

## NOTICE OF A REGULAR MEETING

Pursuant to Section 54954.2 of the Government Code of the State of California, a Regular meeting of the City of Tracy **Planning Commission** is hereby called for:

**Date/Time:** Wednesday, April 10, 2013  
7:00 P.M. (or as soon thereafter as possible)

**Location:** City of Tracy Council Chambers  
333 Civic Center Plaza

Government Code Section 54954.3 states that every public meeting shall provide an opportunity for the public to address the Planning Commission on any item, before or during consideration of the item, however no action shall be taken on any item not on the agenda.

### REGULAR MEETING AGENDA

CALL TO ORDER

PLEDGE OF ALLEGIANCE

ROLL CALL

MINUTES APPROVAL

DIRECTOR'S REPORT REGARDING THIS AGENDA

ITEMS FROM THE AUDIENCE - *In accordance with Procedures for Preparation, Posting and Distribution of Agendas and the Conduct of Public Meetings, adopted by Resolution 2008-140, any item not on the agenda brought up by the public at a meeting, shall be automatically referred to staff. If staff is not able to resolve the matter satisfactorily, the item shall be placed on an agenda within 30 days*

1. OLD BUSINESS

2. NEW BUSINESS

**A. PUBLIC HEARING TO CONSIDER APPROVAL OF AN 8-LOT TENTATIVE SUBDIVISION MAP ON A 1.2-ACRE PARCEL LOCATED AT THE SOUTH END OF ALHAMBRA AND GIBSON COURTS FOR THE CONSTRUCTION OF EIGHT SINGLE-FAMILY HOMES. THE APPLICANT IS MACKAY AND SOMPS AND OWNER IS STANDARD PACIFIC HOMES- APPLICATION NUMBER TSM11-0001**

**B. PLANNING COMMISSION STUDY SESSION REGARDING THE DRAFT CORDES RANCH SPECIFIC PLAN**

3. ITEMS FROM THE AUDIENCE

4. DIRECTOR'S REPORT

5. ITEMS FROM THE COMMISSION

6. ADJOURNMENT

### **April 4, 2013**

Posted date

The City of Tracy complies with the Americans with Disabilities Act and makes all reasonable accommodations for the disabled to participate in public meetings. Persons requiring assistance or auxiliary aids in order to participate should call City Hall (209-831-6000), at least 24 hours prior to the meeting.

Any materials distributed to the majority of the Planning Commission regarding any item on this agenda will be made available for public inspection in the Development and Engineering Services department located at 333 Civic Center Plaza during normal business hours.

**MINUTES**  
**TRACY CITY PLANNING COMMISSION**  
**FEBRUARY 27, 2013**  
**7:00 P.M.**  
**TRACY COUNCIL CHAMBERS**  
**333 CIVIC CENTER PLAZA**

*In accordance with Procedures for Preparation, Posting and Distribution of Agendas and the Conduct of Public Meetings, adopted by Resolution 2008-140 any item not on the agenda brought up by the public at a meeting, shall be automatically referred to staff. If staff is not able to resolve the matter satisfactorily, the member of the public may request a Planning Commission Member to sponsor the item for discussion at a future meeting.*

\* \* \* \* \*

**CALL TO ORDER:** Chair Ransom called the meeting to order at 7:04 p.m.

**PLEDGE OF ALLEGIANCE:** Chair Ransom led the pledge of allegiance

**ROLL CALL:** Roll Call found Chair Ransom, Vice Chair Sangha, Commissioner Johnson, Commissioner Mitracos, and Commissioner Orcutt. Also present were staff members Bill Dean Assistant Director Development Services, Bill Sartor, Assistant City Attorney, Jan Couturier Recording Secretary.

**MINUTES APPROVAL:**

Chair Ransom asked for approval of the April 11, 2012 Minutes. Commissioner Johnson recommended approval as written, Jass Sangha seconded, all in favor, none opposed with Commissioner Orcutt abstaining.

Chair Ransom asked for approval of the April 25, 2012 Minutes. Commissioner Johnson recommended approval as written, Jass Sangha seconded, all in favor, none opposed with Commissioner Orcutt abstaining.

Chair Ransom asked for approval of the May 9, 2012 Minutes. Commissioner Johnson recommended approval as written, Jass Sangha seconded, all in favor, none opposed with Commissioner Orcutt abstaining.

Chair Ransom asked for approval of the August 22, 2012 Minutes. Commissioner Johnson recommended approval as written, Jass Sangha seconded, all in favor, none opposed with Commissioner Orcutt abstaining.

Chair Ransom asked for approval of the November 14, 2012 Minutes. Commissioner Johnson recommended approval as written, Jass Sangha seconded, all in favor, none opposed with Commissioner Orcutt abstaining.

Chair Ransom asked for approval of the December 19, 2012 Minutes.

Commissioner Mitracos commented about the verbatim transcript which had been discussed at the January 23<sup>rd</sup> meeting. He added that this revision of the December 19, 2012 minutes was a better representation, but felt that he would have to abstain from approving them, referencing both the nature of the content of the meeting and comments from Mr. Jarvis which he felt were not properly represented in the revised minutes.

Chair Ransom indicated she felt the revised minutes were much improved and that as summary minutes she indicated she felt the key issues were identified. She added the fact that a transcript does exist as a contingency.

Commissioner Mitracos was specifically concerned about Mr Jarvis's statements made relative to the pending lawsuit and legal fees which he indicated were not mentioned. He too, referred to the recording of the session as an alternative source.

Commissioner Johnson asked about the timeframe for keeping records of the minutes. Bill Sartor, Assistant City Attorney advised the minutes are kept forever. There was a further question about the verbatim transcript of the December 19, 2012 meeting as well as the length of time recordings would be kept. Mr. Sartor advised there was a standard of three years after it is no longer needed and advised of the process required before anything can be destroyed. Mr. Sartor said that the City of Tracy is moving toward electronic storage which allows for increased capacity. There was a general discussion about how long documents are kept.

Chair Ransom asked if there was a general standard such as the three years mentioned. Mr. Sartor indicated that the government standard is two years, but the Tracy City Council recommended three years; which was adopted. That time begins after a document is no longer needed.

Ransom asked if there were any further questions. Commissioner Johnson suggested that with although the December 19 meeting was over four hours; he felt that the most important issues were covered in the minutes. Commissioner Johnson recommended approval as written, Jass Sangha seconded, all in favor, none opposed with Commissioners Mitracos and Orcutt abstaining.

Chair Ransom asked for approval of the January 23, 2013 Minutes. Commissioner Mitracos mentioned that he did not recall saying the second sentence at the top of page five and made a motion that the minutes be approved with the recommendation that the second sentence at the top of page five be stricken. It was seconded by Vice Chair Sangha, all in favor, none opposed with Joseph Orcutt abstaining.

Chair Ransom thanked the Planning Division for getting the minutes caught up to date.

Chair Ransom asked if there was a Directors Report.

**DIRECTOR'S REPORT REGARDING THIS AGENDA:** Mr. Dean reviewed the process of the Planning Department staff preparing the overdue minutes and that staff spent quite a bit of time auditing and reviewing all minutes to get everything up to date.

Mr. Dean welcomed Commissioner Orcutt to the Planning Commission.

**ITEMS FROM THE AUDIENCE:** None

**OLD BUSINESS:** None

**NEW BUSINESS:** None

**ITEMS FROM THE AUDIENCE:** None

**DIRECTORS REPORT:**

Mr. Dean updated the Commission on upcoming projects. He advised that the Cordiss Ranch project should be coming in the spring and an EIR will be forth coming in the next few weeks. He invited the Commission to spend some time during the public review period to study the project and engage in any public forums. He further mentioned the Tracy Hills project, Kagehiro Phase III, Tiburon Village and other residential and apartment complex projects. He advised that the residential market has become very active.

Commissioner Mitracos asked if Tracy Hills project would be happening this fall. Mr. Dean said that the environmental work would come this summer.

Commissioner Johnson asked about the North East Specific Plan. Mr. Dean indicated that Amazon had created a great deal of buzz with various trade shows and suggested everybody is becoming aware of what is going on in Tracy right now.

Commissioner Johnson asked about Caltrans or any road improvements timeframes and Mr. Dean indicated that would be down the road.

**ITEMS FROM THE COMMISSION**

Chair Ransom introduced Commissioner Orcutt and asked that he give a brief biography.

Commissioner Orcutt provided the Commission with information about himself, his family and his community involvement. He indicated that he looked forward to serving on the Planning Commission.

Chair Ransom mentioned the Planning Commission Journal and an interesting article about being a Planning Commissioner.

Chair Ransom asked why the Planning Commissions meetings were not televised. Mr. Dean indicated that council had decided not to have them televised.

Chair Ransom then asked if it might be helpful to have them publicized as another means to reach the public. Mr. Dean suggested that the present system of public outreach including neighborhood meetings was effective. He also advised that there would likely be a cost issue to have the meetings televised. He added that public outreach efforts are more tried and true methods and that the Commissioners could be of assistance in that area. Commissioner Mitracos said that he felt public meetings worked well.

**ADJOURNMENT**

Chair Ransom requested a motion to adjourn. Commissioner Johnson so moved, Vice Chair Sangha seconded; all in favor, none opposed.

The meeting adjourned at 7:38 p.m.

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CHAIR

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STAFF LIAISON

AGENDA ITEM 2 A

REQUEST

**PUBLIC HEARING TO CONSIDER APPROVAL OF AN 8-LOT TENTATIVE SUBDIVISION MAP ON A 1.2-ACRE PARCEL LOCATED AT THE SOUTH END OF ALHAMBRA AND GIBSON COURTS FOR THE CONSTRUCTION OF EIGHT SINGLE-FAMILY HOMES. THE APPLICANT IS MACKAY AND SOMPS AND OWNER IS STANDARD PACIFIC HOMES- APPLICATION NUMBER TSM11-0001**

BACKGROUND

Site and Project Area Description

The subject property is located at the southern end of two cul-de-sacs within the Muirfield 7 subdivision, on Alhambra and Gibson Courts. The project site is also west of Corral Hollow Road, on the north side of Starflower Drive (Attachment A). The project area is 1.2 acres, to be subdivided for the construction of eight single-family homes, herein called the project site. The project site, was annexed to the City in 1997.

The zoning designation of the project site as well as the surrounding properties is Low Density Residential (LDR), with a General Plan designation of Residential Low, allowing for 2.1 to 5.8 dwelling units per gross acre.

DISCUSSION

Subdivision

The proposal is to divide the property into eight lots in order to develop eight detached single-family homes on approximately 1.2 acres (Attachment B). The proposed lot sizes range from 6,235 to 7,043 square feet, which is in compliance with the minimum 5,600-square foot lot size that is required on properties within the LDR zone. The subdivision design includes four lots each at the end of Alhambra and Gibson Courts, consistent with the existing adjacent residential lots already existing on those streets. When the previous phase of this subdivision (Muirfield 7) was approved and constructed, these lots were not mapped at that time, due to a lack of infrastructure capacity to serve additional houses at the time. However, with the adjacent construction of Alhambra and Gibson Courts, these eight lots were improved with all of the road, sidewalk, water, sewer, etc. connections stubbed to the site, as though they were "finished" lots.

Per Tracy Municipal Code Section 12.16.070, the Planning Commission has approval authority for a Tentative Subdivision Map, rather than making a recommendation for City Council action, as is required for Vesting Tentative Subdivision Maps.

Access

There is one existing main access point for the overall subdivision, located on at Corral Hollow and Starflower, which serves as the main access point. There are additional

streets that connect to the site from other directions, such as Dove Drive, which connects with Sycamore Parkway to the east.

#### Building Setbacks, Development Standards

The minimum building setbacks are to be consistent with the standards of the LDR zone, which include front yard setbacks of 15 feet from the sidewalk, side yard setbacks of 10 feet and four feet to the property lines (maintaining 14 feet between buildings) and rear yard setbacks at a minimum of 10 feet, with a 15-foot average. These building setbacks are the same as those of the surrounding houses within both the adjacent Muirfield 7 and Eastgate subdivisions.

The proposed maximum building height within the LDR zone is 2 ½ stories or is 35 feet (whichever is less), which is consistent with the zoning regulations of all of the adjacent single-family homes also within the LDR zone.

Because this project is proposed as a Tentative Subdivision Map (not a Vesting Map) and is within the LDR zone, the development of single-family homes on those lots is exempt from the requirement for Development (architectural) Review. The proposed construction documents will be reviewed by staff at the time of building permit applications for compliance with the applicable Uniform and California Building Codes.

#### RGAs

The project will require eight RGAs for the construction of the eight proposed residential units. The project will be eligible to apply for and receive RGAs per the regulations set forth in the Growth Management Ordinance and Growth Management Ordinance Guidelines after a Tentative Subdivision Map is approved. The RGAs will be required prior to the issuance of any building permits.

#### Schools

The Tracy Joint Unified School District has determined that the project does not need to dedicate property for a school site within the subdivision. However, in order to mitigate the proposed developments' impacts on school facilities, the School District and the developer executed an MOU that caused the property to participate in a Community Facilities District for the payment of school fees mitigating the project's impacts on the local school system.

#### Parks

Parks are required to be established within residential neighborhoods to serve the residents of the homes that are established in Tracy. In order to meet the need for park land, projects are either required to build their own park, or pay park in-lieu fees. Since the minimum park size within the City is typically required to be two acres, this project will pay the park in-lieu fees as established.

#### Environmental Document

The project is consistent with the Negative Declaration approved by the City Council for the Kagehiro Annexation. Pursuant to CEQA Guidelines Section 15162, no additional environmental documentation is required, as no significant changes have occurred to the project or the environment after the approval of the Negative Declaration. An analysis of the project shows that there will be no significant on or off-site impacts as a result of this particular project which were not already discussed in the Kagehiro Mitigated Negative Declaration. There is also no evidence of any significant impacts to occur off-site as a result of the project, as traffic, air quality, land use and other potential cumulative impacts have already been considered within the original environmental documentation. No new evidence of potentially significant effects has been identified as a result of this project.

Public Resources Code section 21083.3 and its parallel Guidelines provision, section 15183, provide for streamlined environmental review for projects consistent with the development densities established by existing zoning, general plan, or community plan policies for which an environmental impact report ("EIR") was certified. Such projects require no further environmental review except as might be necessary to examine whether there are project-specific significant effects that are peculiar to the project or its site. If an impact is not peculiar to the parcel or to the project, has been addressed as a significant impact in the prior EIR, or can be substantially mitigated by the imposition of uniformly applied development policies or standards, then an additional EIR need not be prepared for the project solely on the basis of that impact.

On February 1, 2011, the City adopted a new General Plan and certified the associated General Plan EIR (SCH# 2008092006).

The General Plan land use designation for the Project site is Residential Low. The development density of the project is consistent with the Residential Low land use designation.

Staff has examined the environmental effects of the project and has determined that no further review is necessary because there are no:

- (a) environmental effects that are peculiar to the project or the parcel on which the project would be located. The project is a phase of an existing neighborhood that is consistent with the City's General Plan and Zoning.;
- (b) environmental effects that were not analyzed as significant effects in the General Plan EIR. This project is within the development density contemplated in the general Plan EIR and does not include unique development features;
- (c) potentially significant off-site impacts and cumulative impacts which were not discussed in the General Plan EIR. This project is consistent with all the assumptions of the General Plan EIR; or
- (d) previously identified significant effects in the General Plan EIR which, as a result of substantial new information which was not known at

the time the EIR was certified, is determined to have a more severe adverse impact than discussed in the EIR. No new information has emerged that differs from the impact areas discussed in the General Plan EIR.

Finally, the General Plan EIR specified a number of feasible mitigation measures to address significant effects on the environment that would result in implementing the Plan. To the extent applicable, these mitigation measures are incorporated as part of the project or as part of the project's conditions of approval.

#### RECOMMENDATION

Staff recommends that the Planning Commission approve the Muirfield 7 Phase 3 Tentative Subdivision Map, Application Number TSM11-0001, based on the findings and subject to the conditions contained in the Planning Commission Resolution (Attachment C) dated April 10, 2013.

#### MOTION

Move that the Planning Commission approve the Muirfield 7 Phase 3 Tentative Subdivision Map, Application Number TSM11-0001, based on the findings and subject to the conditions contained in the Planning Commission Resolution (Attachment C) dated April 10, 2013.

Prepared by Victoria Lombardo, Senior Planner

Reviewed by Bill Dean, Assistant DS Director

Approved by Andrew Malik, DS Director

#### ATTACHMENTS

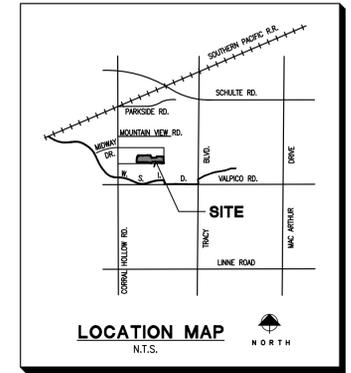
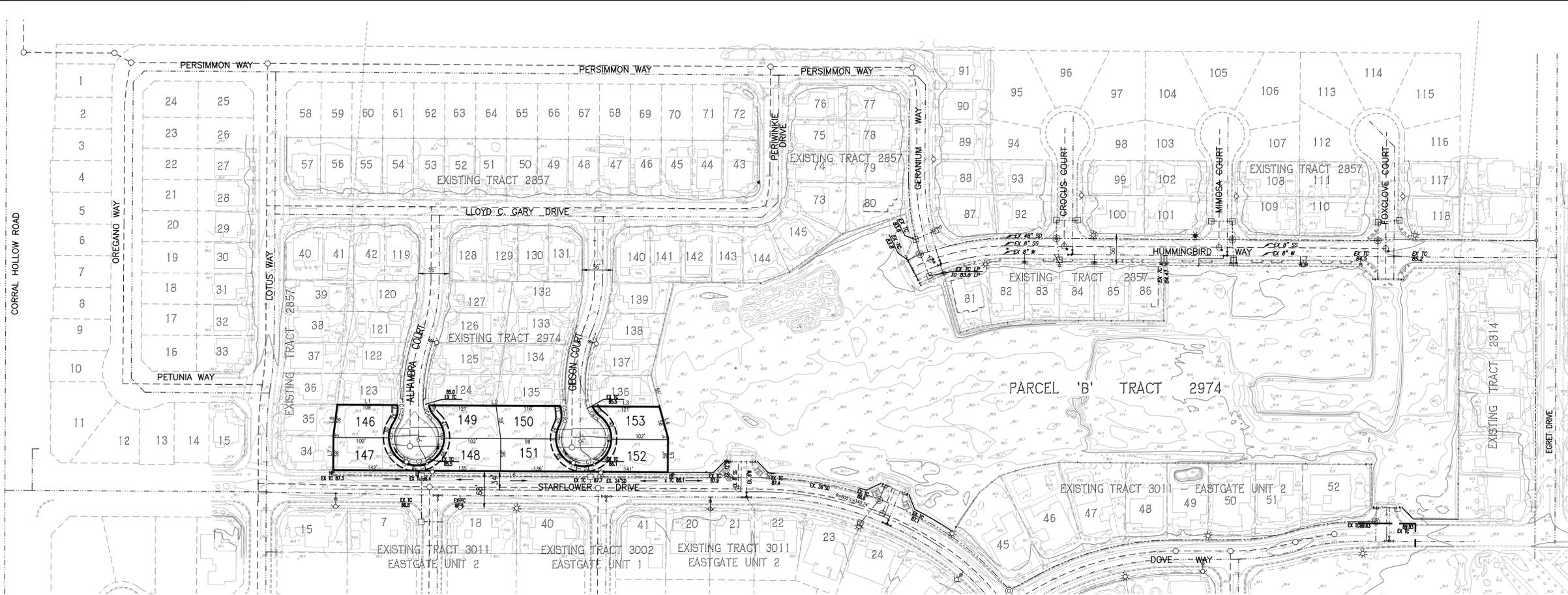
Attachment A—Location Map

Attachment B—Subdivision Map

Attachment C—Planning Commission Resolution to approve TSM

# Location Map





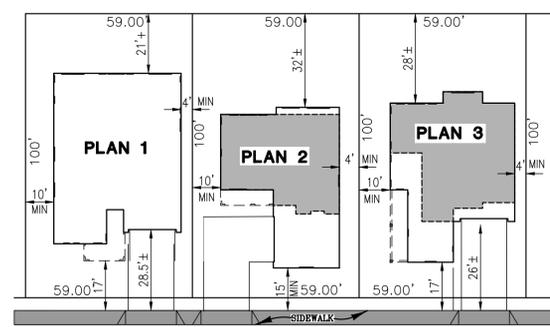
**BASIS OF BEARINGS:**  
 THE LINE BEARING NORTH 0°13'41" WEST BETWEEN CITY OF TRACY CONTROL MONUMENTS 9 AND A6 OF THE CITY OF TRACY MODIFIED GRID SYSTEM, AS SHOWN ON RECORD OF SURVEY FILED IN BOOK 36 OF SURVEYS, AT PAGE 118, SAN JOAQUIN COUNTY RECORDS, WAS USED AS THE BASIS OF BEARINGS SHOWN HEREON. COORDINATES SHOWN ARE BASED ON THE CITY OF TRACY MODIFIED GRID SYSTEM.

**NOTES:**

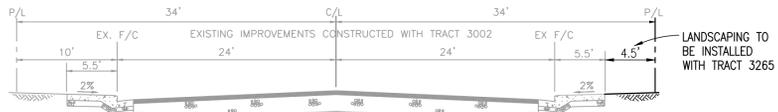
- OWNER & SUBDIVIDER: STANDARD PACIFIC CORP. A DELAWARE CORPORATION (OWNER) STANDARD PACIFIC HOMES OF NORTHERN CALIFORNIA (SUBDIVIDER) 3650 INDUSTRIAL BOULEVARD, SUITE 140 WEST SACRAMENTO, CA 95691 (916) 375 - 5200
- ENGINEER: MACKAY & SOMPS CIVIL ENGINEERS 5142 FRANKLIN DRIVE, SUITE B PLEASANTON, CA 94588 (925) 225-0690
- ASSESSOR'S PARCEL NUMBER: 242-390-28
- EXISTING LAND USE: VACANT
- EXISTING ZONING: LDR, GENERAL PLAN: LOW DENSITY RESIDENTIAL
- PROPOSED LAND USE: SINGLE FAMILY RESIDENTIAL
- ACREAGE: TOTAL=1.2± AC.
- NUMBER OF LOTS: 8 (5,600 S.F. MIN.)
- APPROXIMATE GROSS DENSITY: 6.6± UNITS/ACRE.
- LOT NUMBERS ARE FOR IDENTIFICATION PURPOSES ONLY
- FINAL LANDSCAPE AND IRRIGATION PLANS ARE TO BE SUBMITTED WITH THE FINAL IMPROVEMENT PLANS
- UTILITIES: WATER SERVICE: CITY OF TRACY SANITARY SEWER: CITY OF TRACY STORM DRAIN SYSTEM: CITY OF TRACY GAS & ELECTRICITY: PACIFIC GAS & ELECTRIC TELEPHONE: AT & T CABLE TELEVISION: COMCAST STREET TREES: PER CITY OF TRACY STANDARDS
- UTILITY SIZES AND LOCATIONS, STREET GRADES, AND LOT DIMENSIONS ARE EXISTING AND BASES ON THE APPROVED IMPROVEMENT PLANS FOR TRACT 2974, MUIRFIELD VII, PHASE 2 DATED JANUARY 8, 2001.
- FIRE HYDRANTS AND ELECTROLIERS ARE EXISTING, SEE ABOVE.
- DEVELOPER MAY RECORD ONE OR MORE FINAL SUBDIVISION MAPS ON THIS PROPERTY.

- PUBLIC UTILITY EASEMENTS ARE EXISTING PER TRACT 2974, BK. 35 OF MAPS, PG. 99.
- UNLESS OTHERWISE SPECIFICALLY STATED IN THE CONDITIONS OF APPROVAL, LOCAL AGENCY APPROVAL OF THIS MAP SHALL CONSTITUTE AN EXPRESS FINDING THAT THE PROPOSED DIVISION AND DEVELOPMENT OF THE SUBJECT PROPERTY WILL NOT UNREASONABLY INTERFERE WITH THE FREE AND COMPLETE EXERCISE OF RIGHTS DESCRIBED IN GOVERNMENT CODE SECTION 66436(a)(3)(A)(i).
- PROPERTY DESCRIPTION: PARCELS A OF TRACT 2974, BK 35 OF MAPS, PG. 99 EXCEPTING THERE FROM THAT PORTION OF LAND DESCRIBED IN DOCUMENT 2001-007331 SAN JOAQUIN COUNTY.
- CONTOUR INTERVAL: 1 FOOT. AERIAL PHOTO DATED FEBRUARY, 2011, DATUM = NAVD88
- NO EXISTING TREES ON SITE
- AREAS OF INUNDATION = NONE
- SETBACKS TO CONFORM TO TRACY ZONING ORDINANCE FOR LOW DENSITY RESIDENTIAL.
- GEOTECHNICAL ENGINEER: ENGEO INC. GEOTECHNICAL INVESTIGATION MUIRFIELD UNIT 3, Job No. 9844.000.000, DATED JANUARY 2, 2013
- THE EXISTING CUL-DE-SAC CURB RADI FOR ALHAMBRA COURT AND GIBSON COURT ARE BASED ON THE APPROVED IMPROVEMENT PLANS FOR TRACT 2974, MUIRFIELD VII, PHASE 2 DATED 1/8/2001, MAJOR RADI = 50', MINOR RIADI = 40'.

