15-16	FY16-17	FY17-18	FY18-19		
CURRENT PROJECTS													
79201	Infill Program Management	962,657	F31X-Infill Funds	404,309	94,798	463,550	0	80,000	80,000	80,000	223,550	0	Jun 22 On-going Program Annual Contingency
79203	I205 Area Program Management	802,217	F353-I205 Area Develop	781,839	20,378	0	0	0	0	0	0	0	Jun 14 On-going Program Annual Contingency
79204	Plan "C" Program Management	5,092,511	F391-UIMP Facilities	4,482,448	114,004	496,059	0	100,000	100,000	100,000	196,059	0	Jun 20 On-going Program Annual Contingency
79205	Industrial SP South, Prig Management	1,803,512	Developer's Contributor F354-Indus SP, South	236,980	63,783	0	0	0	0	0	0	0	Jun 20 On-going Program Annual Contingency
79206	NE Industrial Area #1 - Program Management	2,342,326	F351-NE Indus Area #1 Developer's Contributor	2,111,410	63,505	117,594	0	50,000	67,594	0	0	0	Jun 17 On-going Program Annual Contingency
79207	South MacArthur Area - Program Management	383,989	F352-SMPA	242,486	44,868	96,635	0	50,000	46,635	0	0	0	Jun 17 On-going Program Annual Contingency
79208	NE Industrial Area #2 - Program Management	2,226,094	F357-NE Indus Area #2 Developer's Contributor	467,221	161,273	1,597,600	0	80,000	80,000	80,000	1,357,600	0	Jun 20 On-going Program Annual Contingency
79209	Tracy Gateway - Program Management	1,653,510	F356-Tracy Gateway Developer's Contributor	56,962	340,548	1,254,000	0	100,000	100,000	100,000	954,000	0	Jun 20 On-going Program Annual Contingency
79210	Presidio Area - Program Management	437,808	F365-Presidio Area Developer's Contributor	90,839	86,957	0	0	0	0	0	0	0	Jun 13 On-going Program Annual Contingency
79211	Ellis Area - Program Management	2,743,420	F3XX-Ellis Area Developer's Contributor	0	0	2,743,420	0	200,000	200,000	200,000	2,143,420	0	Jun 25 On-going Program Annual Contingency
79310	Development Reviews - FY10 Projects	1,428,279	Developer's Contributor F391-UIMP Facilities	746,174	-35,669	0	0	0	0	0	0	0	Annual Contingency New Developments
79311	Development Reviews - FY11 Projects	998,769	Developer's Contributor	909,634	89,135	0	0	0	0	0	0	0	Annual Contingency New Developments

(Continued)

Group 79 - Miscellaneous Projects

FY14-15 CIP Adopted

Project #	Project Title	Project \$ Total	Funding Sources	Prior Years Expenditures	FY13-14 Appropriations	Total	NEW APPROPRIATIONS REQUIRED					Anticipated Completion & Comments	
							Approved Capital Budget	FY14-15	FY15-16	FY16-17	FY17-18		FY18-19
CURRENT PROJECTS (Continued)													
79312	Development Reviews - FY12 Projects	80,320	Developer's Contribution	80,320	0	0	0	0	0	0	0	0	Annual Contingency New Developments
79313	Development Reviews - FY13 Projects	215,000	Developer's Contribution	10,809	204,191	0	0	0	0	0	0	0	Annual Contingency New Developments
79314	Development Reviews - FY14 Projects	300,000	Developer's Contribution	0	300,000	0	0	0	0	0	0	0	Annual Contingency New Developments
79351	General Plan Update	1,343,032	F101-General F301-General Projects F345-RSP Prgm Mgmt	1,016,353 265,795 24,639	0 36,245 0	0	0	0	0	0	0	0	Jun 13 Work Completed
79352	Zoning Code Update	400,000	F101-General F301-General Projects	61,524 279,544	0 58,932	0	0	0	0	0	0	0	Jun 13 Work Completed
79355	Infrastructure Master Pl.	3,267,345	F391-JUMP Facilities F345-RSP Prgm Mgmt	2,526,294 796,216	55,165 0	0	0	0	0	0	0	0	Jun 13 Work Completed
79356	Downtown Tracy Specific Plan	1,304,874	F318-Comm Devel Age	1,253,210	51,664	0	0	0	0	0	0	0	Jun 13 Work Completed
79357	Way Finding Signage Program	435,000	F101-General F301-General Projects	0 90,653	0 344,347	0	0	0	0	0	0	0	Jun 15 Design Underway
79364	Downtown Brew Pub/ Property Acquisition	1,637,126	F318-Comm Devel Age F345-RSP Prgm Mgmt	637,126 39,658	0 960,342	0	0	0	0	0	0	0	Jun ???
79365	Business Incubator	61,517	F345-RSP Prgm Mgmt	53,530	7,987	0	0	0	0	0	0	0	Project Cancelled
79366	Retail Incentives - Office/Industrial	35,000	F101-General F345-RSP Prgm Mgmt	0 2,745	0 32,255	0	0	0	0	0	0	0	Jun ???
(Continued)						F79							