TYPICAL LOT SIZE: 59' X 100'  
 MINIMUM SETBACKS ARE AS FOLLOWS :  
 FRONT YARD SETBACKS 15' - 20' MIN.  
 REAR YARD SETBACKS 15' MIN.  
 SIDE YARD SETBACKS 4' MIN.  
 STREET SIDE YARD SETBACKS 10' MIN.  
 BUILDING SEPARATION 14'



**CONCEPTUAL HOUSE PLANS**  
 1"=30'



**STARFLOWER DRIVE EXISTING STREET SECTION-68' R/W**  
 N.T.S.

Line Table		
Line #	Bearing	Length
L1	N89°37'09"W	108.98'
L2	N89°37'09"W	238.17'
L3	N89°37'09"W	121.23'
L4	N15°22'23"W	58.19'
L5	N0°22'51"E	61.74'
L6	N89°37'38"W	605.34'
L7	N0°22'51"E	61.83'
L8	N7°12'45"E	56.40'

Curve Table			
Curve #	Radius	Delta	Length
C1	378.00'	0°55'15"	6.08'
C2	40.00'	36°42'07"	25.62'
C3	50.00'	261°22'30"	228.09'
C4	40.00'	36°08'28"	25.23'
C5	303.00'	2°07'27"	11.23'
C6	40.00'	35°43'20"	24.94'
C7	50.00'	261°04'15"	227.83'
C8	40.00'	32°12'15"	22.48'

**CITY ENGINEER'S CERTIFICATE**

I HEREBY CERTIFY THAT THIS MAP HAS BEEN REVIEWED FOR COMPLIANCE WITH THE REQUIREMENTS OF THE TRACY MUNICIPAL CODE AND THE SUBDIVISION MAP ACT AS TO FORM AND CONTENT.

KULDEEP SHARMA CITY ENGINEER DATE

**PLANNING COMMISSION FILING INFORMATION**

THIS TENTATIVE MAP WAS FILED ON THE \_\_\_\_\_ DAY OF \_\_\_\_\_, 201\_\_\_\_, IN THE OFFICE OF THE TRACY PLANNING COMMISSION, TRACY, CALIFORNIA, ACCOMPANIED WITH \_\_\_\_\_ FILING FEES.

**PLANNING COMMISSION CERTIFICATE**

I HEREBY CERTIFY THAT THE PLANNING COMMISSION OF THE CITY OF TRACY RECOMMENDED THIS TENTATIVE MAP BE CONDITIONALLY APPROVED ON \_\_\_\_\_ BY RESOLUTION NO. \_\_\_\_\_ AND THAT ON \_\_\_\_\_ THE CITY COUNCIL CONDITIONALLY APPROVED THIS MAP BY RESOLUTION NO. \_\_\_\_\_.

SECRETARY PLANNING COMMISSION / COMMUNITY DEVELOPMENT DIRECTOR  
 DATE DATE

**PROPERTY OWNER'S SIGNATURE**

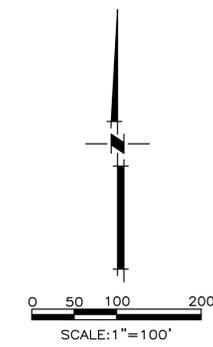
OWNER REPRESENTATIVE DATE  
 TITLE

**PREPARED UNDER THE SUPERVISION OF:**

BY: SUBMITTED FOR 3RD REVIEW  
 JAMES F. TEMPLETON JR. RCE #41637  
 EXPIRES: MARCH 31, 2014



LEGEND		
EXISTING	PROPOSED	
---	---	SUBDIVISION BOUNDARY
○	●	SANITARY SEWER
□	■	STORM DRAIN
+	+	ELECTROLIER
+	+	WATER & VALVE
+	+	FIRE HYDRANT
+	+	PERMANENT BLOW-OFF
~	~	CONTOUR LINE
×	×	80.9 FINISH GRADE
HP	HP	HIGH POINT
LP	LP	LOW POINT
→	→	DIRECTION OF STREET SLOPE



**TENTATIVE MAP**  
**TRACT 3265**  
**MUIRFIELD 7-PHASE 3**  
**SINGLE FAMILY RESIDENTIAL**  
**TRACY, CALIFORNIA**

**MACKAY & SOMPS**  
 CIVIL ENGINEERING • LAND PLANNING • LAND SURVEYING  
 5142B FRANKLIN DR. PLEASANTON, CA 94588 (510)225-0690

OFFICE	SCALE	DATE	JOB NO.
PLEASANTON	1"=100'	FEBRUARY 19, 2013	19565-0

## **RESOLUTION 2 A**

### **PLANNING COMMISSION APPROVING THE 8-LOT MUIRFIELD 7 PHASE 3 TENTATIVE SUBDIVISION MAP FOR A 1.2 (NET)-ACRE SITE LOCATED AT THE SOUTHERN ENDS OF ALHAMBRA AND GIBSON COURTS APPLICATION NUMBER TSM11-0001**

WHEREAS, The subject property was annexed to the City of Tracy in 1997 and is an infill parcel, with a General Plan land use designation of Residential Low and

WHEREAS, The project will create 8 single-family dwelling units, on 1.62 gross acres, with an overall density of approximately 4.9 dwelling units per acre, which is consistent with the General Plan land use and density requirements, and

WHEREAS, The proposed map is consistent with the General Plan, and Title 12, the Subdivision Ordinance, of the Tracy Municipal Code. The General Plan designation of the property is Residential Low, which provides for a density range of 2.1 to 5.8 dwelling units per acre. The General Plan identifies that the characteristic housing for the Low Density Residential categories includes single family detached units, as well as other housing types, and

WHEREAS, The site is physically suitable for the type of development, as the site is virtually flat and the characteristically high clay content of Tracy's soils may require amendments and treatment for proposed landscaping, foundations, and other surface and utility work. The physical qualities of the property make it suitable for residential development in accordance with City standards, and

WHEREAS, The site is physically suitable for the proposed density of development. The 4.9 dwelling units per acre proposed is consistent with the allowable density range prescribed by the General Plan. Traffic circulation is designed in accordance with City standards for the proposed density to ensure adequate traffic service levels are met, and

WHEREAS, The design of the subdivision or the proposed improvements will not cause substantial environmental damage or substantially and avoidably injure fish or wildlife or their habitat. An Environmental Impact Report was certified for the City's General Plan in 2011. Significant fish or wildlife or their habitat have not otherwise been identified on the site and no further environmental documentation is required, and

WHEREAS, The design of the subdivision or the type of improvements will not conflict with easements, acquired by the public at large, for access through or use of, property within the proposed subdivision, and

WHEREAS, The project complies with all other applicable ordinances, regulations and guidelines of the City, including but not limited to, the local floodplain ordinance. The subject property is not located within any floodplain and the project, with conditions, will meet all applicable City design and improvement standards, and

WHEREAS, All the public facilities necessary to serve the subdivision will be in place prior to the issuance of building permits. All the public facilities necessary to serve

the subdivision or mitigate the impacts created by the subdivision will be assured through a subdivision improvement agreement prior to the approval of a final map, and

WHEREAS, The Planning Commission conducted a public hearing to review and consider the Tentative Subdivision Map on April 10, 2013;

NOW, THEREFORE, BE IT RESOLVED, That the Planning Commission does hereby approve the Muirfield 7 Phase 3 Tentative Subdivision Map, Application Number TSM11-0001, subject to conditions stated in Exhibit "1" attached and made part hereof.

\* \* \* \* \*

The foregoing Resolution \_\_\_\_\_ was adopted by the City Council on the 10<sup>th</sup> day of April, 2013, by the following vote:

AYES:            COMMISSION MEMBERS:  
NOES:            COMMISSION MEMBERS:  
ABSENT:        COMMISSION MEMBERS:  
ABSTAIN:       COMMISSION MEMBERS:

\_\_\_\_\_  
Chair

ATTEST:

\_\_\_\_\_  
Staff Liaison

Conditions of Approval for  
Muirfield 7 Phase 3 Tentative Subdivision Map  
Application Number TSM11-0001

A. These Conditions of Approval shall apply to the real property described as Muirfield 7 Phase 3 Tentative Subdivision Map, Application Number TSM 11-0001 (hereinafter "Project"), generally located on approximately 1.62 gross acres (1.2 net acres) at the southern end of Alhambra and Gibson Courts, Assessor's Parcel Number 242-390-28.

1. The following definitions shall apply to these Conditions of Approval:

- a. "Applicant" means any person, or other legal entity, defined as a "Developer".
- b. "City Engineer" means the City Engineer of the City of Tracy, or any other duly licensed engineer designated by the City Manager, or the Public Works Director, or the City Engineer to perform the duties set forth herein.
- c. "City Regulations" means all written laws, rules, and policies established by the City, including those set forth in the City of Tracy General Plan, the Tracy Municipal Code, ordinances, resolutions, policies, procedures, and the City's Design Documents (including the Standard Plans, Standard Specifications, Design Standards, and relevant Public Facility Master Plans).
- d. "Development Services Director" means the Development Services Director of the City of Tracy, or any other person designated by the City Manager or the Development Services Director to perform the duties set forth herein.
- e. "Conditions of Approval" shall mean the conditions of approval applicable to the Muirfield 7 Phase 3 Tentative Subdivision Map, Application Number TSM11-0001. The Conditions of Approval shall specifically include all Development Services Department Conditions set forth herein.
- f. "Project" means the real property consisting of approximately 1.62 gross acres located at the southern end of Alhambra and Gibson Courts, Assessor's Parcel Number 242-390-28.
- g. "Subdivider" means any person, or other legal entity, who applies to the City to divide or cause to be divided real property within the Project boundaries, or who applies to the City to develop or improve any portion of the real property within the Project boundaries. "Subdivider" also means the Developer. The term "Subdivider" shall include all successors in interest.

2. The Developer shall comply with all laws (federal, state, and local) related to the development of real property within the Project, including, but not limited to: the Planning and Zoning Law (Government Code sections 65000, *et seq.*), the Subdivision Map Act (Government Code sections 66410, *et seq.*), the California Environmental Quality Act (Public Resources Code sections 21000, *et seq.*, "CEQA"), and the Guidelines for California Environmental Quality Act (California Administrative Code, title 14, sections 15000, *et seq.*, "CEQA Guidelines").
3. Unless specifically modified by these Conditions of Approval, the Developer shall comply with all City Regulations, including, but not limited to the Low Density Residential Zone district.
4. Unless specifically modified by these Conditions of Approval, the Developer shall comply with all mitigation measures identified in the General Plan Environmental Impact Report, dated February 11, 2011.
5. Pursuant to Government Code Section 66020, including Section 66020 (d)(1), the City HEREBY NOTIFIES the Developer that the 90-day approval period (in which the Developer may protest the imposition of any fees, dedications, reservations, or other exactions imposed on this Project by these Conditions of Approval) has begun on the date of the conditional approval of this Project. If the Developer fails to file a protest within this 90-day period, complying with all of the requirements of Government Code Section 66020, the Developer will be legally barred from later challenging any such fees, dedications, reservations or other exactions.

#### **B. Planning Division Conditions**

1. Prior to the issuance of a building permit, the developer shall document compliance with all applicable school mitigation requirements consistent with City Council standards and obtain certificate of compliance from Tracy Unified School District for each new residential building permit. School mitigation requirements include payment of all special taxes associated with Community Facilities District 97-1 and the Sterling Act "school fee".
2. Prior to the approval of the first Final Map, the Subdivider shall prepare a detailed landscape and irrigation plan consistent with City standards, including the Water Efficient Landscape Guidelines, to the Satisfaction of the Development Services Director.
3. Prior to the recordation of the first Final Map, the Subdivider shall show public utility easements necessary to accommodate the needs of local utility providers in accordance with City standards, to the satisfaction of the City Engineer.

#### **C. Development Services Engineering Division Conditions**

Contact: Criseldo Mina (209) 831-6425 [cris.mina@ci.tracy.ca.us](mailto:cris.mina@ci.tracy.ca.us)

C.1. Tentative Subdivision Map

Prior to signature of the Tentative Subdivision Map by the City Engineer, the Subdivider shall comply with the requirements set forth in this section, to the satisfaction of the City Engineer.

- C.1.1. Submit one (1) reproducible copy of the approved tentative map for the Project within ten (10) days after Subdivider's receipt of notification of approval of the tentative map.

C.2. Final Map

No final map within the Project boundaries will be approved by the City until the Subdivider demonstrates, to the satisfaction of the City Engineer, compliance with all required Conditions of Approval, including, but not limited to, the following:

- C.2.1. The Subdivider has completed all the requirements set forth in this section, and Condition C.1., above.
- C.2.2. The Subdivider has obtained the approval of all other public agencies with jurisdiction over the required public facilities.
- C.2.3. The Final Map shall include dedications or offers of dedication of all rights-of-way and easements required to serve the Project described by the Final Map, in accordance with City Regulations and these Conditions of Approval.
- C.2.4. Horizontal and vertical control for the Project shall be based upon the City of Tracy coordinate system and at least three 2nd order Class 1 control points establishing the "Basis of Bearing" and shown as such on the final map. The final map shall also identify surveyed ties from two of the control points to a minimum of two separate points adjacent to or within the property described by the Final Map.
- C.2.5. Payment of all fees required by these Conditions of Approval and City Regulations.

C.3. Improvement Plans

The Improvement Plans that are required in this section are the marked up improvement plans of the previously approved improvement plans for Muirfield 7, Phase 2, Tract 2974, which shows streets and utilities improvements that are yet to be installed behind the curb and sidewalk and in front of the eight (8) residential lots. Prior to obtaining the City Engineer's signature on the Improvement Plans, all the requirements set forth in this section shall be completed, to the satisfaction of the City Engineer, including but not limited to, the following:

- C.3.1. The Subdivider has completed all the requirements set forth in this section, and Conditions C.1., and C.2., above.
  - C.3.2. Improvement Plans to be signed by the City Engineer must be provided on a 4-mil thick 24" x 36" size polyester film (mylar) and prepared under the supervision of, and stamped and signed by a Registered Civil Engineer.
  - C.3.3. Sanitary Sewer - The Subdivider shall design and install sewer connection for this Project in accordance with City Regulations. The Subdivider is hereby notified that the City will not provide maintenance of the sewer lateral within the public right-of-way unless the sewer cleanout is located and constructed in conformance with Standard Plan No. 203. The City's responsibility to maintain on the sewer lateral is from the wye fitting to the point of connection with the sewer main.
  - C.3.4. Domestic and Fire Service Water - Domestic water service shall be installed in accordance with City Regulations and at the location approved by the City Engineer. City's responsibility to maintain water lines shall be from the water main on the street to the back of the water meter (inclusive) only. Repair and maintenance of all on-site water lines, laterals, sub-meters, valves, fittings, fire hydrant and appurtenances shall be the responsibility of the Subdivider.
  - C.3.5. The Improvement Plans shall contain the Tracy's Fire Marshall's signature indicating their approval on the Project's fire service connection, fire and emergency vehicle access to the Project, and compliance of the City's Fire Department fire protection requirements. Written approval from the Fire Department required in this section shall be obtained by the Subdivider, prior to City Engineer's signature on the Improvement Plans.
- C.4. Grading Permit  
No application for grading permit and encroachment permit within the Project boundaries will be accepted by the City as complete until the Subdivider provides all documents required by City Regulations and these Conditions of Approval, to the satisfaction of the City Engineer, including but not limited to, the following:
- C.4.1. The Subdivider has completed all the requirements set forth in this section and Conditions C.1., and C.2., above.
  - C.4.2. A Grading and Storm Drainage Plan prepared by a Registered Civil Engineer as required in Condition C.3., and accompanied by Soils Engineering and Engineering Geology reports shall be submitted to the City with the Improvement Plans. The reports shall provide

recommendations regarding adequacy of sites to be developed by the proposed grading and also information relative to the stability of soils. Slope easements, if necessary, shall be recorded per City Regulations. Prior to the issuance of the first building permit within the Property, the Subdivider shall submit a letter, signed and stamped by a Registered Geo-technical Engineer, certifying that grading work, including excavation, backfilling, compacting and backfilling work performed by the Subdivider, meets the requirements of the Project's Soils Report and was completed under the supervision of the Project's Geo-technical Engineer (licensed to practice in the State of California).

- C.4.3. All grading shall require a Grading Permit. Erosion control measures shall be implemented in accordance with plans approved by the City Engineer for all grading work not completed before the 15<sup>th</sup> of October of that year. Improvement Plans shall designate all erosion control methods and materials to be employed.
- C.4.4. Prior to the issuance of the Grading Permit, the Subdivider shall submit three (3) sets of the Storm Water Pollution Prevention Plans (SWPPP) and a copy of the Notice of Intent (NOI) submitted to the State Water Quality Control Board (SWQCB) and any documentation or written approvals from the SWQCB, including the Wastewater Discharge Identification Number (WDID#). After the completion of the Project, the Subdivider is responsible for filing the Notice of Termination (NOT) required by SWQCB. The Subdivider shall provide the City, a copy of the completed Notice of Termination. Cost of preparing the SWPPP, NOI and NOT including the filing fee of the NOI and NOT shall be paid by the Subdivider. The Subdivider shall provide the City with the WDID#, prior to the issuance of the Grading Permit. The Subdivider shall comply with all the requirements of the SWPPP and applicable Best Management Practices (BMPs), City's Storm Water Regulations, and the City's Storm Water Management Program.
- C.4.6. All existing on-site wells shall be abandoned in accordance with the City and San Joaquin County requirements. All costs associated with the abandonment of existing wells including the cost of permits, if required, shall be the responsibility of the Subdivider. The Subdivider shall provide the City documentation or copy of permit issued by the San Joaquin County, approving the removal or destruction of existing well(s), if applicable, prior to the issuance of the Grading Permit.

C.5. Encroachment Permit

No application for encroachment permit within the Project boundaries will be accepted by the City as complete until the Subdivider provides all documents required by City Regulations and these Conditions of Approval, to the satisfaction of the City Engineer, including but not limited to, the following:

- C.5.1. The Subdivider has completed all requirements set forth in this section and Conditions C.1., C.2., C.3., and C.4., above.
- C.5.2. A construction cost estimate for all required public improvements or facilities, prepared in accordance with City Regulations. Total construction cost shall include fifteen percent (15%) construction contingencies. Engineering review fees are calculated based on the approved Engineer's Estimate.
- C.5.3. Payment of all applicable processing fees, including improvement plan check fees, engineering fees for processing Conditions of Approval, encroachment and grading permits and inspection fees, and other fees as required by these Conditions of Approval and City Regulations.

C.6. Building Permit

No building permit within the Project boundaries will be approved by the City until the Subdivider demonstrates, to the satisfaction of the City Engineer, compliance with all required Conditions of Approval, including, but not limited to, the following:

- C.6.1. The Subdivider has completed all requirements set forth in this section, and Conditions C.1., C.2., C.3., C.4., and C.5., above.
- C.6.2. Payment of all applicable development impact fees (a.k.a. capital in-lieu fees), San Joaquin County Facilities Fees, Regional Transportation Impact Fees, School Mitigation Fees, and all fees required by these Conditions of Approval and City Regulations. Development impact fees are adjusted annually based on the Construction Cost Index (CCI) published in the Engineering News Record (ENR). The final development impact fees to be paid by the Subdivider are the development impact fees that are in effect at the time of issuance of the building permit.

The Subdivider agreed that no building permit applications will be accepted and processed until the City approves the development impact fees applicable for this Project.

- C.6.3. A letter signed and stamped by the Project's Geo-Technical Engineer certifying that all grading work that were performed by the Subdivider within the Project meets the requirements of the Project's Geo-technical/Soils Report and the recommendations of the Project's Geo-Technical Engineer, and that the grading work was performed under the direct supervision of the Project's Geo-technical Engineer, as required in Condition C.4.2., above.

C.7. Final Building Inspection

The City shall not conduct a final building inspection on any building within the Project boundaries until the Subdivider provides documentation which demonstrates, to the satisfaction of the City Engineer, that:

- C.7.1. The Subdivider has completed all requirements set forth in this section, and Conditions C.1., C.2., C.3., C.4., C.5., and C.6., above.
- C.7.2. The Subdivider has completed construction of public facilities or improvements required to serve the Project for which a building certificate of occupancy is requested. Unless specifically provided in these Conditions of Approval or other City Regulations, the Subdivider shall take all actions necessary to construct all public facilities required to serve the Project, and the Subdivider shall bear all costs related to the construction of the public facilities (including all costs of design, construction, construction management, improvement plans check, inspection, land acquisition, program implementation, and contingency).

AGENDA ITEM 2 B

REQUEST

**PLANNING COMMISSION STUDY SESSION REGARDING THE DRAFT CORDES RANCH SPECIFIC PLAN**

DISCUSSION

The purpose of this Planning Commission study session is to discuss the Draft Cordes Ranch Specific Plan. The primary focus of the study session will be on Chapter 3: Land Use, Zoning, and Development Standards, and Chapter 4: Design Guidelines.

The Cordes Ranch project has been the subject of previous Planning Commission meetings, including a presentation, discussion, and public input during the initial environmental review process (Notice of Preparation of an Environmental Impact Report) on December 21, 2012. City Council has reviewed the project on several occasions, including November 7, 2012, August 7, 2012, and October 18, 2011, to discuss the I-205 corridor. The outcome of these discussions and direction from City Council has been included in the Draft Specific Plan.

The projected schedule for Planning Commission consideration of this project is June of 2013. It is likely that a Planning Commission hearing will be scheduled to receive comments on the Environmental Impact Report for April 24<sup>th</sup>, which will be separately noticed. This study session is an opportunity to further discuss the project and contents of the Draft Specific Plan. Chapters 1, 2, 3, and 4 of the Draft Specific Plan are attached for the purpose of discussion (Attachment A). This agenda item requires no other action than discussion.

RECOMMENDATION

That Planning Commission conducts a study session regarding the Draft Cordes Ranch Specific Plan and provides comments to staff.

Prepared by: Scott Claar, Associate Planner

Reviewed by: Bill Dean, Assistant Development Services Assistant Director

Approved by: Andrew Malik, Development Services Director

ATTACHMENT

A: Chapters 1, 2, 3, and 4 of the Draft Cordes Ranch Specific Plan

MARCH, 2013

**DRAFT MARCH 2013**  
**CORDES RANCH SPECIFIC PLAN: TRACY, CALIFORNIA**

## CHAPTER 1 INTRODUCTION

### 1.1 INTRODUCTION

This Cordes Ranch Specific Plan establishes land use, zoning, development standards and regulations for approximately 1,780 acres located in the northwest region of the City of Tracy. Throughout this Specific Plan, the development contemplated and permitted by this Specific Plan may be referred to as the "Project", and the 1,780 within the boundaries of the Specific Plan area may be referred to as the "Project Area."

The Project Area is bordered by Interstate 205 to the north, Old Schulte Road to the south, a portion of Mountain House Parkway to the west, and then extends northwest, north of the Delta Mendota Canal to I-205, see Figure 1.1. The Specific Plan envisions the development of approximately 1,462 net acres of the Project Area with commercial, office, manufacturing, warehouse, and distribution uses.

The Project Area is at the crossroads of two major transportation corridors, making it ideal for businesses which require large parcels for use as warehousing, manufacturing, research and development, processing, fabrication, and construction related uses.



Figure 1.1, Site Aerial

**CORDES RANCH**  
SPECIFIC PLAN

**Zoning Districts**

-  General Commercial
-  General Office
-  Business Park Industrial
-  Parks
-  I-205 Overlay Zone

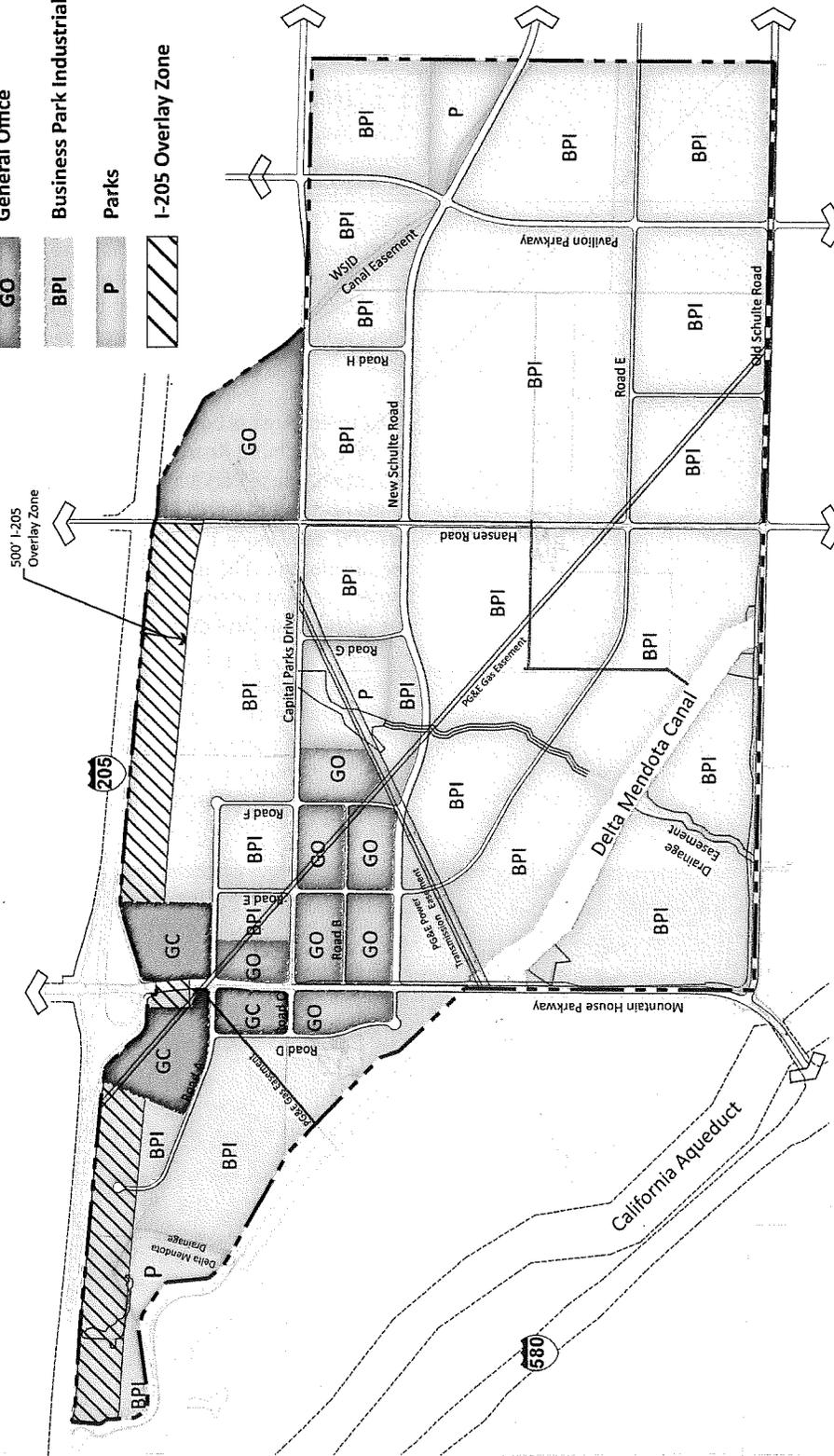


Figure 1.2, Cordes Ranch Specific Plan Zoning

<b>Projected Buildout Land Use Summary</b>			
<b>Gross Acres</b>			
<b>Zoning Districts &amp; Utilities</b>	<b>Gross Acreage</b>		
General Commercial (GC)	55.1		
General Office (GO)	152.2		
Business Park Industrial (BPI)	1476.9		
Parks (P)	96.3		
<b>Total Acres</b>	<b>1780.5</b>		
<b>Net Acres</b>			
<b>Zoning Districts &amp; Utilities</b>	<b>Net Acreage</b>	<b>Max FAR</b>	<b>Total Building Square Footage</b>
General Commercial (GC)	45.3	30%	591,980
General Office (GO)	125.8	45%	2,465,932
Business Park Industrial* (BPI)	1291.6	50%	27,789,102
Parks (P)	88.6		
Central Green	(35.3)		
Eastside Park	(17.8)		
Northwest Open Space Area	(15.8)		
Drainage Channel/Riparian Corridor	(16.8)		
WSID Linear Park/Open Space Corridor	(2.9)		
Roads	149.2		
Delta Mendota Canal	39.3		
Detention Basins	34.0		
Water Tank	4.0		
PG&E Gas Facility	2.7		
<b>Total Net Acres</b>	<b>1780.5</b>		<b>30,847,014</b>
<b>*Note:</b>			
Business Park Industrial includes the I-205 Overlay which has a reduced FAR of 40%. The total projected buildout of this area is approximately 1,300,000 SF which has been accounted for in the above figures.			

Table 1.1, Buildout Land Use Summary

**CORDES RANCH**  
**SPECIFIC PLAN**

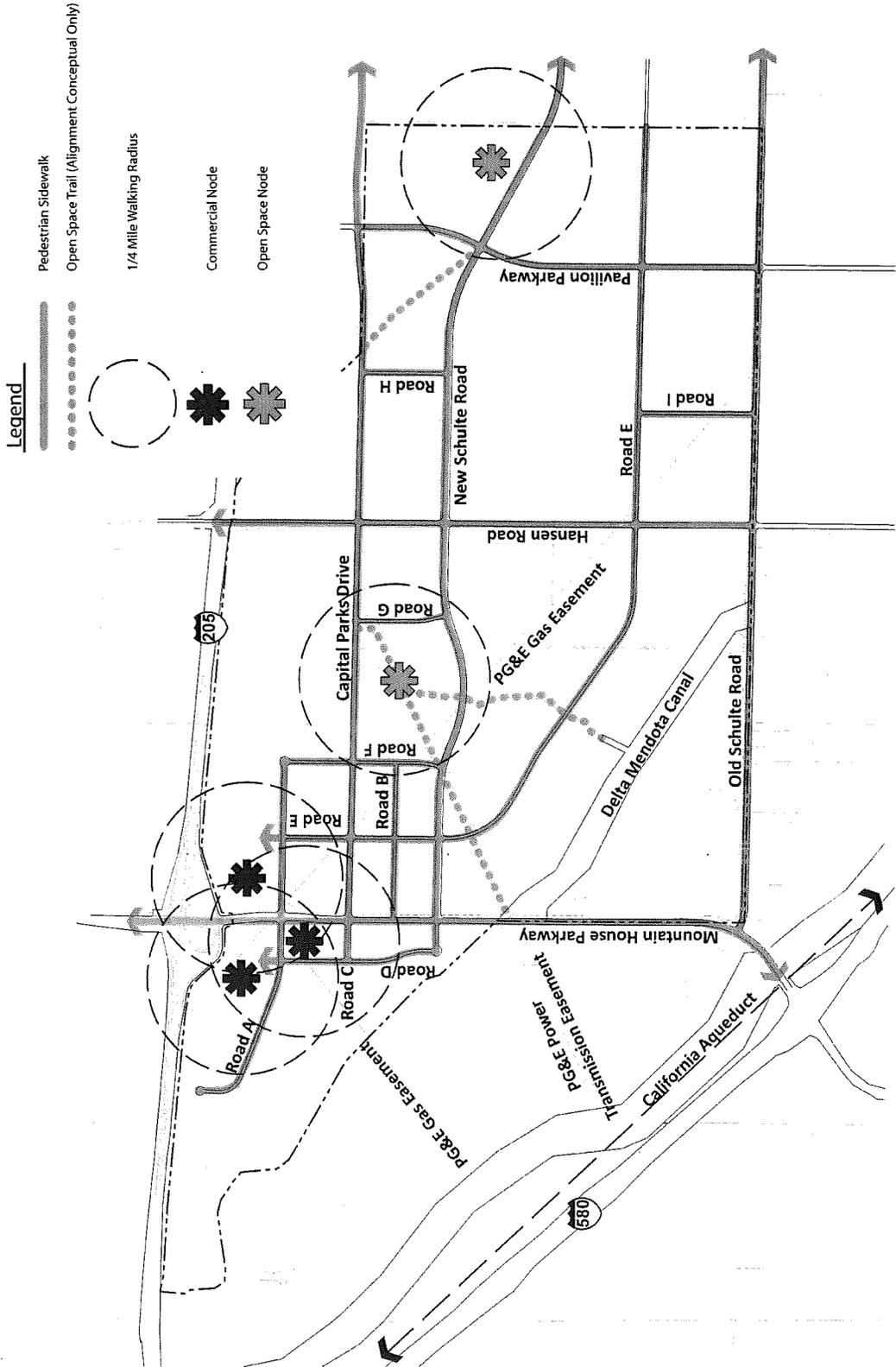


Figure 1.3, Pedestrian Path Plan

**a. Vision**

The vision for the Cordes Ranch Specific Plan is to create a commerce and business park, providing flexibility in development so that a variety of uses can be attracted to develop within the City of Tracy, see Figure 1.2.

**b. Buildout Land Use Summary**

The Specific Plan is intended to allow for flexibility in development. Table 1.1 presents the approximate acres of zoning districts and building square footage within Cordes Ranch as currently envisioned for buildout. The Project includes a mix of General Commercial, General Office, Business Park Industrial and Park uses. The zoning districts are in conformance with the General Plan designations of Industrial, Office, Commercial, and Park. The Project will attract a wide variety of businesses that will generate jobs, and provide for business development needs of the City of Tracy over the project buildout.

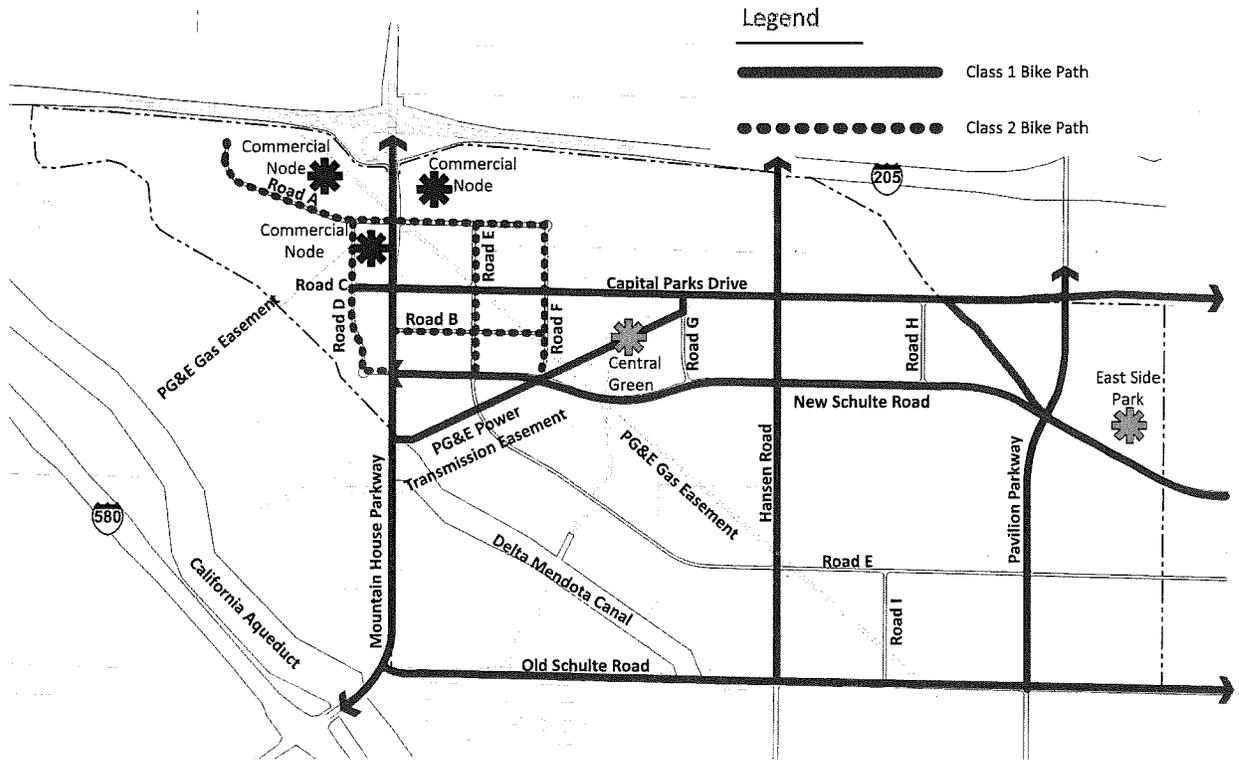


Figure 1.4, Bike Path Plan

**c. Plan Concept**

The Specific Plan is organized around an approximate 35-acre "Central Green" consisting of passive use open space areas, and a joint use park/stormwater detention facility. A strong open space and trails network will utilize the existing utility easements and drainage easements to provide passive use areas, and pedestrian and bicycle trails amenities that all connect to the Central Green, see Figure 1.3. The Project is designed on a grid street pattern to create additional linkages between development sites and includes separated sidewalks, and Class I and II bike-ways on a majority of the streets, see Figure 1.4. The design concept for the project is to maintain a high level of site design and architecture, especially along the major corridors, namely I-205, New Schulte Road, Capital Parks Drive, and the section of Mountain House Parkway north of the Delta Mendota Canal, see figure 1.5. The I-205 frontage is comprised of the properties abutting I-205 and portions of Mountain House Parkway and is intended for development of high identity businesses with an emphasis on commercial, office, and small scale business park indus-

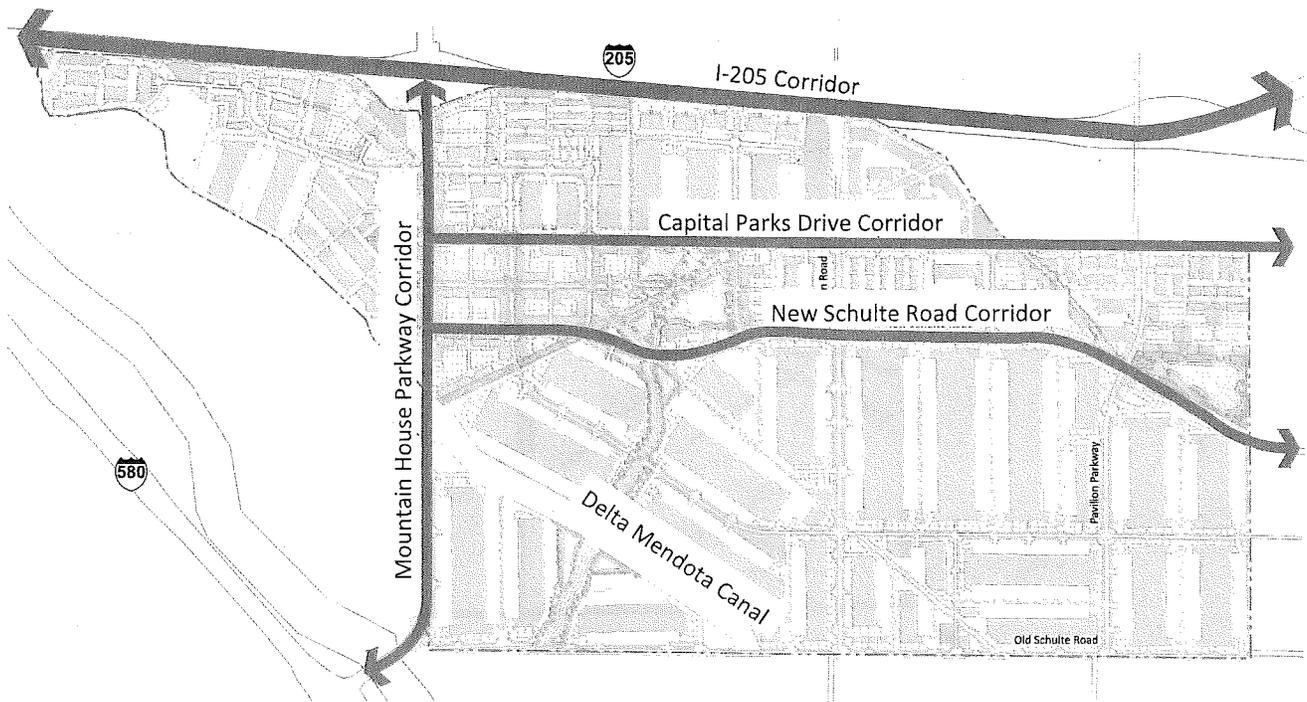


Figure 1.5, Areas of Special Design Standards

trial uses, see Figure 1.6. Parcels within 500' of I-205 are included within the I-205 Overlay Zone which requires higher development and design standards with an emphasis on building orientation, architectural design, and landscape planting and screening.

The area surrounding the west and east sides of the Central Green between Capital Parks Drive and New Schulte Road is important as a high identity area of the Specific Plan. The western most portion between Mountain House Parkway and the Central Green is zoned General office, and will provide for business services and uses that will support the larger functions of Cordes Ranch. The design intent is to create office uses which are pedestrian oriented and capitalize on the visibility from Mountain House Parkway, and provide pedestrian corridors to the Central Green, see Figure 1.7. The development character will include: wide sidewalks for pedestrians; buildings framing the street with the entries from the street and diagonal on-street parking.

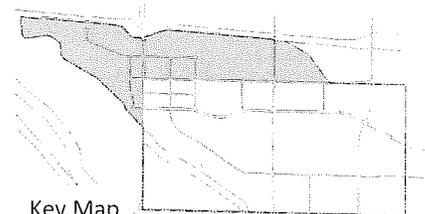
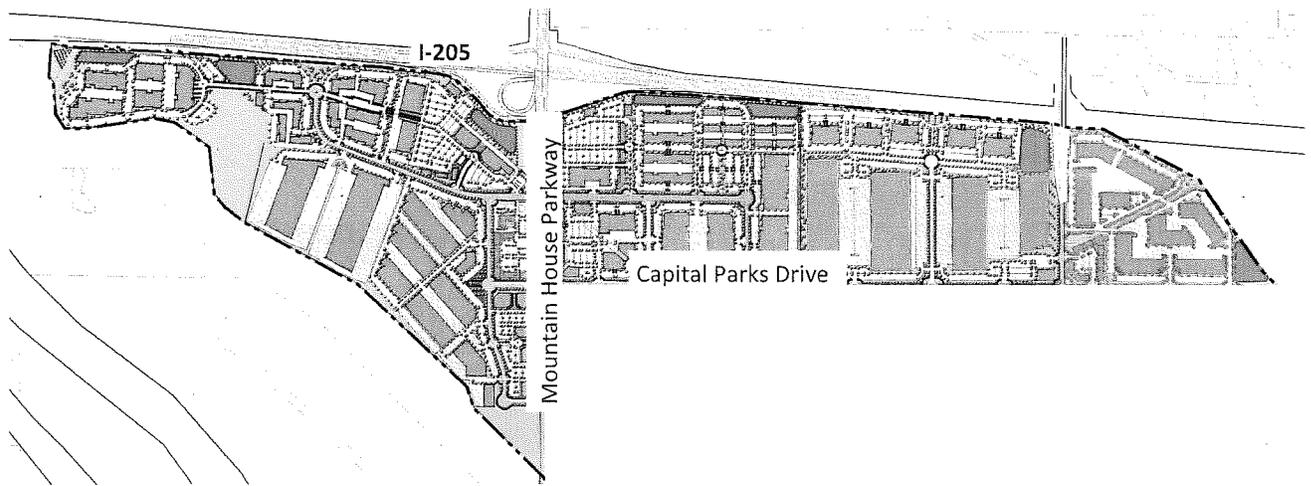


Figure 1.6, I-205 Corridor Conceptual Illustrative Development

Key Map

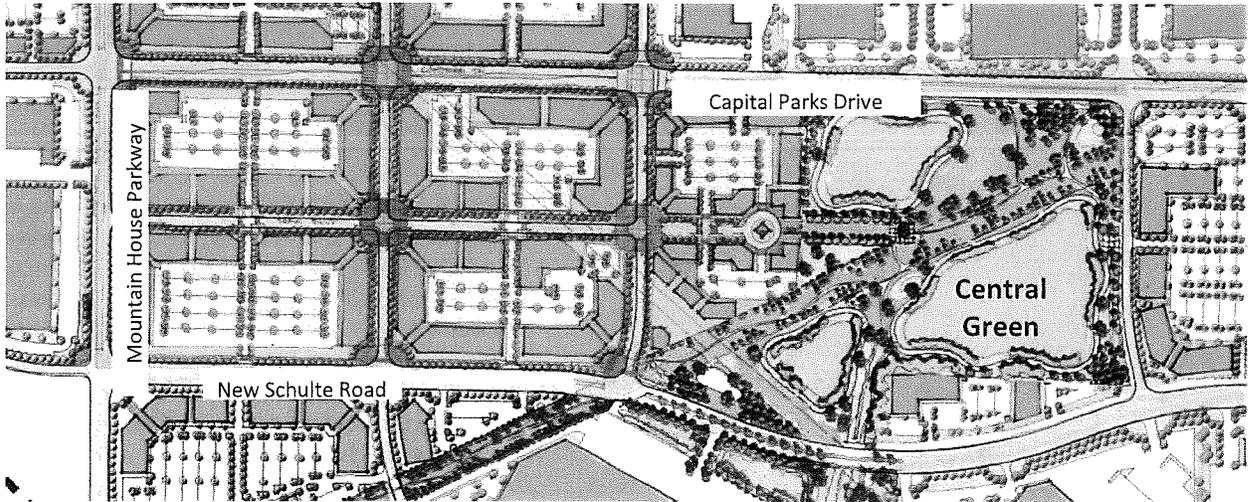
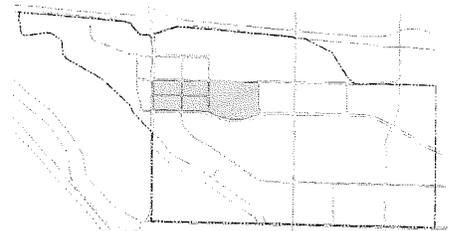


Figure 1.7, General Office Conceptual Illustrative Development



Key Map

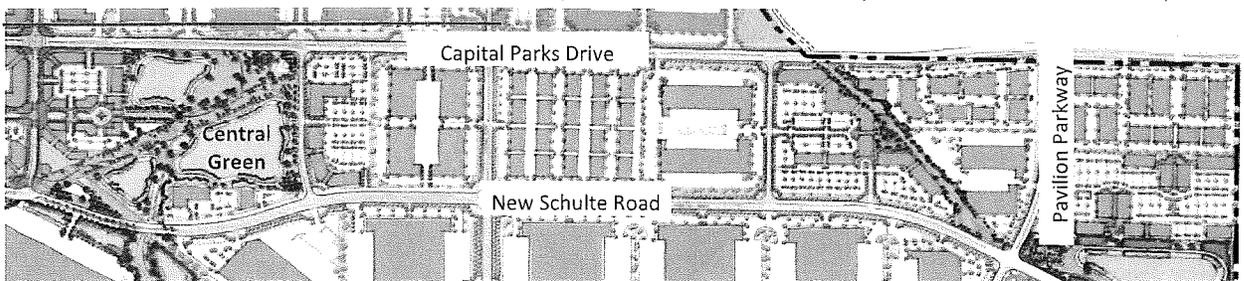
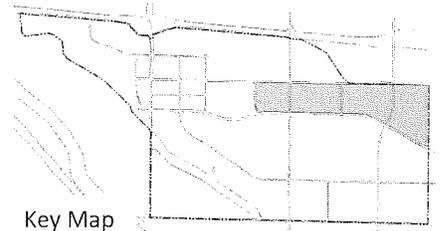


Figure 1.8, Business Park Industrial Conceptual Illustrative Development



Key Map

On the east side of the Central Green, uses will transition to Business Park Industrial uses, see Figure 1.8. Shorter block lengths are anticipated to accommodate and attract incubator users requiring smaller buildings. The street frontage along Capital Parks Drive and New Schulte Road will include additional landscaping for the location of Class I bikeway and pedestrian paths. These corridors will provide linkages between the Central Green and the Eastside Park. A trail and path may be included within the West Side Irrigation District (WSID) right-of-way if development of adjacent parcels allows. The pedestrian and bike network will connect to the Eastside Park at the eastern property boundary north of New Schulte Road.

The remainder of the Project Area south of New Schulte Road is intended for development of Business Park Industrial uses. The street pattern south of New Schulte Road is anticipated to include larger block lengths to allow for parcel sizes that accommodate the function and space requirements of Business Park Industrial buildings, see Figure 1.9. The street frontages along New Schulte Road, Hansen Road, and Pavilion Parkway will include additional landscaping to provide for screening of buildings and to accommodate a Class I bikeway and pedestrian path corridor providing linkages to the Central Green and the Eastside Park.

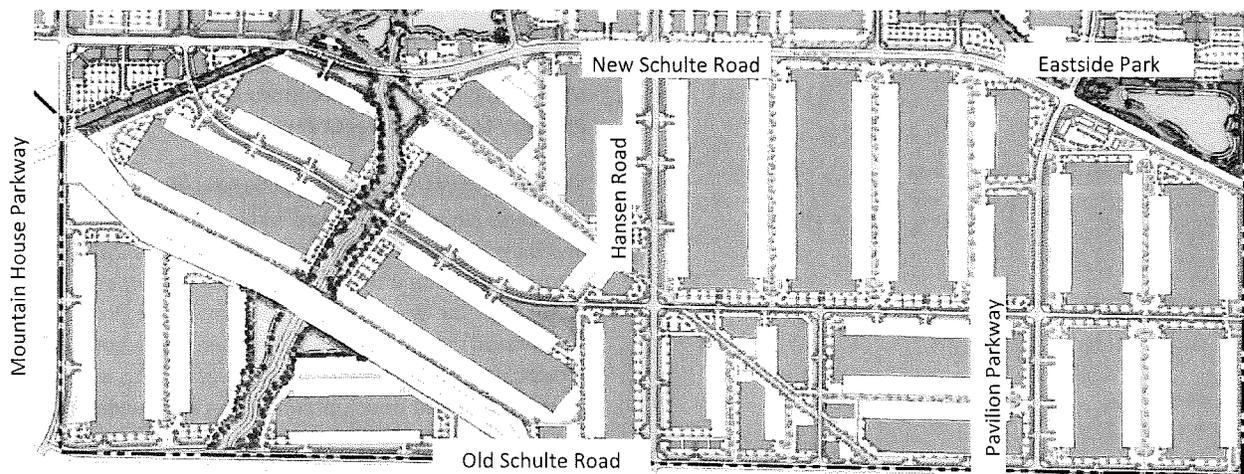
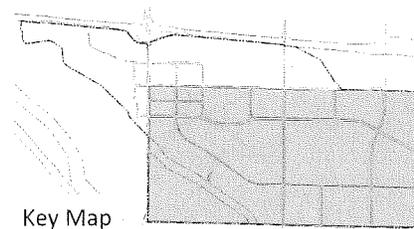


Figure 1.9, Business Park Industrial Conceptual Illustrative Development



A strong open space and trails network will utilize the existing utility easements and drainage corridor to provide passive use areas, and pedestrian and bicycle trail amenities for use by employees and the community. The network of trails will create links between the two park areas, providing for alternatives to vehicle transportation.

Figure 1.2 references a third park site, the Westside Open Space, in the northwest vicinity of the Project Area. This site is partially owned by the United States Bureau of Reclamation, and this parcel serves as storm drainage conveyance from off-site. The remainder of the area has been set aside as open space. This area is not anticipated for development and its intended use for the foreseeable future is as open space, to be zoned Park.

The existing PG & E transmission easement that bisects the site and extends through the Central Green will be utilized as an open space corridor and will include pedestrian and bicycle paths, see Figure 1.10. The ultimate location for the open space corridor along with the bicycle and pedestrian paths will be refined as part of the Project's subdivision map process. If the open space corridor is relocated outside the PG & E easement to accommodate adjacent development, then a 30' wide corridor shall be provided to maintain the connection from Mountain House Parkway to the Central Green.