Group 79 - Miscellaneous Projects

FY14-15 CIP Adopted

Project #	Project Title	Project \$ Total	Funding Sources	Prior Years Expenditures	FY13-14 Appropriations	Total	NEW APPROPRIATIONS REQUIRED					Anticipated Completion & Comments	
							FY14-15	FY15-16	FY16-17	FY17-18	FY18-19		
NEW PROJECTS													
79315	Development Reviews - FY14-15	800,000	Developer's Contribution	0	0	800,000	800,000	0	0	0	0	0	Annual Contingency New Developments
79413	Computer Replacement Citywide - FY14-15	50,000	F605-Eqpt Acq	0	0	50,000	50,000	0	0	0	0	0	Jun 15 Priority A Equipment Replacement
Totals						850,000	850,000	0	0	0	0	0	
New Projects						850,000	850,000	0	0	0	0	0	

Group 79 - Miscellaneous Projects

FY14-15 CIP Adopted

Project #	Project Title	Project \$ Total	Funding Sources	Prior Years Expenditures	FY13-14 Appropriations	NEW APPROPRIATIONS REQUIRED					Anticipated Completion & Comments	
						Total	FY14-15	FY15-16	FY16-17	FY17-18		FY18-19
FUTURE PROJECTS												
79PP- 001b	Development Reviews - Future Projects	1,200,000	Developer's Contribution	0	0	1,200,000	0	300,000	300,000	300,000	300,000	Annual Contingency New Developments
79PP- 031b	Computer Replacement Citywide - Future Years	840,000	F605-Eqpt Acq	0	0	840,000	0	210,000	210,000	210,000	210,000	Annual Contingency Equipment Replacement
79PP- 051	Infrastructure Managemen Software	300,000	F301-General Projects	0	0	300,000	0	300,000	0	0	0	Jun 15 Priority B
79PP- 058	Housing Element Update	100,000	F301-General Projects	0	0	100,000	0	100,000	0	0	0	Jun 15 Priority B
Totals						2,440,000	0	910,000	510,000	510,000	510,000	
4	Future Projects	2,440,000		0	0	2,440,000	0	910,000	510,000	510,000	510,000	

Group 799FR - Interfund CIP Reimbursements FY14-15 CIP Adopted

by Project Type	Group \$ Total	Prior Years Expenditures	FY13-14 Appropriations	Total	NEW APPROPRIATIONS REQUIRED				
					FY14-15	FY15-16	FY16-17	FY17-18	FY18-19
4 Reimbursement Project:	0	0	0	0	0	0	0	0	0
Approved Capital Budget									
	0	0	0	0	0	0	0	0	0
by Funding Sources									
F101-General	0	0	0	0	0	0	0	0	0
F322-Drainage Plan "C"	4,701,000	4,701,000	0	0	0	0	0	0	0
F323-Arterials Plan "C"	6,781,100	6,781,100	0	0	0	0	0	0	0
F325-Utillities - Plan "C"	5,616,300	6,390,500	0	-774,200	0	-774,200	0	0	0
F342-RSP Drainage	0	0	0	0	0	0	0	0	0
F343-RSP Arterials	-406,500	-406,500	0	0	0	0	0	0	0
F345-RSP Prgm Mgmt	-13,574,700	-11,984,100	0	-1,590,600	0	-795,300	-795,300	0	0
F351-NE Indus Area #1	12,416,000	6,593,000	0	5,823,000	0	4,720,200	1,102,800	0	0
F352-So MacArthur PA	323,450	323,450	0	0	0	0	0	0	0
F353-1205 Area Spec Pl	6,198,600	1,333,300	0	4,865,300	0	2,970,000	1,895,300	0	0
F354-Indus SP, South	774,200	0	0	774,200	0	0	774,200	0	0
F355-Residio Area	1,325,700	1,325,700	0	0	0	0	0	0	0
F356-Tracy Gateway Ar	738,800	0	0	738,800	0	738,800	0	0	0
F357-NE Indus Area #2	9,204,000	9,204,000	0	0	0	0	0	0	0
F513-Water	-15,978,900	-13,588,000	0	-2,390,900	0	-2,390,900	0	0	0
F523-Wastewater	-12,762,800	-5,317,200	0	-7,445,600	0	-5,242,800	-2,202,800	0	0
F834-AD 84-1 Debt	-3,418,900	-3,418,900	0	0	0	0	0	0	0
F835-CFD 89-1	-1,937,350	-1,937,350	0	0	0	0	0	0	0
Developer's Contributor	0	0	0	0	0	0	0	0	0
	0	0	0	0	0	0	0	0	0