The existing WSID right-of-way between Capital Parks Drive and New Schulte Road will include pedestrian and bicycle paths to connect to the Eastside Park, see Figure 1.11. The ultimate location for the open space corridor will be refined as part of the Project's subdivision map process. If the open space corridor is relocated outside the WSID right-of-way to accommodate adjacent development, then a 30' wide corridor will be provided to maintain the connection to

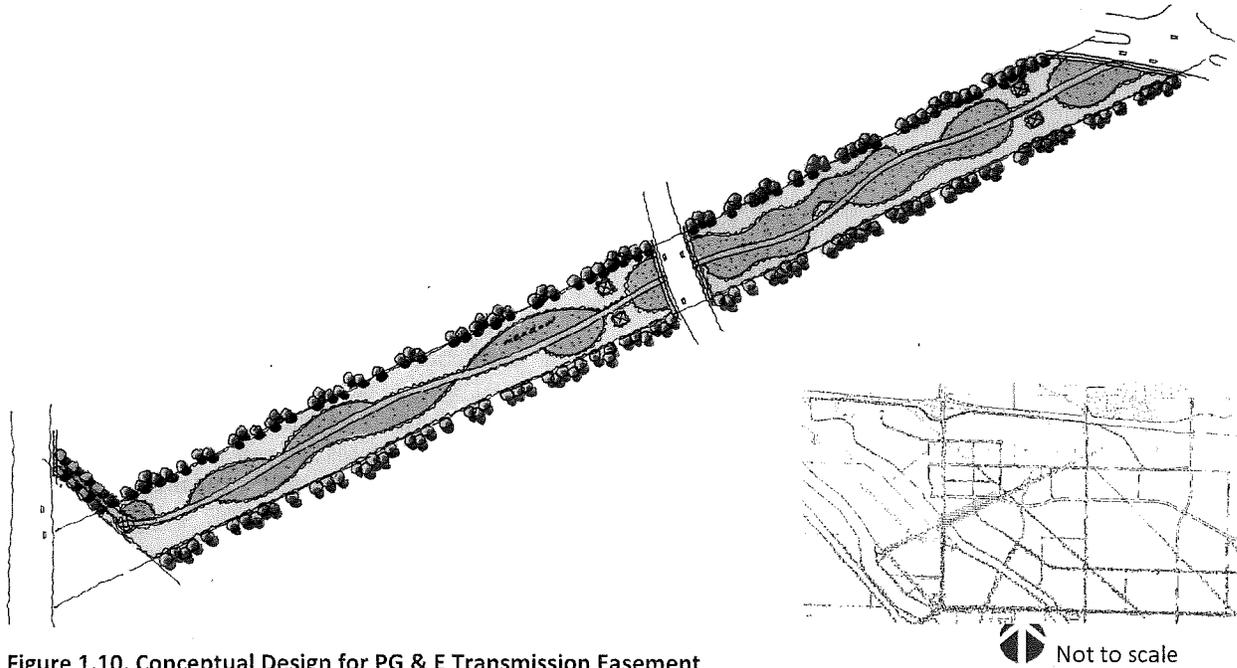


Figure 1.10, Conceptual Design for PG & E Transmission Easement

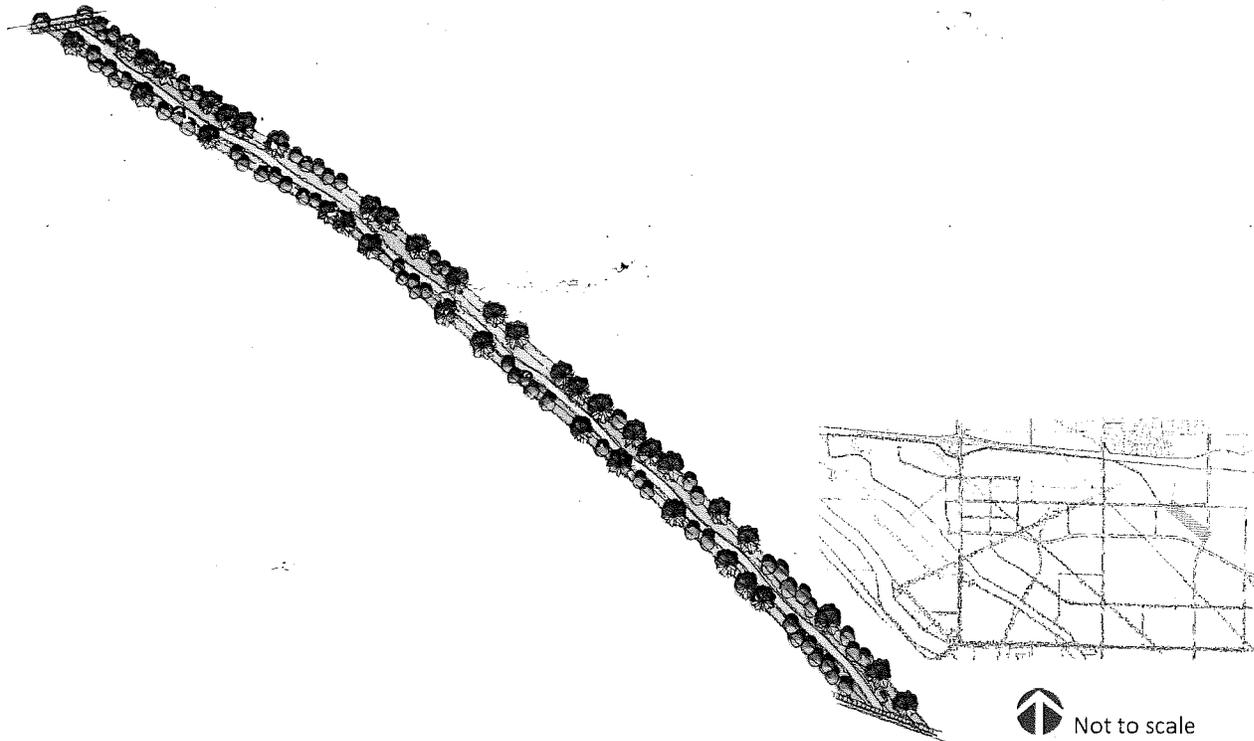


Figure 1.11, Conceptual Design for West Side Irrigation District Canal



**General Commercial**



**General Office**



**Business Park and Industrial**

the Eastside Park.

**d. Goals**

The Specific Plan will ensure that future development creates an identity of its own with a commitment to sustainability, thoughtful site design, and well designed buildings and public spaces. The following goals have been established for the Project.

- Accommodate a variety of land uses including highway and retail commercial; office; office/warehouse; light industrial; warehouse and distribution facilities to foster the growth of research and development and manufacturing and distribution uses.
- Capitalize on the existing transportation corridors of Interstates 580 and 205 and increased demand for manufacturing and distribution space from the Bay Area.
- Create opportunities to generate jobs and contribute to a vibrant workplace for the City of Tracy and the San Joaquin Valley.
- Create a thematic gateway to the City of Tracy, introducing the City's character with enhanced landscape treatments and sculptural monument signage along the I-205 freeway edge.
- Implement a range of sustainability measures aimed at conserving resources, decreasing energy and water consumption, and reducing air and water pollutants.

**1.2 CALIFORNIA GOVERNMENT STATUTORY REQUIREMENTS**

California Government Code Section 65451 requires that a specific plan include text and a diagram or diagrams which specify all of the following in detail:

- (1) The distribution, location, and extent of the uses of land, including open space, within the area covered by the plan.
- (2) The proposed distribution, location, and extent and intensity of major components of public and private transportation, sewage, water, drainage, solid waste disposal, energy, and other essential facilities proposed to be located within the area covered by the plan and needed to support the land uses de-

scribed in the plan.

(3) Standards and criteria by which development will proceed, and standards for the conservation, development, and utilization of natural resources, where applicable.

(4) A program of implementation measures including regulations, programs, public works projects, and financing measures necessary to carry out paragraphs (1), (2), and (3). The specific plan shall include a statement of the relationship of the specific plan to the general plan.

The specific plan may address any other subjects which in the judgment of the planning agency are necessary or desirable for implementation of the general plan.

**1.3 RELATIONSHIP TO OTHER PLANS**

**a. City of Tracy General Plan**

The Cordes Ranch Specific Plan includes land uses, amenities, and design elements that are consistent with the General Plan. Below is a review of the relevant key goals and objectives from the General Plan and a discussion of how the Cordes Ranch Project will help im-

plement the General Plan.

- **Objective LU-2.3 Expand the City's Industrial Base, P-3.** Cordes Ranch will provide for a variety of commercial office and high quality business opportunities along the I-205 corridor at the western edge of the City.
- **Objective CC-1.1 Preserve and enhance Tracy's unique character, P-2.** The Project will include a pedestrian friendly central core area of office and service commercial uses that will be linked to the adjacent Central Green area by means of open space corridors, pedestrian sidewalks and bicycle paths.
- **Objective CC-2.1 Maximize direct pedestrian, bicycle and vehicle connections in the City, P-1.** The Project has been designed on a grid pattern of through streets to create connectivity between uses, reduce vehicle miles traveled, orient buildings on an east-west orientation to take advantage of solar orientation, and to provide increased connectivity for pedestrians and bicy-

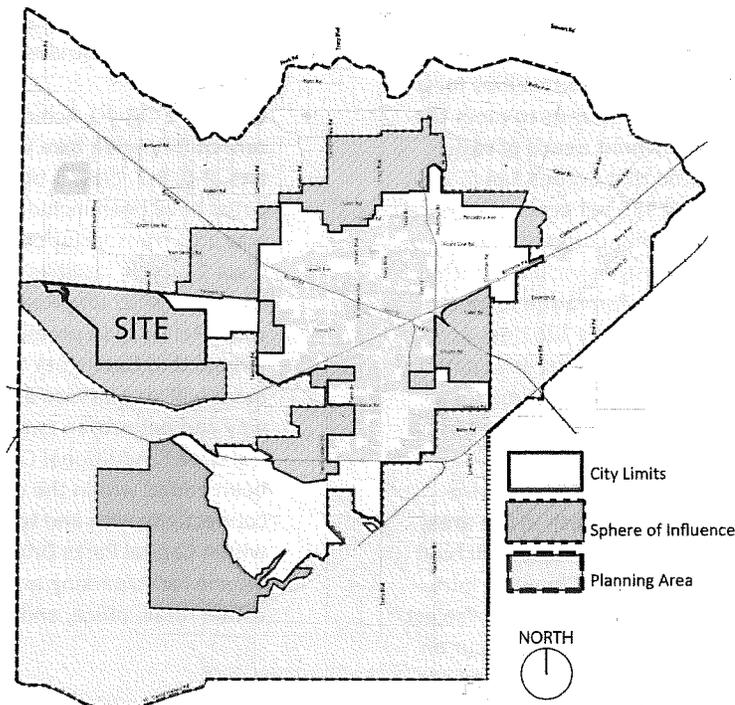


Figure 1.12, Project Vicinity

- cles.
- **Objective CC-9.1 Develop Village Centers that serve several Neighborhoods or Employment Areas, P-1.** General Commercial and General Office uses will provide services and goods for the employees within the Project. The General Office Uses have been located at the intersections of the major circulation roads for the Project at Mountain House Parkway, Capital Parks Drive, and New Schulte Road. Pedestrian paths and bike lanes and paths have also been designed as part of the Project to link businesses throughout the Project to the Central Green area.
- **Goal CC-11 Well designed Employment areas that are integrated with other parts of Tracy, P-1, P-3.** Cordes Ranch will include an approximately 35-acre Central Green that will provide for the main focal point for the Project and an amenity for the employees of the business park. Another approximate 18-acre Eastside Park and storm drain detention pond with dual use potential as a park is located at the eastern edge of the Project will provide for passive and active uses. The street network, Class 1 bikeways and pedestrian sidewalks have all been designed to connect with park amenities.
- **Objective ED-1.1 Attract emerging growth industries to increase employment opportunities for a wide range of skill levels and salaries to meet the current and future employment needs of residents, P-1.** It is anticipated that Cordes Ranch will provide for approximately 125 net acres of General Office uses which will allow for corporate headquarters and emerging industries including technical, finance, insurance, and information technologies uses. Another approximately 1291 net acres of Business Park Industrial uses is also anticipated which will allow for a variety of service, manufacturing, distribution, and warehousing related uses.
- **Objective ED-5.3 Support I-205/I-580/I-5 infrastructure as key to economic growth in the area, P-2, P-3.** The development of the Project will fund and extend infrastructure per the City-wide Infrastructure Master Plans from the east to the Project Area. This will allow for development to occur on the site and allow business to expand and provide employment opportunities along the I-205 and I-580 corridors.
- **Objective ED-6.7 Develop higher end office and office –flex uses, particularly along entryways to the City along I-205 and I-580, P-1, P-2, P-3.** Cordes Ranch will provide opportunities for the development of General Office uses that contain a variety of amenities such as plazas, framed open space areas and pedestrian pathways and connections to open spaces. Design standards and guidelines included in the Specific Plan support the design of high quality business spaces, and architecture along major entryways and streets within the Project.
- **Objective CIR-1.2 Provide a high level of street connectivity, P-3, P-5.** The road circulation network and block lengths within the Project Area will provide for a variety of uses. The streets will provide for multi-modal transportation including autos, trucks, and public transportation to promote a high level of connectivity between individual parcels and services within the Project Area. Arterial and collector streets will include Class 1 bike ways and pedestrian sidewalks. Industrial streets will include sidewalks to provide pedestrian circulation within the warehouse and distribution development areas. Street intersections will be designed to provide for safe crossing by pedestrians and bicycles.
- **Objective CIR 3.1 Achieve a comprehensive system of City-wide bike way and pedestrian facilities, P-6, P-7.** Class 1 bike ways and pedestrian paths have been included per the Citywide Roadway and Transportation Master Plan. Additional Class 1 bicycle facilities have been included within New Schulte Road, Hansen Road and Pavilion Parkway to promote safe bicycle travel on streets that have truck traffic. The PG & E easement will also include a combination Class 1 bike way and pedestrian path to link uses to the Central Green. Additional Class II bike ways have been added within the central area to provide connectivity with and to the Class I bike ways within Capital Parks Drive and New Schulte Road. Bicycle racks/parking areas will be included within retail, office, and manufacturing and dis-

- tribution projects.
- **Objective OSC-4.2 Ensure that new development is responsible for providing parks and recreation facilities throughout the City of Tracy.** The Project Area has been designed with an approximately 35-acre Central Green area and an approximately 18-acre Eastside Park, which include joint use detention/park facilities. The Project also preserves the existing wetland and drainage corridor and will include pathways and passive use areas. These facilities will provide park and outdoor use areas for employees and users of the business park and Tracy residents.
  - **Objective PF-6.5 Use recycled water to reduce non-potable water demands whenever practicable and feasible, P-2.** The Project Area will include a recycled water "purple pipe system" to utilize tertiary treated water when available from the wastewater treatment plant .
  - **Objective PF-7.3 Promote coordination between land use planning and wastewater conveyance, treatment and disposal, P-3.** Cordes Ranch property owners will pay their proportional share of the cost of wastewater treatment and conveyance facilities and infrastructure identified in the Tracy Wastewater Master Plan. The property owners will also set up an "owners association" responsible for the operation and maintenance of private/public facilities that will be identified as the Project infrastructure is designed.

**b. City of Tracy Infrastructure Master Plans**

This Specific Plan has been prepared in conformance with the ultimate improvements depicted in the City-wide infrastructure Master Plans.

**c. Airport Plans**

The Tracy Municipal Airport is located approximately 3.3 miles south and east of the Specific Plan boundary. The San Joaquin County Airport Land Use Compatibility Plan, June 2009, depicts the Airport Influence Area extending to approximately the intersection of South Lammers Road and Valpico Road, 1.3 miles south and east of the Project Area. The Safety Element of the General Plan requires new development to be consistent with both the County and City airport plans. The Cordes Ranch Specific Plan does not conflict with either

plan since it is outside the Airport Influence area.

**1.4 USE OF THE SPECIFIC PLAN**

The Cordes Ranch Specific Plan provides architects, urban planners, landscape architects, and developers with the necessary tools for the design and generation of development proposals for submission to the City. The City of Tracy will utilize this document to evaluate development proposals against the goals, objectives, design and development standards, and guidelines in making the findings for individual project approval.

The Specific Plan is divided into 8 chapters that provide the development and design standards and guidelines for the project and will ensure for the level of quality the City has envisioned for the Project Area. Outlined below is a brief description of the content within the remaining chapters of the Specific Plan.

**Chapter 2-Existing Site Conditions**

Chapter 2 outlines the site context including existing conditions, topography, easements, drainage corridors, and existing utility infrastructure and roadways.

**Chapter 3-Land Use, Zoning, and Development Standards**

Chapter 3 further describes the Project concepts, land use pattern, and zoning districts that will be utilized to manage development. Development standards have been developed for each zoning district that dictate permitted and conditionally permitted land uses, setbacks, building heights, floor area ratios, parking, and landscaping standards.

**Chapter 4-Design Guidelines**

Chapter 4 presents the design guidelines that will be used in conjunction with development standards in Chapter 3 to generate site plans, building architecture, and landscape architecture designs for the various development parcels. Included in the chapter are imagery and preliminary concept plans to illustrate

the intent of the guidelines.

#### **Chapter 5-Master Landscape Plan**

Chapter 5 presents the landscape themes, concepts, and guidelines that will be used to create the strong framework and backdrop that will unify the Project Area and streetscapes.

#### **Chapter 6-Streets and Infrastructure**

Chapter 6 outlines the road and other infrastructure improvements necessary to support the level of development intensity proposed by the Project, the sources of anticipated infrastructure funding for construction, and the conceptual phasing of these improvements. It also provides descriptions and concepts for vehicle, truck, bicycle, and pedestrian circulation networks.

#### **Chapter 7-Natural Resources and Sustainability**

Chapter 7 describes the preservation and enhancement of the existing drainage corridor and other site resources and habitat areas. The chapter also includes sustainability guidelines to reduce vehicle trips and conserve resources and energy.

#### **Chapter 8-Plan Review and Administration**

Chapter 8 outlines the development application review process and the submittal requirements.

### **1.5 DESIGN PROCESS**

The design process for each parcel will generally consist of three steps, see Figure 1.11.

Step one is to review Chapter 3 to determine land uses which are permitted and conditionally permitted, verify the development standards and setbacks, and determine the allowable intensity of development based on parking and building heights.

Step two is review the design guidelines that apply to direct the development of site plans, architecture, and landscaping to create the quality of development anticipated by the City of Tracy, in accordance with this Specific Plan.

Step three is to prepare a development application for review and consideration by the City of Tracy. Development applications shall be prepared as required by the Tracy Municipal Code.

## How to use the Specific Plan

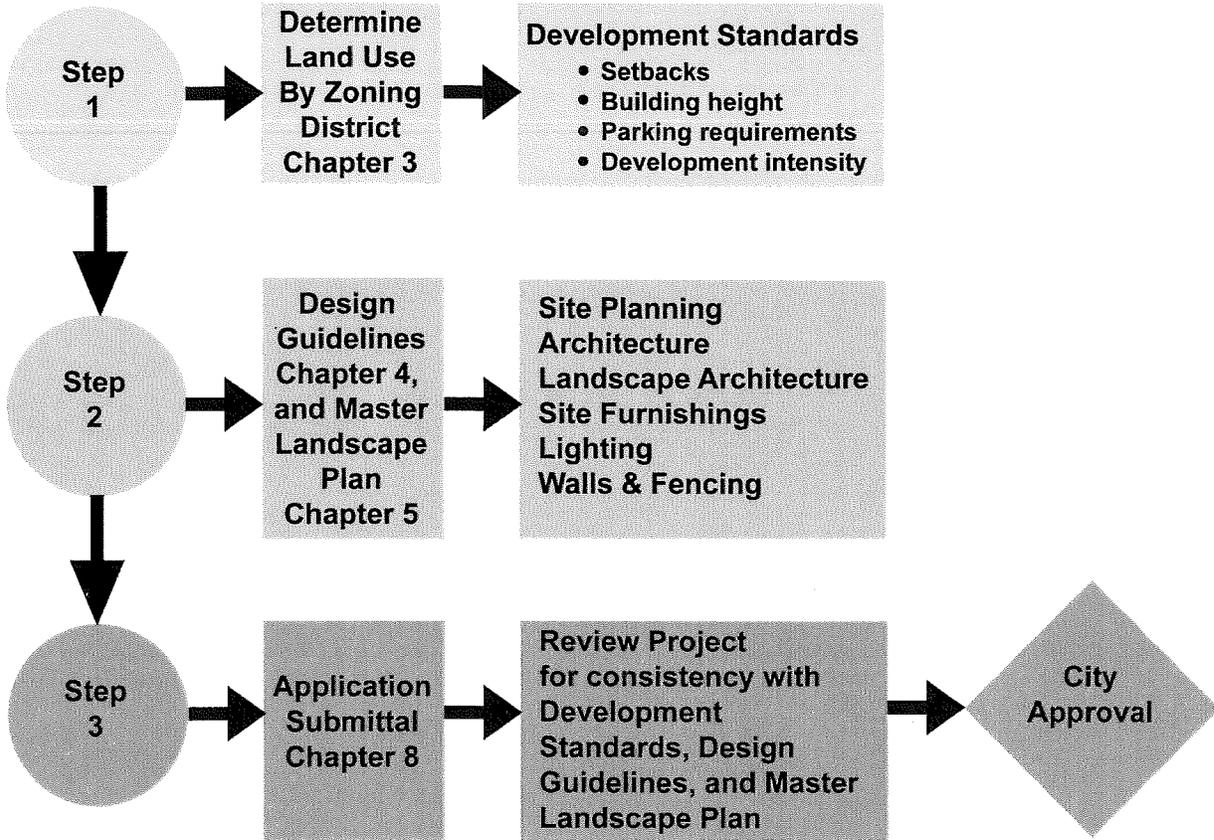


Figure 1.11, Development Process





**2.4 EXISTING PUBLIC SERVICES**

An existing fire station is located at the southwest corner of Hansen Road and Old Schulte Road. The station is anticipated to provide the necessary fire protection services for the project, see Figure 2.3.

**2.5 EXISTING UTILITY AND DRAINAGE EASEMENTS**

A number of utility easements traverse the Project Area, see Figure 2.4. An approximately 40' and 80' Pacific Gas and Electric (PG & E) transmission line easements enter the mid-portion of the Project Area from the southwest and extend diagonally and exit to the northeast near I-205. Two PG & E gas line easements with widths of approximately 50' and approximately 15' extend from southeast at the southern Project Area boundary, through the PG & E maintenance yard and exit near the northwest boundary at I-205. There is a 12" Chevron oil pipeline located within the 50' PG & E easement. A third approximately 50' wide high pressure PG & E natural gas easement extends from the southwest near Mountain House Parkway and terminates at the PG & E maintenance yard. A fourth PG & E natural gas ease-

ment containing a low pressure gas line begins at the PG & E maintenance yard and extends east to an electrical transmission easement near Hansen Road. This gas line will either be abandoned and removed or relocated within Capital Parks Drive right-of-way as part of development under this Specific Plan.

**2.6 EXISTING UTILITIES**

Existing water and wastewater mains located within the Hansen Road and Old Schulte Road right of ways currently serve the Patterson Pass Business Park, which is adjacent to the Project Area to the south. An existing 24" water transmission main extends west within Old Schulte Road and terminates at Mountain House Parkway. From the transmission main, a 14" water line extends north within Mountain House Parkway along the western property boundary. An existing 21" wastewater sewer trunk line extends from Old Schulte Road north to Hansen Road then to the northeast Project Area boundary.

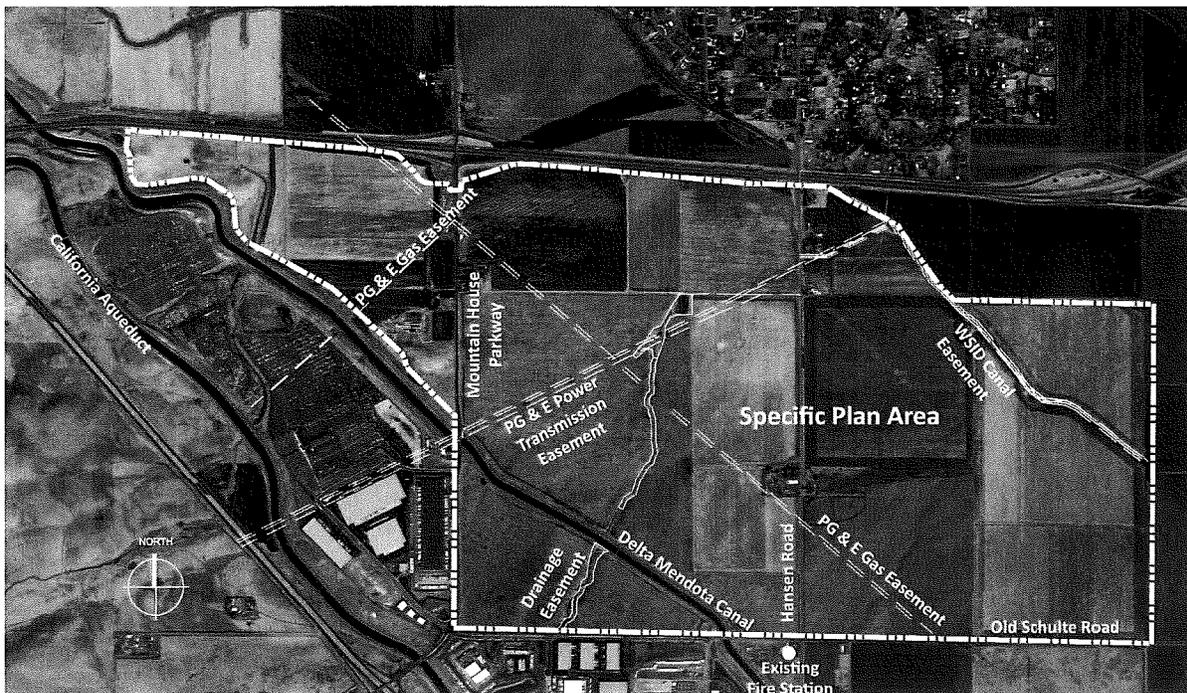


Figure 2.3, Aerial Photo

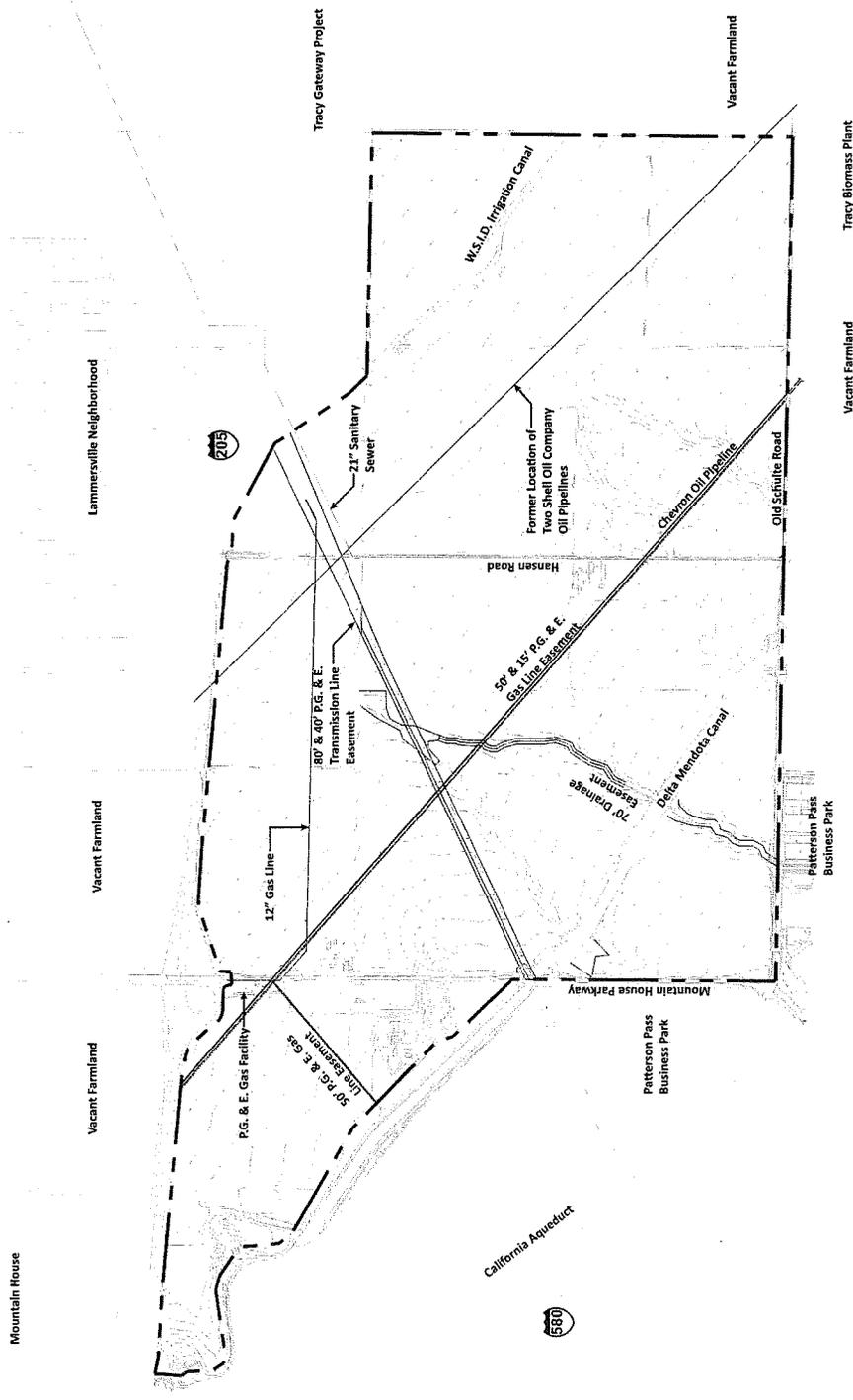


Figure 2.4, Topography & Existing Utility & Drainage Easements

## 2.7 FORMER OIL PIPELINES

Shell Oil Company owned two crude 8" oil pipelines that were reportedly installed in the 1920's and abandoned in place in the mid-1960's. These were known as the Central Valley Crude Oil Pipelines and were buried parallel in the approximate location shown on Figure 2.4. Shell Oil Company removed these two oil pipelines in 2001. During the removal process, contamination was discovered. Shell has been required to undertake remediation efforts in accordance with applicable laws and regulations, under the supervision of RWQCB. Currently, there are no active remediation efforts underway, although RWQCB (SLIC # 0607708243) continues to oversee this issue and may require additional remediation from Shell Oil Company in the future.

MARCH, 2013

**DRAFT MARCH 2013**  
CORDES RANCH SPECIFIC PLAN: TRACY, CALIFORNIA

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**CHAPTER 3  
LAND USE, ZONING, AND DEVELOPMENT STANDARDS**

**3.1 INTRODUCTION**

The vision for the Cordes Ranch Specific Plan is to create a state of the art commerce and business park, that provides flexibility so that a wide variety of uses can be attracted to develop within the City of Tracy. The site's proximity to the San Francisco Bay Area's deep water port facilities, two major freeways, and rail transportation create the ideal location for businesses that serve the greater Bay Area and those that require distribution of products throughout California.

Permitted land uses within Cordes Ranch are grouped into four zoning districts: General Commercial, General Office, Business Park Industrial, and I-205 Overlay. The General Commercial Zone provides for retail commercial goods and services which are oriented to the community and Cordes Ranch. The General Office Zone provides office space for professional services, research and development. The Business Park Industrial Zone provides for warehouse,

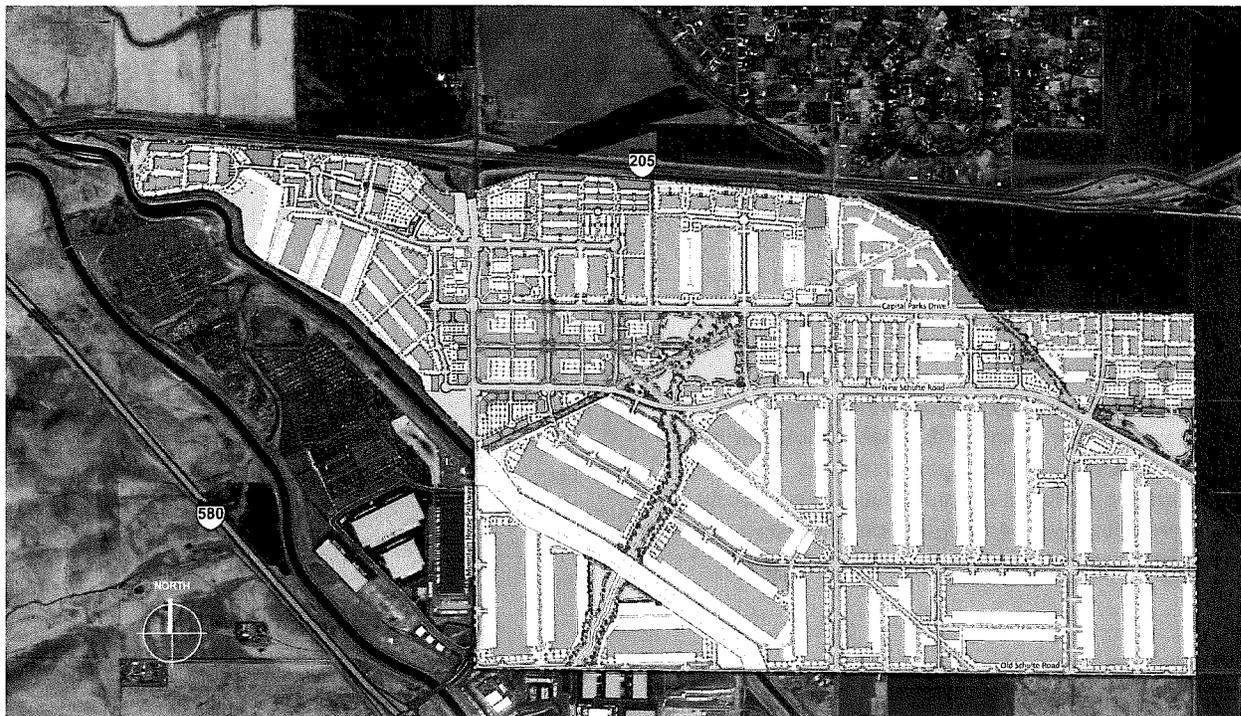


Figure 3.1, Project Concept

distribution, manufacturing, storage, industrial flex, and distribution related uses.

Parcels within 500' of I-205 are included within the I-205 Overlay Zone, which requires higher development and design standards with emphasis on building orientation, architectural design, and landscape planting and screening.

Development flexibility is created through a wide range of permitted and conditionally permitted uses, which anticipate the current and future development market, and development standards, which guide the design of buildings to meet the requirements of users with a commitment to sustainability and quality architecture.

### 3.2 ZONING DISTRICTS

The Cordes Ranch Specific Plan incorporates four zoning districts and an overlay zone with a variety of allowed uses. The zoning districts allow for flexibility to accommodate the future needs of the project and the City of Tracy. The following sections describe the zone districts and the intent of each, permitted and conditionally permitted uses, allowable floor area ratios, and other development standards. Chapters 4 and 5, Design Guidelines and Master Landscape Plan, will further guide development within the Project Area and will be used in conjunction with the development standards in this chapter.

#### General Commercial (GC)

The General Commercial Zone includes highway and retail commercial uses and business services designed to serve the immediate Project as well as the broader region.

#### General Office (GO)

The General Office Zone includes office, office/warehouse, research, light industrial, manufacturing and other service-related business services. This zone can also contain a limited amount of supportive and compatible commercial uses (such as restaurants) or other small-scale business-serving retail uses.

#### Business Park Industrial (BPI)

The Business Park Industrial Zone includes warehouse, distribution logistic facilities, manufacturing, assembly, and production uses.

#### I-205 Overlay Zone

The I-205 Overlay Zone includes property within 500' of I-205. The Overlay Zone addresses the high visibility of development adjacent to I-205. This zone includes a refined range of uses from the BPI Zone to promote high visibility development opportunities that allow a blend of office with light assembly, manufacturing, and business industrial flex uses.

#### Parks (P)

The Park Zone is designed to provide for open space areas and park facilities which offer recreational, cultural, entertainment, community gardens, and similar uses.

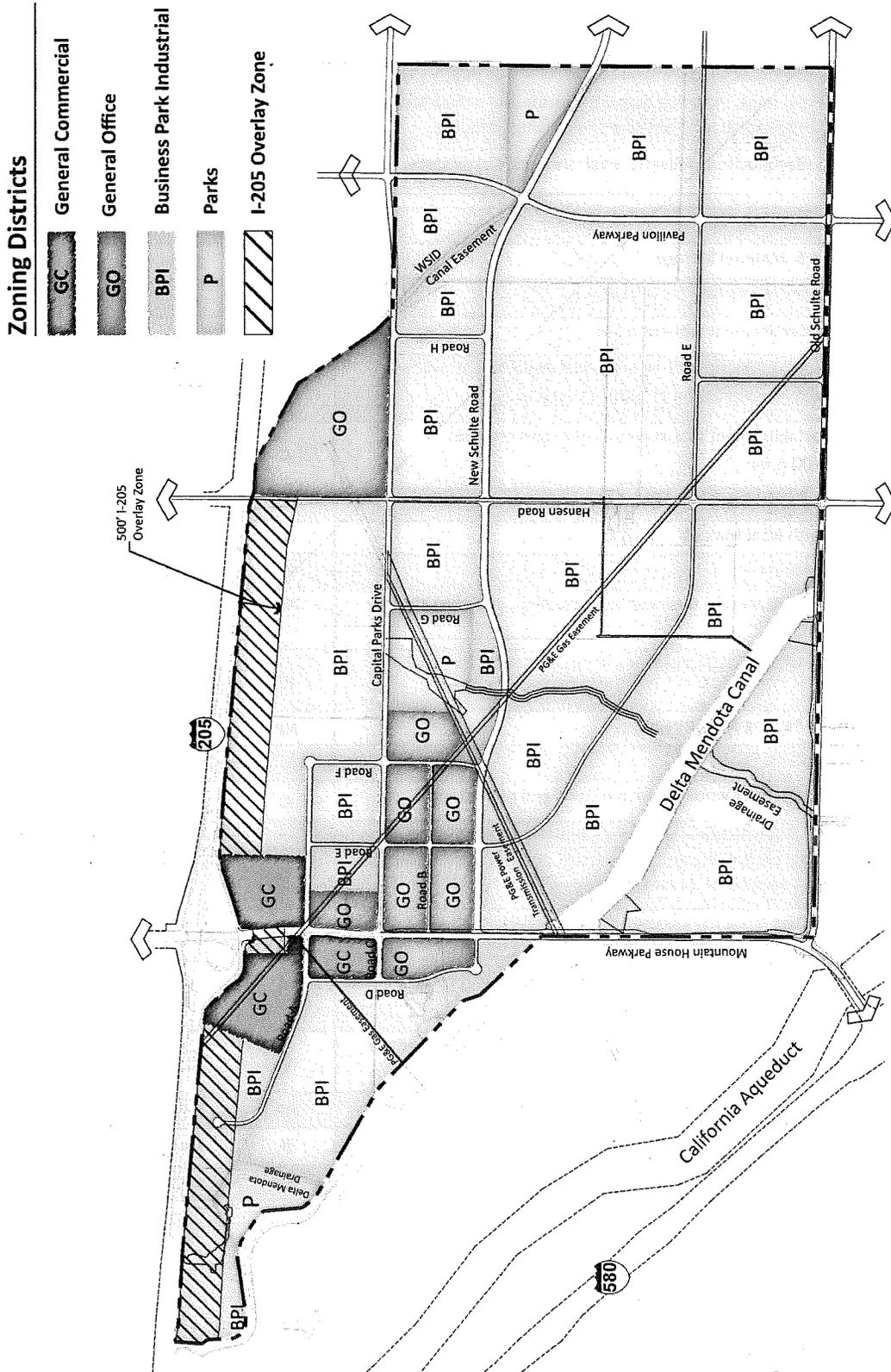


Figure 3.2., Cordes Ranch Specific Plan Zoning Districts

**Permitted and Conditionally Permitted Uses**

Uses	General Commercial (GC)	General Office (GO)	Business Park Industrial (BPI)	I-205 Overlay
Agricultural Processing, Sales, and Services Includes: Packing and shipping of agricultural products. Processing, including canning freezing and dehydrating. Wine grape processing and making, wine bottling and packing, shipping.	NP	NP	P <sup>4</sup>	C <sup>4</sup>
Business Services (e.g., reproduction, delivery, repair services, postal store, and restaurant supply).	P	C	P	P
Contract Construction	NP	NP	P	NP
Construction Equipment & Material Storage	NP	NP	P <sup>3</sup>	NP
Day Care Centers (e.g., community care facilities)	C	P	C	C
Eating and/or drinking establishment without a bar.	P	P	P	P
Eating and/or drinking establishment (with or without entertainment) without serving alcohol and providing entertainment <sup>7</sup> after 11:00 p.m.	P	NP	NP	NP
Eating and/or drinking establishment that serves alcohol and provides entertainment <sup>7</sup> after 11:00 p.m.	C	NP	NP	NP
Equipment Rental and Sales Construction Equipment	NP	NP	P	NP
Gas & Service Stations with Mini Market	P	C	P	P
Lodging (e.g., hotels, motels)	P	C	C	C
Manufacturing, Processing, Assembly, Business Industrial Flex, including storage and shipping uses.	NP	NP	P <sup>4, 6,</sup>	P <sup>4, 5, 6,</sup>
Offices (e.g., Business, professional, laboratories, medical/ dental, financial services)	P <sup>1</sup>	P <sup>1</sup>	P <sup>1</sup>	P <sup>1</sup>
Off-site Truck and Trailer Parking and Storage	NP	NP	C	NP
Park & Ride or Off-site Parking Facilities	C	C	C	C
Places of Assembly (e.g., places of worship, private clubs and related uses)	C	C	C	C
Recreational, Educational & Instructional Uses (e.g., miniature golf, bowling alley, instructional or educational performing arts, gymnastics, post-secondary education (including school campus), vocational training, tutoring services, etc).	C	C	C	C
Recycling Collection Facilities	NP	NP	P <sup>4</sup>	NP
Retail & Consumer Services (e.g., building materials and hardware stores, garden center, clothing and shoe stores, department stores, drug stores and grocery stores, and personal services such as nail, hair and tanning salons).	P	NP	NP	C
Retail & Consumer Services as ancillary uses oriented to serve the daily needs of workers in the GO and BPI	P	C	C	C
Truck Stops, Truck Fuel Stations, Truck Wash Facilities, and Truck Repair services	NP	NP	C <sup>2</sup>	NP
Vehicle Sales, Service, & Rental	P	NP	NP	C
Warehouse & Distribution	NP	NP	P	NP

P = Permitted  
C = Conditionally Permitted  
NP = Not Permitted

Table 3.1, Permitted and Conditionally Permitted Uses

**Table 3.1 Notes:**

1. These uses shall be allowed to include warehousing and storage as an accessory use.
2. Truck stops are not permitted north of Capital Parks Drive and west of Mountain House Parkway.
3. These outdoor storage uses must be completely screened from view from I-205 and public streets.
4. All of these uses must be conducted wholly within a building, including storage.
5. Permitted only in buildings 75,000 square feet or smaller.
6. Includes accessory space for showrooms/sales.
7. "Entertainment" means such uses as live music, disc jockeys, dancing, karaoke, comedy shows, modeling, or live performances.

**3.3 PERMITTED AND CONDITIONALLY PERMITTED USES**

Table 3.1 presents the permitted and conditionally permitted land uses within the Project Area. In addition, accessory uses and temporary uses shall be allowed as provided in the Tracy Municipal Code including temporary construction activities and on-site construction staging areas with concrete and/or asphalt batch facilities. Accessory uses and temporary uses shall be allowed as provided in the Tracy Municipal Code, including temporary construction activities and on-site construction staging areas with concrete and/or asphalt batch facilities.

Nonconforming agricultural uses within the Project Area shall be broadly interpreted to allow continued agricultural operations until development in conformance with this Specific Plan occurs. Agricultural crops or operations may change to another, such as row crops to orchards, without the property losing its non-conforming status. Such uses may include the continued use of barb wire fencing.

Table 3.2 presents the prohibited uses which will not be allowed to develop within any zoning district within the Cordes Ranch Specific Plan.

<b>Prohibited Uses (All Categories)</b>
<b>Uses</b>
Adult Businesses or Adult Uses as defined in the T.M.C.
Massage Parlors
Trash Transfer Stations
Outdoor Recycling Facilities
Composting Facilities
Junk Yards and Automobile Wrecking Yards
Explosives Handling
Funeral and Interment Services
Animal, Poultry, and Fish Farming, Including Breeding, Raising, Maintaining, or Slaughtering
Any Use Prohibited by State or Federal Law
Any Use Not Listed in Table 3.1.

Table 3.2, Prohibited Uses

### 3.4 DEVELOPMENT STANDARDS

Development standards have been prepared for each of the zoning districts outlined in section 3.2. Table 3.3 presents the standards for development which include minimum lot requirements, minimum setback requirements, maximum building heights, and landscape setbacks. Modifications in these standards may be necessary to respond to unique site characteristics and/or changes in development requirements to respond to market conditions. Modifications to these standards will require Planning Commission and City Council review per the City of Tracy Municipal Code requirements. Unless otherwise established herein, all definitions and land use terms shall be as stated in the Tracy Municipal Code.

**Development Standards by Zoning District**

	General Commercial (GC)	General Office (GO)	Business Park Industrial (BPI)	I-205 Overlay Zone
<b>Building Coverage and Height</b>				
Minimum Lot Size <sup>1</sup>	N/A	N/A	N/A	N/A
Minimum Lot Frontage <sup>2</sup>	N/A	N/A	N/A	N/A
Minimum Lot Depth <sup>1</sup>	N/A	N/A	N/A	N/A
Floor Area Ratio (F.A.R.) maximum	30%	45%	50%	40%
Maximum Building Area	N/A	N/A	N/A	See Table 3.1 for building size limitations
Maximum Height <sup>2</sup> (Building height is the vertical distance measured from the average elevation of the front street curb to the highest point of the structure. Where buildings are set back from the front lot line, the height shall be measured from the average elevation of the finished grade of the front yard)	80'	80'	100'	80'
<b>Building Setbacks (as measured from property line)</b>				
Front Yard/Street Setback	See Private Frontage Landscaping for minimum building setbacks	See Private Frontage Landscaping for minimum building setbacks	See Private Frontage Landscaping for minimum building setbacks	See Private Frontage Landscaping for minimum building setbacks
Side Yard Setback (non street)	10'	10'	10'	10'
Rear Yard Setback (non street)	10'	10'	10'	10'
I-205 Setback	30' Min	30' Min	N/A	100' Min
<b>Private Frontage Landscaping (as measured from property line)</b>				
Mountain House Parkway	30'	30'	30'	30'
Capital Parks Drive	N/A	25'	25'	25'
New Schulte Road	30' @ Class 1 bike path 25' @ sidewalk	30' @ Class 1 bike path 25' @ sidewalk	30' @ Class 1 bike path 25' @ sidewalk	N/A
Old Schulte Road	25'	25'	25'	N/A
Hansen Road	25'	25'	25'	25'
Pavilion Parkway	25'	25'	25'	N/A
Street Section E	15' At Class 1 bike path 25' at sidewalk	15' At Class 1 bike path 25' at sidewalk	15' At Class 1 bike path 25' at sidewalk	15' At Class 1 bike path 25' at sidewalk
Street Section F	15'	15'	15'	15'
Street Section G	15'	15'	15'	15'
Street Section H	15'	15'	15'	15'
Street Section I	15'	15'	15'	N/A
Street Section J	N/A	0'	N/A	N/A
I-205 Setback	30' Min	30' Min	N/A	30' Min

**Notes:**

1. No lot shall be created with size or dimensions rendering it incapable of meeting the land use, public utilities, or development standards of this Specific Plan.
2. Structures in the Project Area may exceed 80' in height upon approval of a Conditional Use Permit.

**Table 3.3, Development Standards**

**3.5 OFF-STREET PARKING**

The Tracy Municipal Code off-street parking requirements shall apply to the Cordes Ranch Specific Plan except as modified herein. On-street diagonal parking on Street Section J, along the parcel frontage of Streets B, E, and F may be counted as part of the off-street parking requirement.

<b>Required Off-Street Parking</b>	
<b>Use</b>	<b>Required Parking based on use</b>
Retail Commercial	One space per 250 square feet of gross floor area.
Vehicle Sales and rentals including RV's and mobile homes.	One space per 250 square feet of gross floor area, plus one space per vehicle for sale or stored on lot.
Offices: businesses, professional (not including medical or dental), and banks.	One space per 250 square feet of gross floor area.
Dental or medical clinics or offices	One space per 200 square feet of gross floor area.
Motor vehicle repair garages	One space per 600 square feet of gross floor area; repair stalls not counted as parking spaces.
Cafes, restaurants and other establishments for the sale and consumption of food and beverages	Dining: one space per 45 square feet of customer area and one space per 250 square feet of all other areas.
Hotels and motels	One space per guest room.
Warehouse and storage buildings	One space per 1,000 square feet of the first 20,000 square feet of gross floor area, plus one space per 2,000 square feet of the second 20,000 square feet of gross floor area, plus one space per 4,000 square feet of the remaining square feet of gross floor area.
Manufacturing, processing, and assembly	One space per 600 square feet of gross floor area, or if the number of employees on the maximum work shift can be verified, one space per one employee on the maximum work shift.
Auditoriums, churches, sports arenas, theaters and other places of assembly	One space per five (5) fixed seats or, if the assembly does not have fixed seats, one space per 60 square feet of assembly area. If the number of parking spaces required for the sum of all accessory uses does not exceed the number of parking spaces required for the assembly area no additional parking is required. Uses which are not accessory to the assembly use, shall provide additional off-street parking in accordance with City parking regulations.
Street Section J	On-street diagonal parking on Street Section J, along the parcel frontage of Streets B, E and F may be counted as part of the off-street parking requirement.

Table 3.4, Required Off-site Parking

**3.6 LANDSCAPE STANDARDS FOR OFF-STREET PARKING AREAS**

Off-street parking areas will require landscaping per the standards established by the Tracy Municipal Code. Parking area landscaping shall be provided in accordance with Tracy Municipal Code standards unless otherwise provided herein.

<b>Landscape Standards for Off-Street Parking Areas</b>	
<b>a. Landscaping Shall Be Installed at the Following Rate:</b>	
<b>Number of Required Auto Spaces</b>	<b>Percent of Parking Area in Landscaping</b>
1 through 15	5%
16 through 30	10%
31 through 60	15%
Over 60	20%
<b>b. Landscaping shall consist of plant materials and shall include a combination of trees, shrubs, and ground cover.</b>	
<b>c. Trees shall be of a type approved by the Development Services Director. Trees shall be required at the rate of one tree per each five required auto parking spaces. Such trees shall be a minimum 24" box in size. Canopy trees shall be evenly distributed throughout the parking area so that 40 percent of the area shall be shaded at tree maturity. Canopy trees shall be the type that normally achieves a minimum canopy diameter of 25 feet, as approved by the Development Services Director.</b>	
<b>d. Trees shall be planted according to the City of Tracy Standard Specifications for street trees.</b>	
<b>e. The requirements for parking lot landscaping for industrial development, as indicated in this section, may be decreased by a maximum of 50% provided a corresponding minimum increase in perimeter landscaping of fifty (50%) percent is provided.</b>	
<b>f. Screening of the parking area from public rights-of-way shall be provided at a minimum height of two and one-half feet and a maximum height of three feet measuring from the top of the parking area pavement.</b>	
<b>g. Screening from public rights-of-way may consist of one or a combination of the following:</b>	
<ol style="list-style-type: none"> <li>1. Berms landscape with ground cover, trees, and shrubs;</li> <li>2. Solid, low profile, decorative masonry walls;</li> <li>3. Evergreen shrubbery which, when solely used as screening, shall be continuously maintained to provide solid screening.</li> </ol>	
<b>h. Trash receptacles/enclosures and loading areas shall be screened on all sides, with a gate provided for access, and shall be landscaped.</b>	
<b>i. All landscaping shall be protected with a six inch raised concrete curb.</b>	
<b>j. Landscaped areas and planters shall be serviced by a permanent automatic irrigation system approved by the Development Services Director.</b>	
<b>k. All parking areas, landscaping and screening shall be continuously maintained by the property owner. Landscaping and screening shall be free of weeds, debris, litter, and dead plants. Any dead plant material shall be replaced with similar type of</b>	
<b>l. Parking area and perimeter landscaping shall be installed or secured as required by this article prior to any authorization to occupy any building(s) served by required parking areas.</b>	
<b>m. Landscaping shall be designed to obscure views of loading and other service areas, including trash storage areas, front rights of way and adjoining property containing such loading facilities.</b>	

Table 3.5, Landscape Requirements for Parking Areas

### 3.7 ADDITIONAL LANDSCAPING STANDARDS

#### Parking Area Landscape

- When located adjacent to the freeway frontage, parking should be screened by use of landscaping, berming, low decorative walls or combination of these.
- To the extent feasible, parking lot planting islands and tree placement should be coordinated with freeway edge planting and views into signage and any special architectural detailing.
- Parking lot trees should be provided at a minimum of one tree per 5 spaces. Trees may be clustered to define circulation routes and frame site views. Large scale, high branching shade trees should be used in all parking areas.
- Vegetated bioswales are encouraged in parking lot planting islands to treat on-site stormwater. Pedestrian circulation should be carefully coordinated with bioswales.
- A portion of the required landscaping shall be integrated with the building frontage.
- There shall be a minimum 10 foot wide (inside dimension) landscaped strip along property lines adjacent to the public rights-of-way and private streets. The landscaped strip shall be continuous except at required access to the site or parking area.