Group 799 - Interfund CIP Reimbursements

FY14-15 CIP Adopted

Project #	Project Title	Project \$ Total	Funding Sources	Prior Years Expenditures	FY13-14 Appropriations	NEW APPROPRIATIONS REQUIRED					Anticipated Completion & Comments	
						Total	FY14-15	FY15-16	FY16-17	FY17-18		FY18-19
REIMBURSEMENT PROJECTS												
79911	Arterial CIP Reimbursements Was 7347	0	F323-Arterials Plan *C* F835-CFD 89-1 F343-RSP Arterials	6,781,100 -463,000 -406,500	0 0 0	0	0	0	0	0	0	0 Jun 07 Reimbursements Reimbursements
79912	Wastewater CIP Reimbursements Was 7420	0	F353-1205 Area Spec P F523-Wastewater F834-AD 84-1 Debt F325-Uilities - Plan *C* F362-So MacArthur PA F361-NE Indus Area #1 F355-Residio Area F356-Tracy Gateway Ar F357-NE Indus Area #2 F835-CFD 89-1	1,333,300 -5,317,200 -3,418,900 3,753,500 233,600 3,423,500 586,500 0 677,000 -1,271,300	0 0 0 0 0 0 0 0 0 0	2,604,000 -7,445,600 0 0 0 4,102,800 0 738,800 0 0	1,504,000 -5,242,800 0 0 0 3,000,000 0 738,800 0 0	1,100,000 -2,202,800 0 0 0 1,102,800 0 0 0 0	0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0	0 Jun 16 Priority B Reimbursements Reimbursements Reimbursements Priority A Priority B Reimbursements	
79913	Water CIP Reimbursements	0	F353-1205 Area Spec P F513-Water F325-Uilities - Plan *C* F351-NE Indus Area #1 F354-Indus SP, South F357-NE Indus Area #2	0 -13,588,000 2,637,000 2,600,000 0 8,351,000	0 0 0 0 0 0	670,700 -2,390,900 -774,200 1,720,200 774,200 0	670,700 -2,390,900 0 1,720,200 0 0	0 0 -774,200 0 774,200 0	0 0 0 0 0 0	0 0 0 0 0 0	0 Jun 15 Priority B Reimbursements Reimbursements Priority A Priority C Priority A	
79914	Drainage CIP Reimbursements	0	F322-Drainage Plan *C* F345-RSP Prgm Mgmt F351-NE Indus Area #1 F352-So MacArthur PA F353-1205 Area Spec P F355-Residio Area F357-NE Indus Area #2 F835-CFD 89-1 F345-RSP Prgm Mgmt	4,701,000 -739,200 58,200 89,850 0 739,200 64,200 -203,050 -4,710,200	0 0 0 0 0 0 0 0 0	-1,590,600 0 0 0 1,590,600 0 0 0 0	-795,300 0 0 0 795,300 0 0 0 0	-795,300 0 0 0 795,300 0 0 0 0	0 0 0 0 0 0 0 0 0	0 Jun 07 Reimbursements 0 Jun 06 Reimbursements 0 Jun 06 Priority C 0 Jun 07 Priority A 0 Jun 09 Reimbursements Reimbursements		
Totals		0			0	0	0	0	0	0	0	0
4 Reimbursement Project:		0			0	0	0	0	0	0	0	0

RESOLUTION 2014-_____

REPORT OF CONSISTENCY WITH THE CITY OF TRACY'S GENERAL PLAN GOALS,
POLICIES AND ACTIONS FOR THE CAPITAL IMPROVEMENT PROGRAM FOR FISCAL
YEAR 2014/2015 THROUGH FISCAL YEAR 2018/2019
APPLICATION NUMBER DET14-001

WHEREAS, City staff provided a proposed Capital Improvement Program to City Council in 2014 for budget adoption for Fiscal Year 2014/2015 through Fiscal Year 2018/2019, and

WHEREAS, City staff has analyzed the Capital Improvement Program to ensure the consistency of the proposed projects within the Capital Improvement Program with the goals, policies, and actions of the City of Tracy's current General Plan, and

WHEREAS, The Planning Commission conducted a public meeting to review and consider the Capital Improvement Program on August 27, 2014;

NOW, THEREFORE BE IT RESOLVED, The Planning Commission hereby reports that the Capital Improvement Program for Fiscal Year 2014/2015 through Fiscal Year 2018/2019 is consistent with the City's General Plan goals, policies and actions.

The foregoing Resolution 2014-_____ was adopted by the Planning Commission on the 27th day of August, 2014, by the following vote:

AYES: COMMISSION MEMBERS:
NOES: COMMISSION MEMBERS:
ABSENT: COMMISSION MEMBERS:
ABSTAIN: COMMISSION MEMBERS:

Chair

ATTEST:

Staff Liaison