**CORDES RANCH**  
**SPECIFIC PLAN**

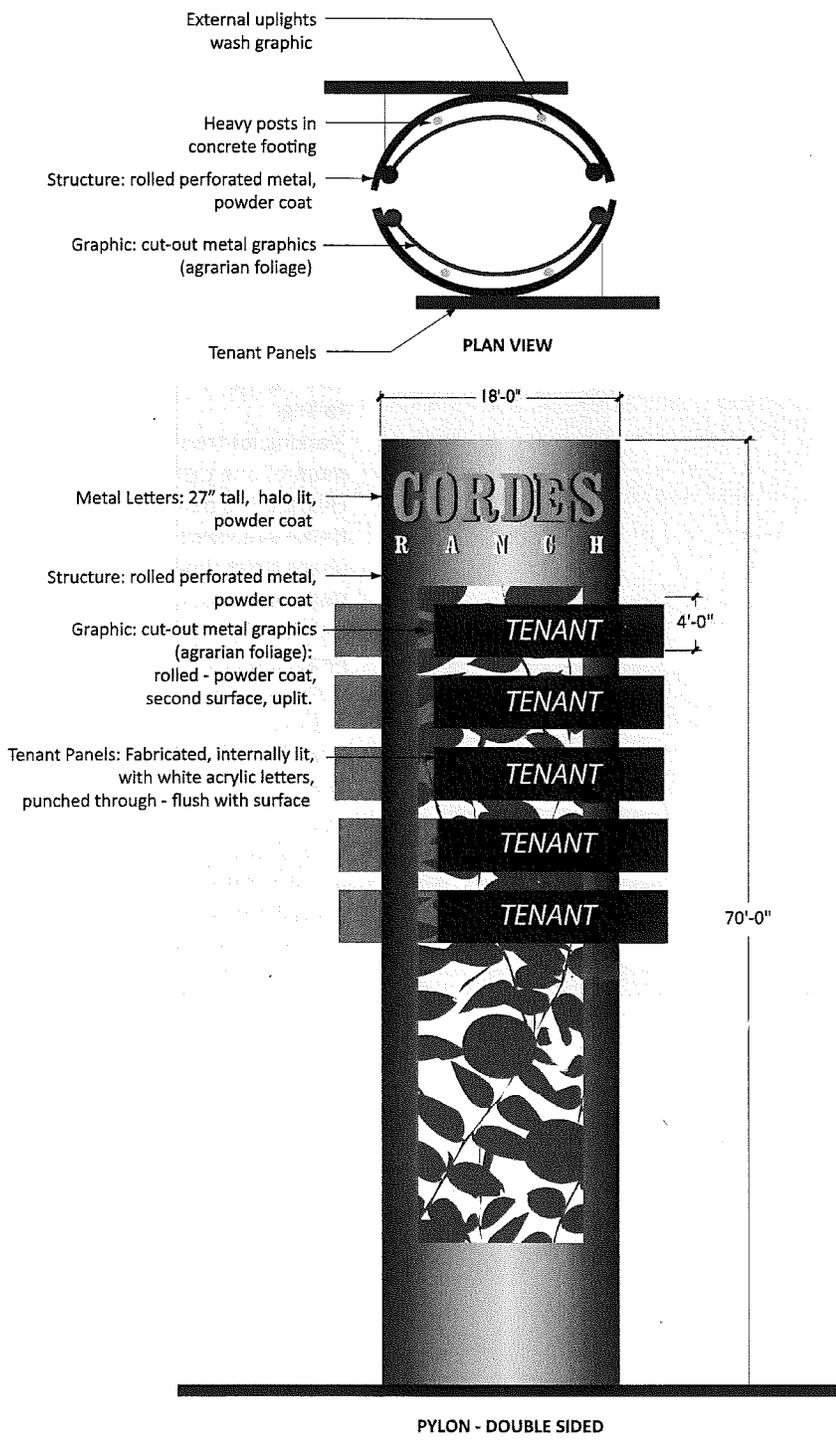


Figure 3.3, Freeway Sign Design

3.8 SIGN STANDARDS

Signs within the Project Area shall be in accordance with Tracy Municipal Code standards, except as modified herein.

- 1. Maximum height: 70'
- 2. Maximum width: 18'
- 3. Maximum area: 300 square feet per sign face
- 4. Minimum ground clearance: 8'
- 5. Maximum number of signs permitted: 1 per parcel.

Freeway Sign

A total of two Freeway signs visible from both eastbound and westbound directions of I-205 are allowed within the Cordes Ranch Specific Plan. They shall be located within the General Commercial parcels adjacent to I-205, and will provide advertising of businesses located within the General Commercial Zone. Figure 3.3 depicts the design and dimensions for the Freeway sign, and Figure 3.4 depicts the approximate location for each.

Freeway Sign Design Standards

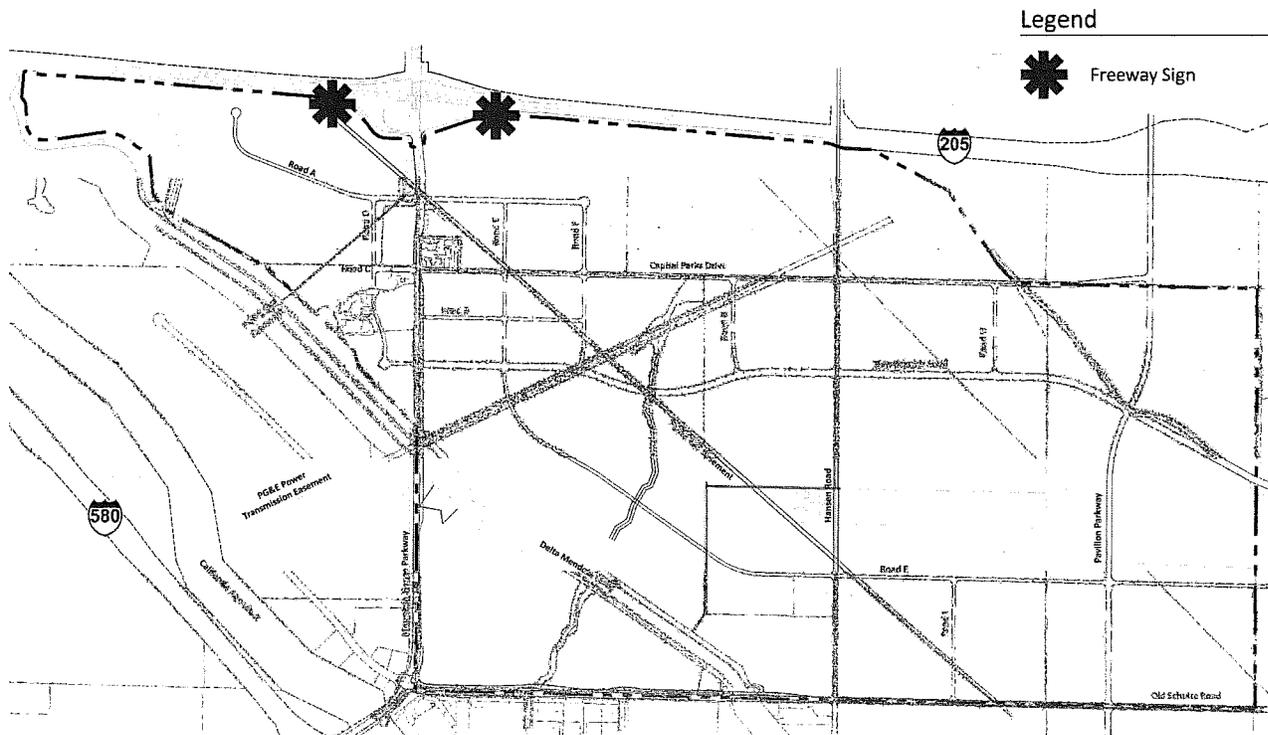


Figure 3.4, Freeway Signage Locations



**CHAPTER 4  
DESIGN GUIDELINES**



General Commercial



General Office



Business Park Industrial

**4.1 INTRODUCTION**

The design guidelines set forth in this chapter serve to steer development of Cordes Ranch by establishing criteria for development character, architecture and detailing, and landscape themes for the General Commercial, General Office, Business Park Industrial, and the I-205 Overlay zones.

The guidelines are to be used in conjunction with the Development Standards in Chapter 3 which provides the standards for setbacks, building height, intensity of development, and the permitted and conditionally permitted uses. Chapter 8 outlines the Development Review process that will utilize these guidelines to evaluate development applications in order to make the necessary findings for project approval.

**Design Goals**

The goal of these design guidelines is to develop General Commercial, General Office and Business Park Industrial facilities that:

- Establish a sense of place for Cordes Ranch through quality architecture and well designed buildings;
- Guide the site planning and building orientation to capitalize on the location and unique opportunities each site presents;
- Establish a consistent landscape theme that provides a gateway to the project and to the City of Tracy, and creates a unifying design element for Cordes Ranch;
- Provide flexibility to allow for a variety of development options and opportunities to generate jobs in the City of Tracy;
- Create a gateway to the City consisting of well designed buildings and enhanced landscaping design along the I-205 freeway edge.

## 4.2 DESIGN ELEMENTS

The Project Area includes a number of design elements that create the frame work for development, See Figure 4.1. These consist of the following:

- a. Project park and open space amenities;
- b. Freeway and road frontage corridors;
- c. City gateway and Project entry features.

### a. Project park and open space amenities

The Project has been organized to capitalize on the large public space Central Green which is “hub” of the project. The existing drainage easement and corridor will be enhanced with trails, landscaping and other amenities to create public open space and gathering places for employees and City of Tracy residents. The road network and bicycle and pedestrian paths have all been designed to lead to the Central Green. To further create connectivity with the Central Green, the street frontages along Mountain House Parkway, Capital Parks Drive, and New Schulte Road include

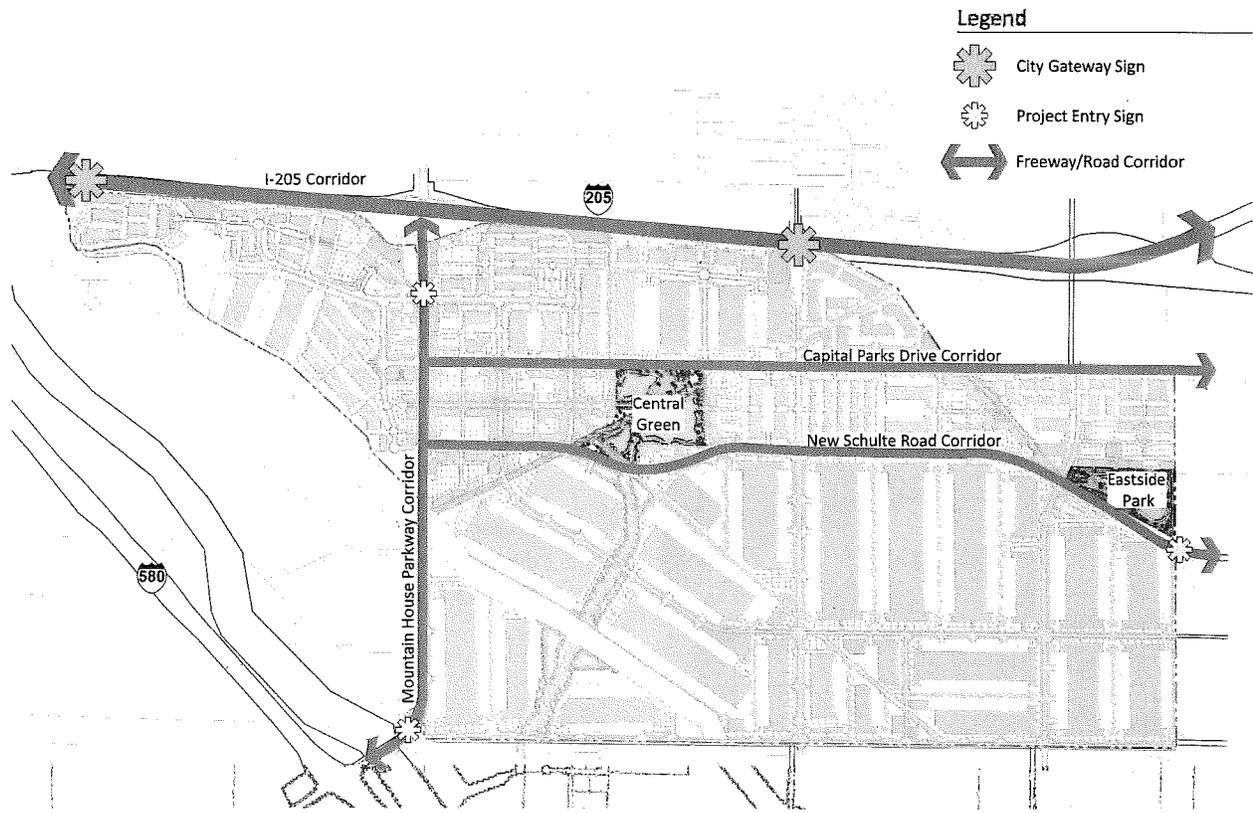


Figure 4.1, Project Design Elements

additional landscaping to create corridors or "spokes" to provide for Class I bike paths and pedestrian sidewalks. The Central Green is connected with a network of roads, bicycle, and pedestrian "spokes" that connect the project and creates a gathering place for employees of the business park.

**b. Freeway and road frontage corridors**

I-205, Mountain House Parkway, Capital Parks Drive, and New Schulte Road are the main points of access to the Project, see Figure 4.1.

Landscaping will be a key element in combination with the building architectural design that will create these design edges. Loading docks and service doors shall be screened from view from these public street corridors with either landscaping, berming, or screen walls or any combination of these methods. Building architecture and orienting the office function to face the street and corners will be important to creating a strong streetscape experience.

**c. City Gateway and Project Entry Features**

To denote the entry to the City, the northwest corner of the project will include a grouping of three gateway signs. The signs have been arranged so that they are visible from both directions and will include lettering and/or imagery to identify the Cordes Ranch project as well as the City of Tracy. A second gateway sign element will be located near the mid portion of the Project, see Figure 4.1.

The signage elements will not only denote the gateway to the City, they will also establish a consistent identity and Project branding for the freeway sign, project entries, intersections, and entry monuments.

Three project entry monuments are included to create a sense of entry to the Project. These include entry monuments at Mountain House Parkway and Road 'A', Mountain House Parkway and Old Schulte Road, and New Schulte Road at the eastern property boundary near the Eastside Park. The entry monuments will include 20' high c-shaped metal panel identity monuments corten and corrugated metal walls or similar materials.



**Orient building entries towards public streets**



**Buildings should frame and front streets**



**Cluster buildings to create courtyards and plazas**

### **4.3 GENERAL DESIGN GUIDELINES**

The following design guidelines will support the implementation of the design elements described in Section 4.1 and are applicable to all Zone Districts.

#### **Site Design**

##### **a. Site Planning and Building Orientation**

- Buildings at corners and vehicle entries should frame the street and provide pedestrian connections between the street and the buildings.
- Buildings should be oriented to include adequate setbacks to create public spaces.
- Main vehicle access drives shall be oriented to terminate at the building entrances to provide visitors with a clear pathway to entries.
- Establish visual links in multi-building complexes by using landscaping and other site design elements that allow pedestrians to easily navigate within a complex of buildings.
- Site planning and parking lot design should consider travel speeds and view corridors from the freeway to businesses, associated signage and special architectural features.
- Landscaping at site entries should support the character of the project and provide a sense of arrival. A variety of elements can be used to enhance entries, such as monoliths, low ornamental walls or fences, accent planting, and special paving.
- Signage and landscape treatment should distinguish the entries that serve the main building from service entries. Service vehicle traffic should be separated from employee and visitor circulation. A clear travel route should be provided between the street and the building or complex entry.
- Provide for efficient site circulation by creating landscaped drive aisles that divide parking fields and direct vehicles to parking adjacent to buildings.
- Provide adequate stacking length at main entries and the first drive aisle to limit vehicle ingress and egress conflicts.
- Buildings should be setback from the property line to allow for employee and customer parking adjacent to the building.
- The office portions of buildings should be oriented to the main public street or located at the building corner.



Setback building from I-205 to create appropriate massing



Create landscaped drive isles to direct vehicles and pedestrians



Design buildings with offsets and recesses

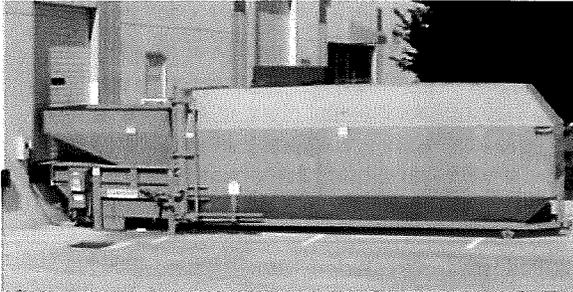
- Provide for vehicle circulation and parking in front of buildings that will assist with creating appropriate building massing at public streets. To achieve this, buildings that parallel the public streets shall be set back a minimum of 75' to the face of the building.

**b. Pedestrian Circulation**

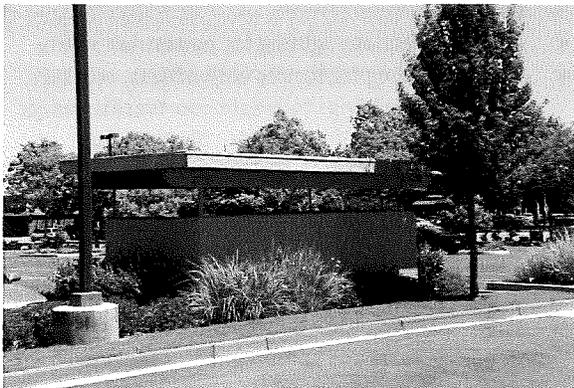
- Provide clear, convenient pedestrian connections from the public streets, sidewalks, transit stops and trails to business entries.
- Distinguish pedestrian pathways from vehicular drives through the use of differing paving texture, color and/or materials. Where pedestrian pathways cross vehicular drives, provide clearly delineated crosswalks and consider raising the pedestrian paving surface for more visual differentiation.
- Provide adequate lighting for pedestrian safety.
- Design building footprints with offsets, recesses, and orient buildings to create courtyards, and/or plazas to provide for a variety of gathering places.

**c. Screening and Utilities**

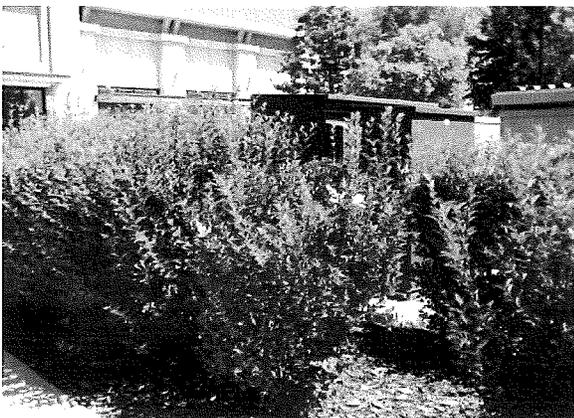
- Loading docks, truck trailer parking and service doors shall be allowed to face public streets, but screened with either landscaping, berming, or screen walls or any combination of these methods.
- Include ample landscaping to screen views of the truck trailer parking, service doors, and loading docks from public streets.
- Parcels with more than one building should cluster buildings so that service doors and loading docks oppose each other to screen views from public streets.
- Loading docks and service doors shall not be visible from I-205.
- Incorporate storm water treatment improvements into the overall site design and parking lot layout of each parcel. Storm water control shall be designed in accordance with adopted standards.
- Outside storage will be permitted only if completely screened from public view. Utilize screen walls, fences, landscaping, and berming or any combination of these methods to provide proper screening.



Trash enclosure near loading dock.



Design trash enclosures to be compatible with Project architecture



Exterior utility equipment screened with planting

- For uses such as auto, RV or boat repair or storage, as well as for uses involving outdoor parking of industrial vehicles such as fork lifts or construction equipment, well screened rear parking is required. The areas should be screened with a solid wall or fenced compatible with the building architecture and landscape. Chain link fencing is not permitted where visible by the public for such particular uses.
- Site planning shall anticipate the location of any above-ground utilities including, but not limited to, PG&E transformers, phone company boxes, fire department connections, backflow preventers, irrigation controllers and other on-site utilities, which shall be screened from view from any public right-of-way behind landscaping, structures, walls or fences that are designed to be compatible with the buildings and landscape/hardscape features on the site.
- Trash enclosures shall be designed with solid doors, interior concrete curbs, and exterior materials and colors compatible with the adjacent building exteriors on a site. All trash enclosures shall be sized to fit both trash and recycling containers that will be necessary to serve the users of the site.
- Enclosed metal trash compactors adjacent to the loading docks are permitted and will be screened from public view as part of the truck court/trailer storage screening.
- Trash enclosures shall be screened from view from all public rights-of-way (including I-205) by buildings or landscaping, with openings oriented away from public view, and shall be located in a manner that allows for accessibility of the trash/recycling vehicles.

#### d. Parking and Circulation

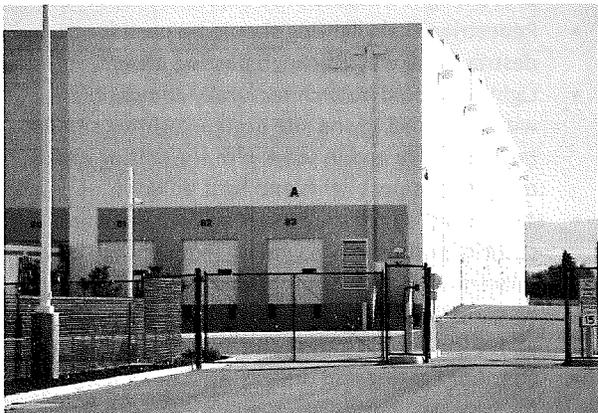
- Create a clear visual entry to the project by use of signage, entry walls, vertical landscape elements, and accent hardscape/paving.
- Parking, when located adjacent to frontage streets, shall incorporate landscaping to screen the parking areas from the public view.
- Large parking areas should include landscaped drive aisles that divide parking fields to provide clear circulation to parking adjacent to buildings.

#### e. Parking Lots

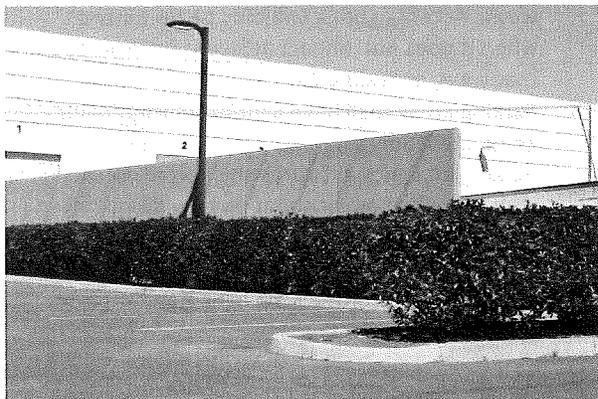
- Tree planting in parking areas should create an "orchard" effect, shading and softening the appearance of the parking lot. At least 40% of the paved area shall be shaded within five years.



Include landscaped planters to divide large parking areas



Provide separate truck entrances when possible



Use of wing walls and landscaping to conceal loading docks, and service doors

- If possible, provide separate entrances for automobiles and trucks clearly marked to promote safe site circulation. In many cases, there will be shared vehicle access.
- Where landscape planters are parallel and adjacent to vehicular parking spaces in customer parking lots, the planter areas shall incorporate a 12-inch wide concrete curb along their perimeter that is adjacent to the parking space in order to allow access to vehicles without stepping into landscape planters.

**f. Walls and Fences**

- Landscape walls and fences should be of high quality materials compatible with the architecture and landscape design.
- In addition to landscaping and berming, walls and fences can be used to screen the entries to the service and loading dock function of the buildings.
- Walls and fences should be designed and constructed of materials similar to and compatible with the overall design character and style of the development.
- Permitted materials include pre-cast concrete walls, split-face masonry, stone veneer, brick, and tubular steel. Material selection will be at the choice of the owner/developer.
- Security gates should be constructed of the same materials and detailing as the fencing for the project.
- Fencing shall be limited to a maximum height of 12'. When security fencing is required adjacent to the landscape setback area, it should be constructed of tubular steel or similar material.
- Gates for pedestrian and vehicular access to restricted areas that are visible from public areas (i.e., parking lots, drive aisles) shall be constructed of solid durable material, tubular steel, or similar material.
- Chain-link is only permitted on the side or rear project boundary when not visible from public view. Barbed wire, razor wire, integrated corrugated metal, electronically charged or plain exposed plastic concrete/PCC fences are not permitted.



Gates visible from public areas are to be constructed of tubular steel or similar material



Typical parking lot lighting



Provide pedestrian scale lighting along walkways

- Site security may sometimes call for walls and/or fences, which may be comprised of a variety of different materials, including but not limited to tube steel, masonry, or any combinations thereof. The use of chain link fencing is not preferred, but is allowable if it is designed in conjunction with the overall site and landscape plan.

**g. Lighting**

- High efficiency lighting and lighting control systems are encouraged and light should not trespass onto adjacent properties.
- Site lighting should be attractive and consistent with the overall character of the project.
- Site lighting should highlight building entries, open spaces, walkways, and architectural features.
- Site lighting should be dark-sky compliant and prevent light trespass onto adjacent properties.
- Pedestrian scale lighting should be used for pedestrian walkways through parking areas.
- Lighting should be architecturally compatible with the building and site design. Lighting should be low profile and in scale with the setting and may include post lights and light bollards.
- All projects shall include lighting for safety and security purposes. All lighting fixtures shall be fully shielded with cut-off fixtures so that there is no glare emitted onto adjacent properties or above the lowest part of the fixture.
- Outdoor lighting and other means of illumination for signs, structures, landscaping, and similar areas, shall be made of durable materials.
- Accent lighting shall be used to enhance the appearance of a structure, draw attention to points of interest, and define open spaces and pathways. Accent lighting will only be permitted when it does not impact adjacent development, roadways, or residences.
- Parking areas shall have lighting which provides adequate illumination for safety and security. Parking lot lighting fixtures shall avoid conflict with tree planting locations so they do not displace intended tree plantings.
- Pole footings in traffic areas shall be designed and installed to protect the light standard from potential vehicular damage.

MARCH, 2013



Accent bollard lighting



Light fixture bases should be protected



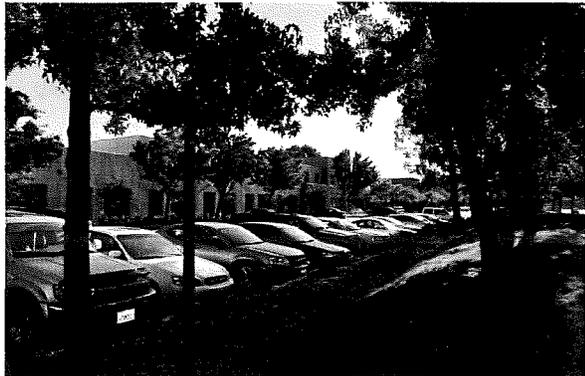
Contemporary Agrarian Landscape

#### 4.4 ON-SITE LANDSCAPE GUIDELINES

Landscape design plays an important role in creating a uniquely attractive, sustainable and health-promoting environment for Cordes Ranch. The character is contemporary agrarian, which is a uniquely California aesthetic. Native and climate adapted plantings in rural patterns such as orchards and hedgerows create a rustic, yet visually ordered environment. Natural materials in clean, simple designs create a sophisticated character. The project is visually unified with thematic signage, coordinated furnishings and fixtures, enhanced hardscape and plant palette, which all work together to create a sense of "place".

The Cordes Ranch Landscape Guidelines are intended to provide a framework for achieving the high quality landscape character envisioned for the project. The guidelines are not intended to limit innovation, but rather to provide clear direction on design elements that are key to achieving the desired character. The detailed design criteria provided here will support planners, architects and landscape architects in meeting the intent of the Specific Plan. In the case of conflict between the provisions of this Specific Plan and City of Tracy standards, the provisions herein shall take precedence.

- Vehicle parking when fronting I-205 shall be screened by landscaping and berming.
  - Fast-growing trees closely spaced in groupings to create visual mass are encouraged.
  - Planting areas should be provided between parking and roads to provide visual relief in large expanses of hardscape.
  - Screening and sound attenuation along roads should be achieved through siting, berming and landscaping.
  - Property owners are responsible for installing and maintaining the landscape setbacks within their properties, in accordance with the applicable CC&Rs. Setback planting concepts are shown in streetscape descriptions in Chapter 5.
- a. Non-HOA Site Landscaping
- Design should be generally consistent with the overall contemporary agrarian character of the project.



Screen parking with landscaping and berming



Encourage creative, innovative landscape designs



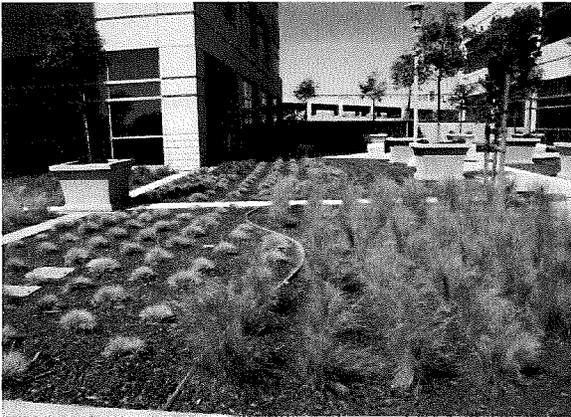
Stormwater management as part of landscape

- Sophisticated designs with simple plant palettes, such as rows and masses of native and climate adapted grasses and orchard style tree plantings are encouraged. There should be a consistency of landscape design throughout a development. Unrelated random placement of plant materials should be avoided.
- Sites should be landscaped to the maximum extent feasible in order to optimize the aesthetic appeal and comfort for employees and visitors. All portions of a site not devoted to buildings, structures, parking, outdoor storage or paving should be landscaped, to the extent feasible. Landscapes should be designed to reach a reasonable level of maturity within five years.
- Large scale buildings should be screened by large scale planting.
- Trees shall be provided at a ratio of an average of at least one tree for every 1,000 square feet of landscape/hardscape area, not including required parking lot trees.
- Trees shall be installed at a minimum size of 24" box.
- Parking lot trees should be provided at a minimum of one tree per 5 spaces. Trees may be clustered to define circulation routes, frame site views, and reinforce freeway edge planting. Large scale, high branching shade trees should be used in all parking areas.
- Vegetated bioswales are encouraged in parking lot planting islands to treat on-site stormwater and provide visual relief within the hardscape.

**b. Materials**

- Natural materials, including stone, metals and wood in keeping with the general character of the project are preferred.
- Refer to the Plant Palette provided in this section for suggested plant materials.
- Locally sourced, salvaged and recycled content materials in the landscape, including recycled content concrete are encouraged.
- The use of renewable energy in the landscape such as photovoltaics and wind turbines is encouraged.
- The use of native, climate adapted and large stature species is encouraged to promote/create habitat, minimize use of water, fertilizers and pesticides, promote biodiversity and sequester carbon.

**MARCH, 2013**



**Native/climate adapted plants in simple designs**



**Outdoor space separated from parking with planters**



**Turf minimized in the landscape**

- Species listed on the CAL-IPC list of invasive species shall not be used in the landscape.
- Turf should be minimized in the landscape, except where needed for recreational purposes. The use of turf for solely decorative purposes is strongly discouraged.
- Stormwater Best Management Practices, such as rain gardens, bioswales and rainwater harvesting, should be incorporated into the landscape to maximize on-site infiltration of stormwater, to the extent possible.

**c. Sustainability**

- Sustainable landscape design employing the most current technologies are strongly encouraged.
- High-efficiency, weather based irrigation systems should be used.
- Recycled water/greywater should be used for landscape irrigation wherever feasible.
- Appropriate placement of landscape materials should provide summer shade on buildings, parking spaces, drives and paths.
- Enhanced building entries and other special landscape features are encouraged and should feature bold foliage accent planting in pots or planters, colored paving, spreading shade trees and seating elements. Accent lighting is also encouraged.
- Large scale trees and shrubs appropriate to the scale of the architecture should be emphasized to minimize visual dominance of large architecture.

**d. Site Furnishings**

- Site furnishings should be high quality and contemporary in design and compatible with the overall landscape design.
- Site Furnishings should be durable and vandal resistant.

**Suggested Plant Palette**

The following plant list provides suggested species suitable for the design aesthetic desired for the project.

Botanical Name	Common Name
<i>Acer rubrum</i> 'Red 'Sunset'	Red Sunset Maple
<i>Celtis sinensis</i>	Japanese Hackberry
<i>Cercis Canadensis</i> 'Forest Pansy'	
<i>Cercis occidentalis</i>	Western Redbud
<i>Crataegus cordata</i>	Washington Hawthorne
<i>Crataegus oxycantha</i>	Hawthorn
<i>Cupressus sempervirens</i>	Italian Cyprus
<i>Fraxinus hololricha</i> 'Moraine'	Moraine Ash
<i>Fraxinus velutina</i> 'Rio Grande'	Rio Grande Velvet Ash
<i>Fraxinus uhdei</i>	Evergreen Ash
<i>Lagerstoemia indica</i>	Crape myrtle
<i>Liriodendron tulipifera</i>	Tuliptree
<i>Nyssa sylvatica</i>	Saucer Magnolia
<i>Pistacia chinensis</i> (Male only)	Chinese Pistache
<i>Platanus acerifolia</i> 'Yarwood'	London Planetree
<i>Prunus cerasifera</i> 'krauter Vesuvius'	Krauter Vesuvius Flowering Plum
<i>Pyrus calleryana</i> 'Aristocrat', 'Capital', 'Red Spire', 'Whitehouse'	Flowering Pear, Callery Pear, Capital, Red Spire, Whitehouse Callery Pear
<i>Pyrus calleryana</i> 'New Bradford'	New Bradford Pear
<i>Pyrus calleryana</i> 'Cleveland Select'	Cleveland Flowering Pear
<i>Quercus agrifolia</i>	Coast Live Oak
<i>Quercus cocchineia</i>	Scarlet Oak
<i>Quercus lobata</i>	Valley Oak, White Oak
<i>Quercus rubra</i>	Red Oak

Botanical Name	Common Name
<i>Quercus suber</i>	Cork Oak
<i>Quercus virginiana</i>	Southern Live Oak
<i>Schinus molle</i>	California Pepper Tree
<i>Zelkova serrata</i> 'Green Vase' or 'Village Green'	Japanese Zelkova

**4.5 GENERAL COMMERCIAL GUIDELINES**

General Commercial development will include approximately 20 -acres of retail and highway commercial serving services and uses. Site planning should orient buildings to face the primary highway/street frontage and/or entry drives to maximize exposure for businesses. Buildings should be clustered to create plazas, and framed spaces for seating, fountains and other design amenities. Parking should be located behind buildings and/or screened with landscaping and berming. Parking layout should be oriented perpendicular to the buildings to provide for easy pedestrian access to the buildings. In large retail centers of over 100,000 sf, a pedestrian paseo should be incorporated into the parking field to provide a linkage and clear pathway for safe pedestrian access between buildings. A typical illustrative site plan is presented in Figure 4.2.

Small commercial developments will include a mix of retail commercial uses, business and professional services. Site planning should orient buildings to face the primary highway/street frontage, and/or entry drives to maximize exposure for businesses. Buildings should frame the street and be sited at the minimum setback or have only a single row of parking between the building and street. Buildings should be clustered to create plazas, and framed spaces for seating, fountains and other design amenities. A typical illustrative plan is presented in Figure 4.3.

**a. General Commercial Guidelines**

- Building facades can be oriented to face either the freeway frontage or the main public street so that businesses and commercial uses are highly visible.
- Vehicle parking when fronting I-205 shall be screened by landscaping and berming.
- Commercial and Office Buildings along the freeway can be setback at the minimum 30' landscape setback.
- Design building footprints with offsets, recesses, and orient buildings to create courtyards, and/or plazas to provide for a variety of gathering places.

- Trash enclosures shall be completely screened from I-205 and public streets and located to allow for collection vehicle turning and access.
- Site planning shall anticipate the location of above ground utilities and backflow preventers. Utilities and backflow preventers shall be screened from public view when feasible. Use landscaping or "green screen" walls to reduce the visibility of utilities and other infrastructure that require location above ground.
- Incorporate storm water treatment improvements into the overall site design and parking lot layout of each parcel. Storm water control shall be designed in accordance with adopted standards.



Design buildings with recesses and outdoor spaces



Incorporate stormwater treatment within landscape areas

**CORDES RANCH**  
**SPECIFIC PLAN**

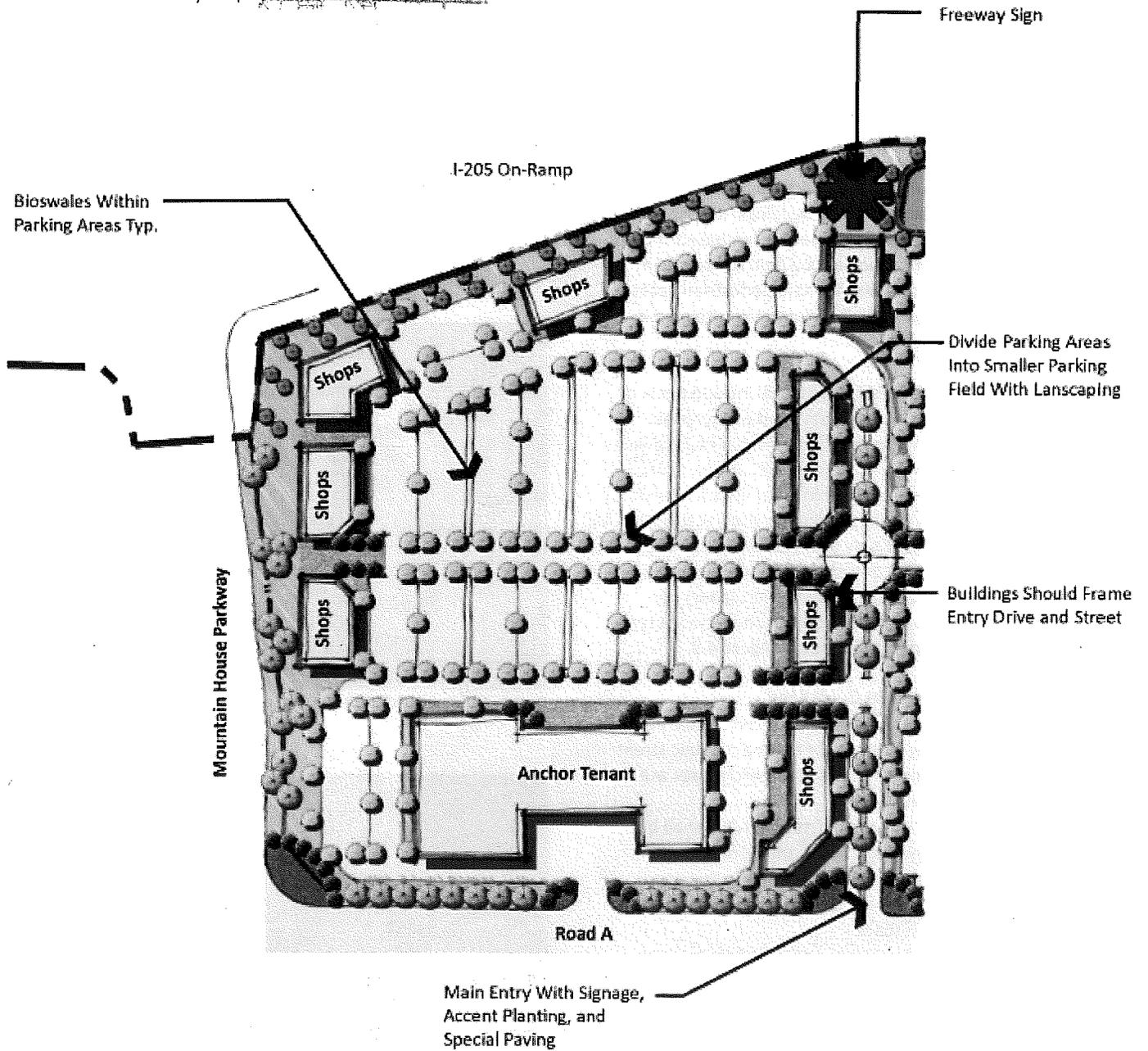
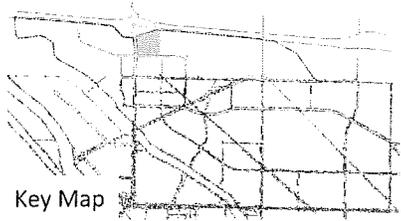


Figure 4.2, Conceptual Large Commercial Illustrative Plan

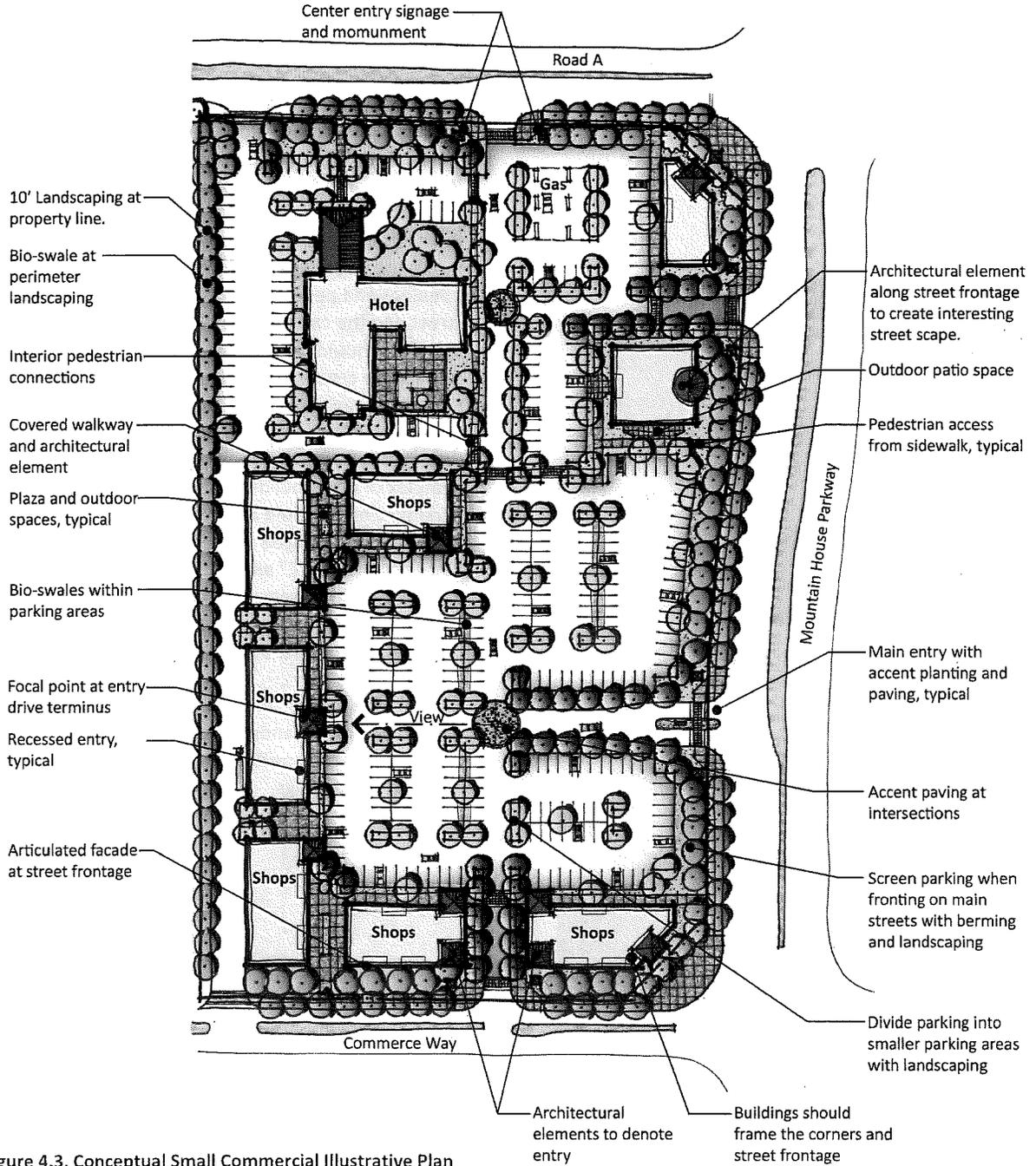
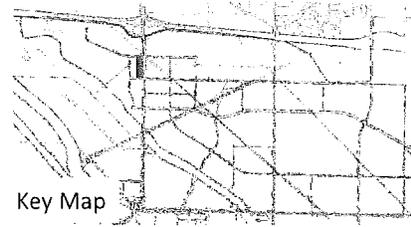


Figure 4.3, Conceptual Small Commercial Illustrative Plan



Landscape parking adjacent to public streets



Include public spaces and plazas in the site design



Orient building facades to face I-205 and public streets

#### 4.6 GENERAL OFFICE GUIDELINES

General Office development to the west of the Central Green will consist of shorter street block lengths to create a more pedestrian friendly district. Buildings will be allowed to be multiple stories in height and will frame the streets and corners. Diagonal on-street parking will provide direct access to businesses and services with additional parking encouraged to be located behind buildings and screened with landscaping and berming. A typical illustrative site plan is presented in Figure 4.4.

- Parcels with frontage along Mountain House Parkway, Capital Parks Drive, New Schulte Road, and Roads B, E, and F, should orient buildings to the street.
- Buildings at corners and vehicle entries should frame the street and include plazas, or gateway openings and pedestrian connections between the street and the campus of buildings.
- Buildings should be oriented to include adequate setbacks to create public spaces and plazas.
- Establish visual links in multi-building complexes by using landscaping and other site design elements that allow pedestrians to easily navigate within a complex of office buildings.
- Parking, when located adjacent to frontage streets, should be discouraged in the General Office area along streets "B" "E" and "F". When infeasible, parking should be screened by use of either landscaping, berming, or low walls or any combination of methods from the public view and pedestrian circulation.
- Large parking areas within General Office should include dedicated landscaped drive aisles that divide parking fields to provide clear circulation to parking adjacent to buildings.

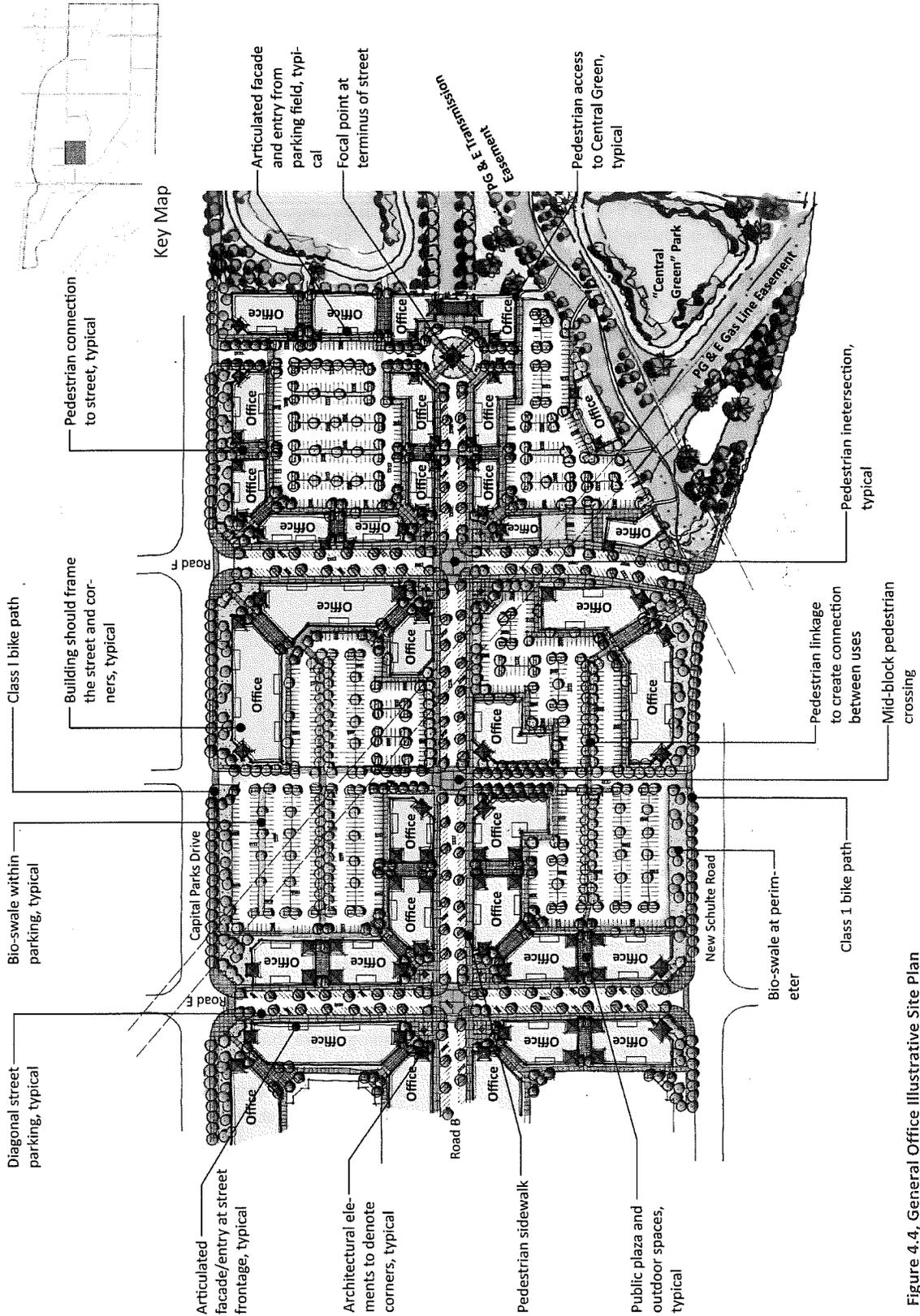


Figure 4.4, General Office Illustrative Site Plan

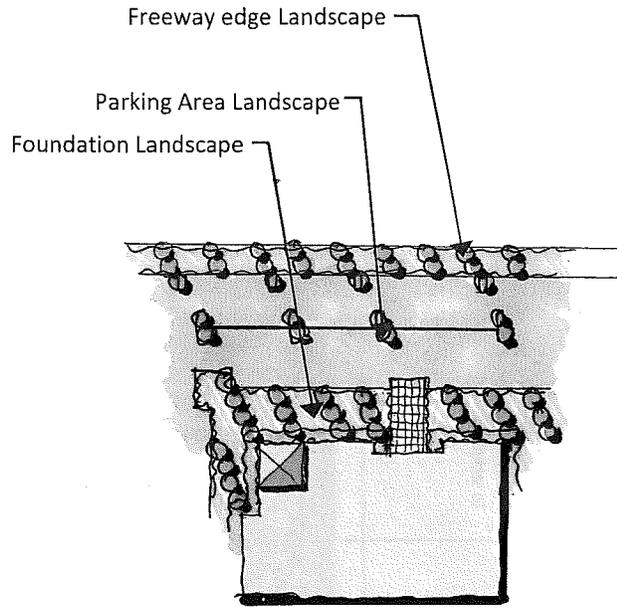
**4.7 I-205 OVERLAY ZONE GUIDELINES**

The I-205 Overlay Zone is the “front door” to the Project and the City. The freeway edge provides opportunities for highly visible freeway development. The vision is to create a strong thematic entry to the City, create a gateway to the project from I-205 at Mountain House Parkway, and create a development fabric of well designed buildings that are oriented to the freeway that will establish a visually interesting building edge.

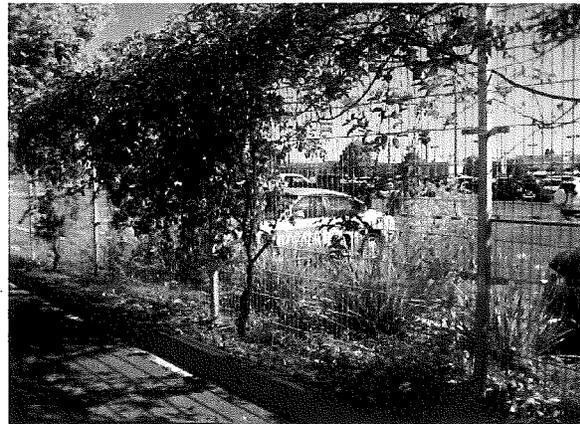
The overlay zone includes the first 500 feet from the property line, adjacent to I-205 see Figures 4.4 and 4.5. The Overlay Zone will guide the orientation of buildings, the design and detailing of building architecture, and establishes the landscape character of the freeway frontage.

The following guidelines have been established to guide development of parcels within the I-205 Overlay Zone.

- Loading docks and service doors are not allowed to face I-205.
- Development with more than one building should orient buildings so that loading docks and service doors oppose each other and face the interior to screen views from I-205.
- Parking and/or frontage/access roads shall be located adjacent to the freeway to create a minimum 100’ building setback from the property line at I-205 to assist in reducing the visual massing of buildings.
- Site planning shall provide for two “tiers” of landscaping adjacent to I-205:
  1. A 30’ minimum landscape area from the property boundary paralleling I-205.
  2. Landscaping within the parking field shall be required to meet the minimum parking shading requirements for the City of Tracy;
- Parking when located adjacent to the freeway frontage should be screened by use of landscaping, low berming, and low walls or a combination of all.
- Landscaping of the 30’ minimum area parallel to I-205 shall adhere to the concept plan in Chapter 5 and provide for and include view corridors to increase the visibility of businesses and associated signage.



Provide tiers of landscaping along the I-205 frontage



Screen walls used to conceal parking, loading docks, and service doors

- Screen views of interior facing service doors and loading docks that may be visible from the freeway and public streets with landscaping, berming, screens walls, or any combination of all.
- Screening walls shall be utilized to obscure views of interior services doors and loading docks. Walls should be designed and constructed of the same or complimentary materials as primary buildings.
- Building architecture should include additional articulation of roof/parapet and wall design.

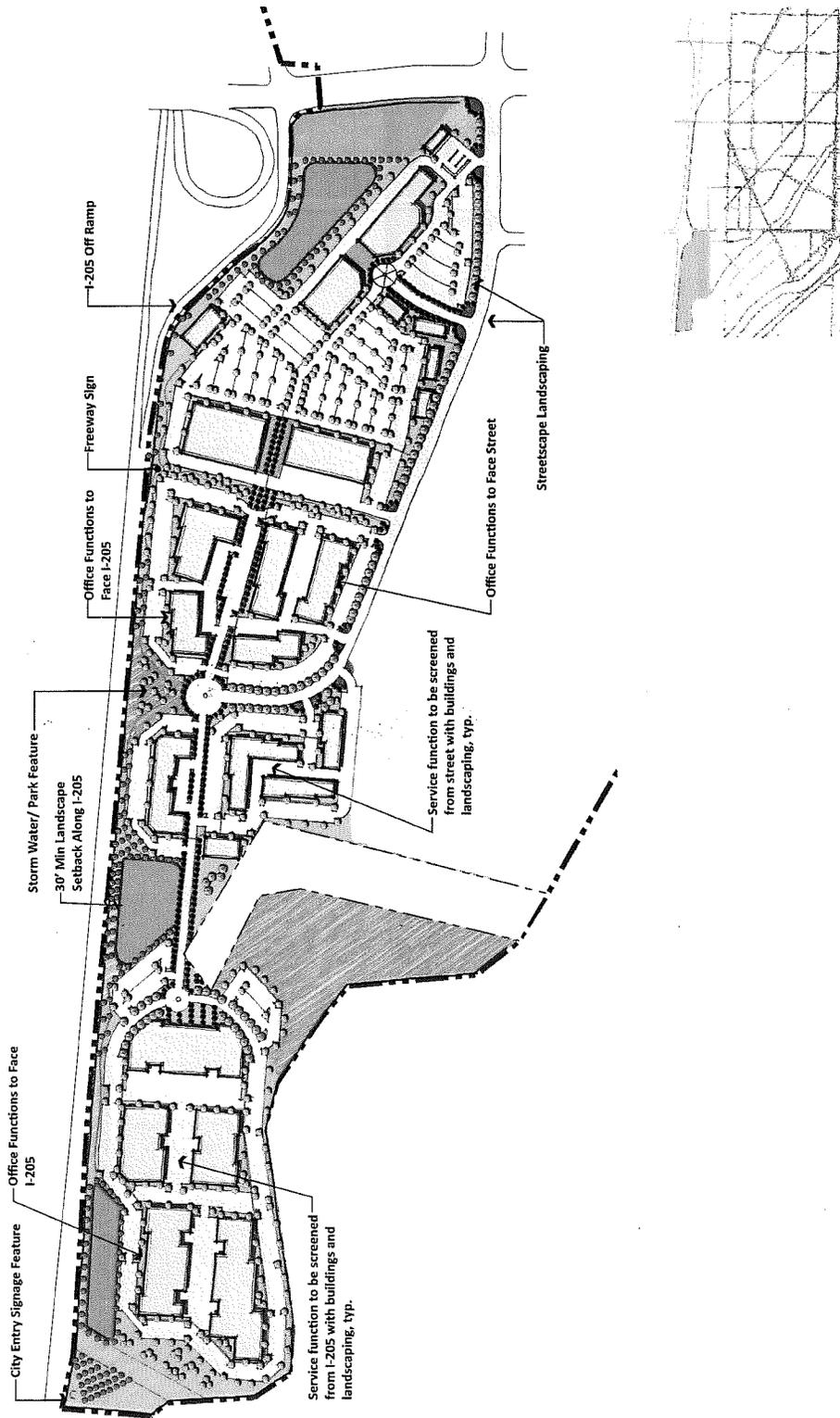


Figure 4.4, I-205 Overlay Zone Illustrative Plan - West of Mountain House Parkway

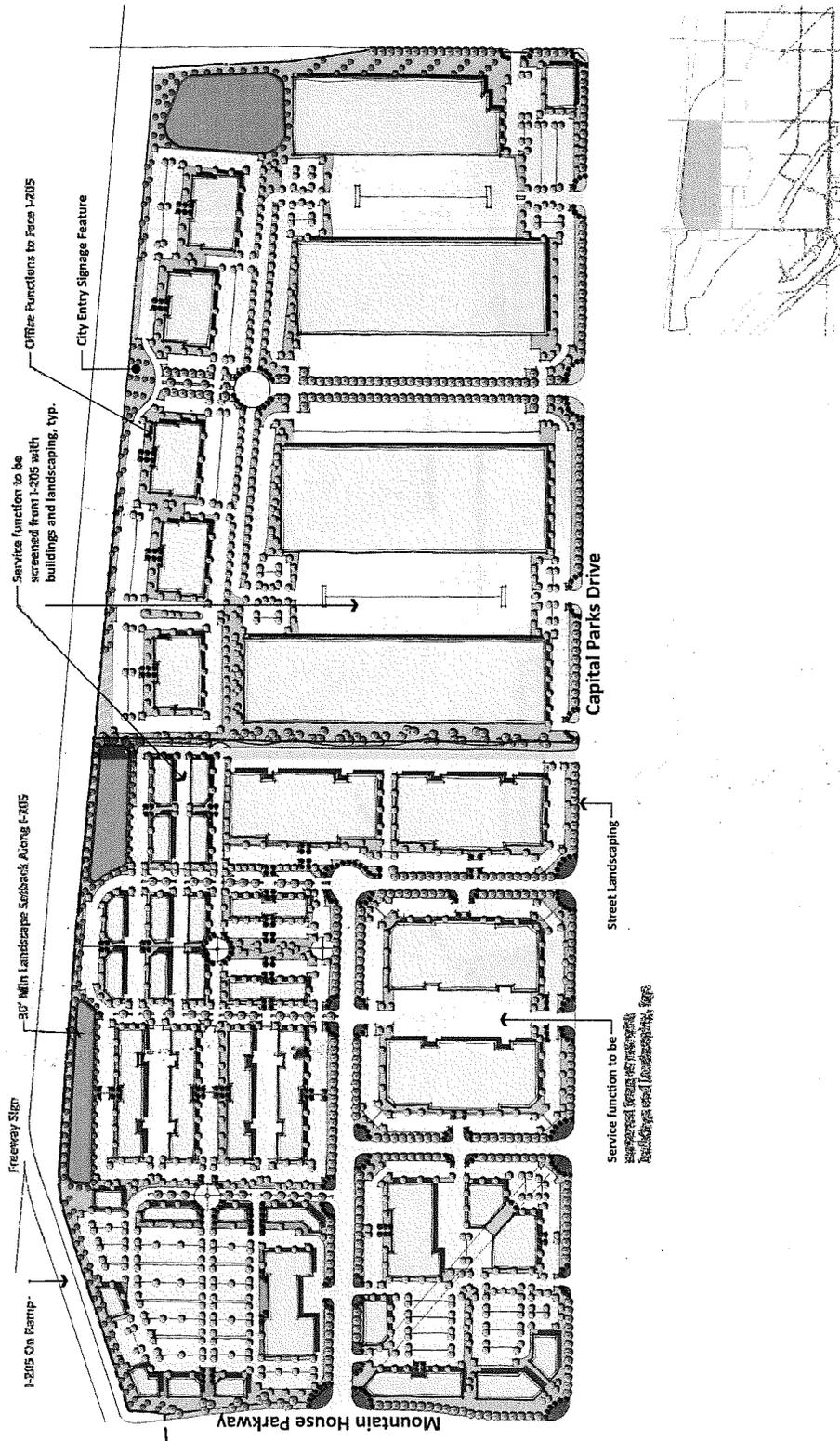
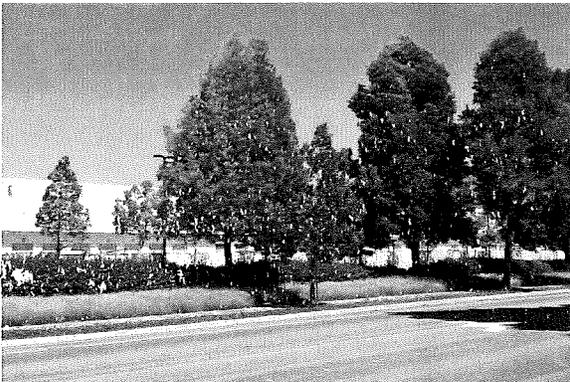


Figure 4.5, I-205 Overlay Zone Illustrative Plan - East of Mountain House Parkway

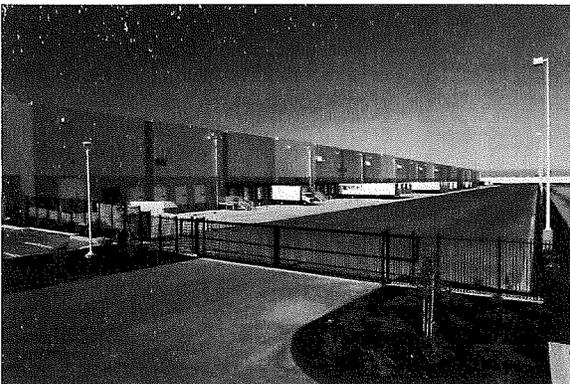
MARCH, 2013



**Orient office functions to buildings to face public streets**



**Screen loading docks with landscaping and/or screen walls**



**Provide separate entrances for trucks**

#### **4.8 BPI DESIGN GUIDELINES**

Business Park Industrial facilities will generally consist of large parcels that will allow for large buildings, many over 500,000 square feet. Buildings should be designed to face office functions and building entries to the street and provide screening of truck and trailer parking, loading docks, and service doors with either landscaping, berming or screen walls or any combination of these methods. Parking should also be screened with landscaping and berming and include trees to provide shading to reduce heat gain. A typical illustrative concept site plan is presented in Figure 4.6.

- Buildings should be setback from the property line to allow for employee and customer parking adjacent to the building.
- Buildings with an office function should be oriented to the main public street or located at the building corner.
- Parcels with more than one building should cluster buildings so that service doors and loading docks oppose each other to screen views from public streets.
- Include ample landscaping to screen views of the loading docks, truck trailer parking, and service doors from public streets.
- Parking, when in front of buildings, will be screened by use of landscaping or berming from the public view.
- If possible, provide separate entrances for automobiles and trucks clearly marked to promote safe site circulation. In many cases there will be shared vehicle access.
- Parking areas for trucks and trailers shall be allowed to face public streets, but should be screened from public view. Utilize screen walls, fencing, landscaping, and berming or any combination of these methods to provide proper screening.
- Allow for adequate truck stacking length at the security building and the street entry to limit conflicts with site circulation.

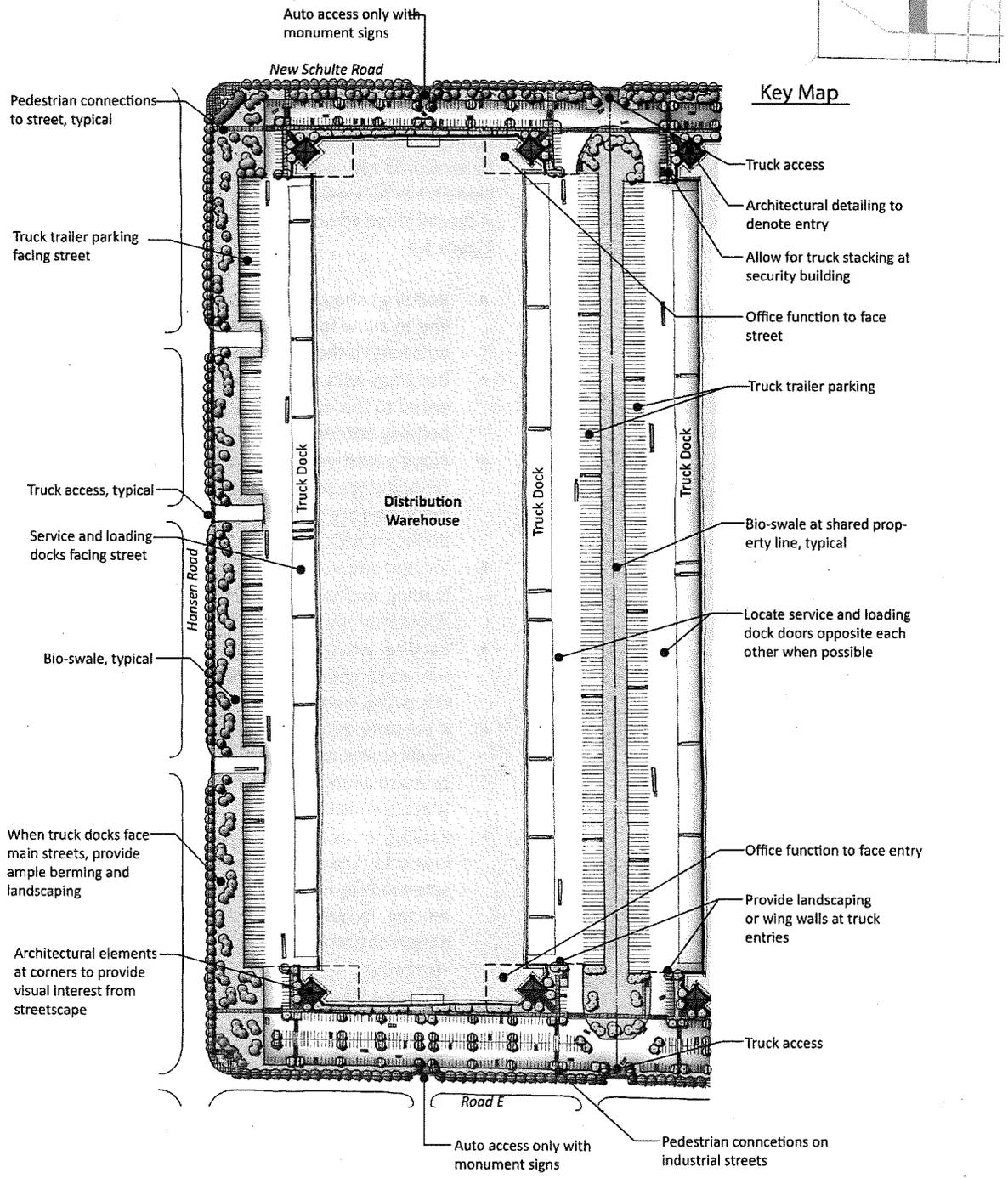


Figure 4.6, Business Park Industrial Illustrative Plan

MARCH, 2013



Use a variety of materials in the building design



Use vertical and horizontal design elements to create façade breaks



Use simple shapes and forms to create visual interest

#### 4.9 ARCHITECTURAL GUIDELINES-ALL ZONING DISTRICTS

These architectural design guidelines are intended to provide direction for the development of well-designed structures through the use of high-quality materials and attention to detail that will meet or exceed the high standards envisioned through this Specific Plan. These guidelines will assist in ensuring a base level of quality of architecture consistent with the vision and goals of the Specific Plan, rather than relying on standardized market prototypes to drive the design of the various building types.

- Building base materials may consist of, but not be limited to, wood, stucco, stone, brick, concrete or slump block, and concrete tilt-up panels. Accent materials may consist of, but not be limited to, tile, glass, stone, brick, wood, stucco and metal. All buildings should utilize a variety of colors and materials.
- Buildings with primarily metal exteriors are not permitted unless an exception is made based on meritorious design.
- Visual interest on buildings with simple shapes shall be provided through the use of both vertical and horizontal façade breaks that should be visible from street view, including, but not limited to, varying roof heights and pitches, stepped out columns, awnings, windows, recessed entries, score lines, and a mix of colors and materials.
- All separate structures on a site shall have consistent architectural detail and design elements to create a visually cohesive development. It is not necessary or even desired for buildings to “match”, but they should utilize similar architectural elements, colors and materials, or styles so that there is not an aesthetic disconnect between buildings on a site.
- Utilitarian portions of buildings, such as vents, gutters, downspouts, flashing, electrical conduit, and other wall-mounted utilities shall be painted to match the color of the adjacent surface or otherwise designed in harmony with the building exterior.
- All buildings shall be designed to completely screen any roof-mounted equipment, including, but not limited to, HVAC units, vents, fans, antennas, sky lights and dishes from view of all public rights-of-way.



Simple architectural forms with clean lines



Variety of materials applied to the base, wall, and cap



Utilize warm earth and neutral color palettes

#### **4.10 GENERAL COMMERCIAL ARCHITECTURAL GUIDELINES**

The General Commercial architectural design guidelines are intended to provide direction for the development of buildings that will house commercial retail and consumer service land uses. These buildings should be designed with elements that consider the human scale in order to promote the comfort of the customers by providing protection from the elements through awnings, covered walkways, and other pedestrian-friendly elements. Often times, all sides of commercial buildings will be visible to the public and should be designed in a manner where they are welcoming to customers from the street as well as the parking lot and service areas.

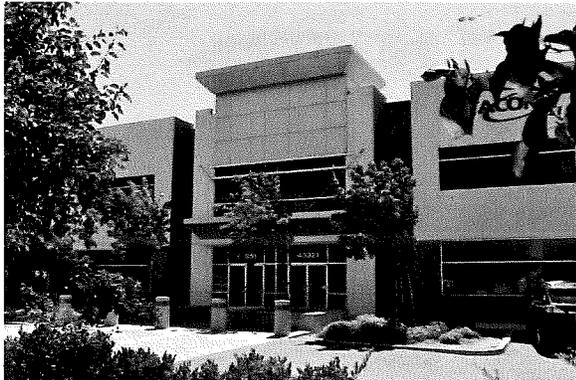
- Elements that promote pedestrian activity such as awnings, covered arcades, windows, and hardscape features (benches, stepping stones, etc.) shall be incorporated into the design of commercial buildings.
- All publicly visible sides of commercial buildings shall be designed with a complementary level of detailing and quality of materials so that there is equal visual interest on all sides. This may include, but not be limited to, the use of spandrel glazing, awnings, trims, covered doorways, accent colors and accent materials. Multiple building entries are encouraged when feasible.

##### **a. Architectural Styles**

The General Commercial retail images are intended to guide the style of the architecture and detailing for commercial retail development. see Figure 4.7.



Figure 4.7, Typical General Commercial Architecture



Include architectural details at entries



Use simple building forms and massing to unite building features



Clean architectural lines with simple details

#### 4.11 GENERAL OFFICE ARCHITECTURAL GUIDELINES

The General Office design guidelines are intended to ensure high-quality office buildings with design details that set them apart from buildings in the Business Park Industrial Areas. Offices may be single or multi-story, and may stand alone or be grouped in a campus-style design.

- Colors and materials should be used strategically in keeping with the building's architectural theme.
- Building entries should be highlighted with pedestrian-scale elements to direct customers and employees to the entrance and distinguish it from the remainder of the building.
- Office buildings should be designed with a high window to wall ratio. The use of glass walls is encouraged. Spandrel glazing may be used to provide the illusion of glass for large portions of a building where structural elements constrict the use of full glass walls.
- Repetition of shapes, lines and dimensions should be strategically used to create a sense of architectural rhythm that visually unites the building features.

##### a. Architectural Styles

The General Office images are intended to guide the style of the architecture and detailing for development of multi-function buildings that create an inviting work place, see Figure 4.8.

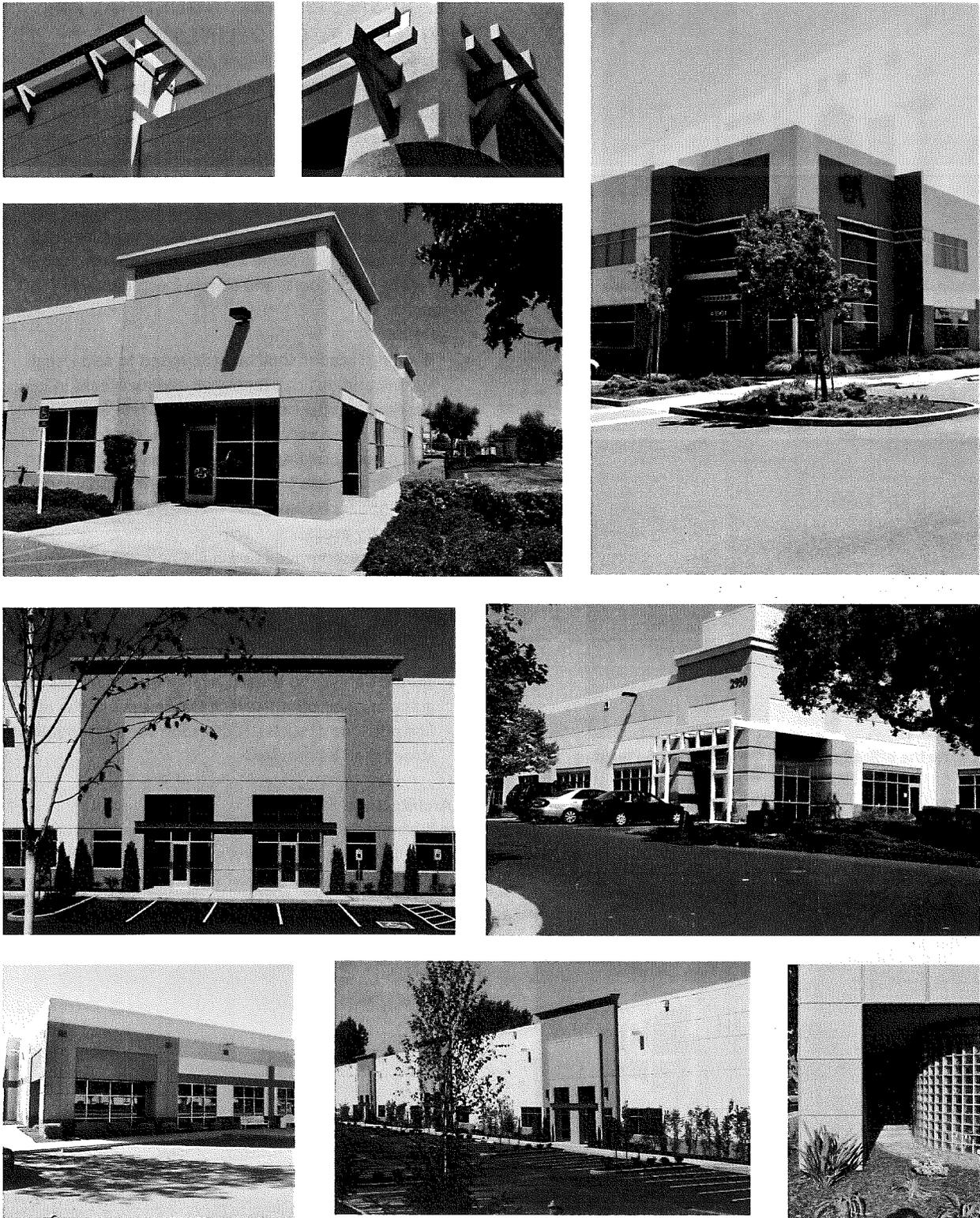


Figure 4.8, Typical General Office Architectural Styles



Clean, simple architecture and detailing



Locate the office function at the corner of the building



Provide architectural focal points at entries

#### 4.12 BUSINESS PARK INDUSTRIAL ARCHITECTURAL GUIDELINES

Buildings within the Business Park Industrial Zone will vary in size and function, but many will be very large warehouse/distribution or manufacturing facilities. In order to prevent long, straight building facades that are uninteresting and uninviting, these buildings will be designed with visual variety that may include color, changes in parapet wall height, score lines, and similar design elements without compromising the functional aspects necessary to serve the occupants, such as their large scale, dock doors, and simple (rectangular) shapes.

- Building facades shall be articulated to add visual variety and distinctiveness by adding breaks in long building facades at least every 200 feet in the form of score lines, varying roof heights, and/or color variations. Building entries shall be designed with the human scale in mind by concentrating windows and enhanced colors and materials at the office and entry areas.
- Metal is discouraged as a building's primary exterior except where the industrial nature of the use seems to mandate this type of construction. If metal buildings are found appropriate, decorative features, textural changes, or relief techniques should be used to break up large building faces and glass, brick or other surface treatments to the office portions of such structures in view of a public street shall be required.

##### a. Architectural Styles

The Business Park Industrial buildings presented in the images in provide the quality, general architectural styles and detailing for typical warehouse/distribution or manufacturing facilities for Cordes Ranch, see Figure 4.9.

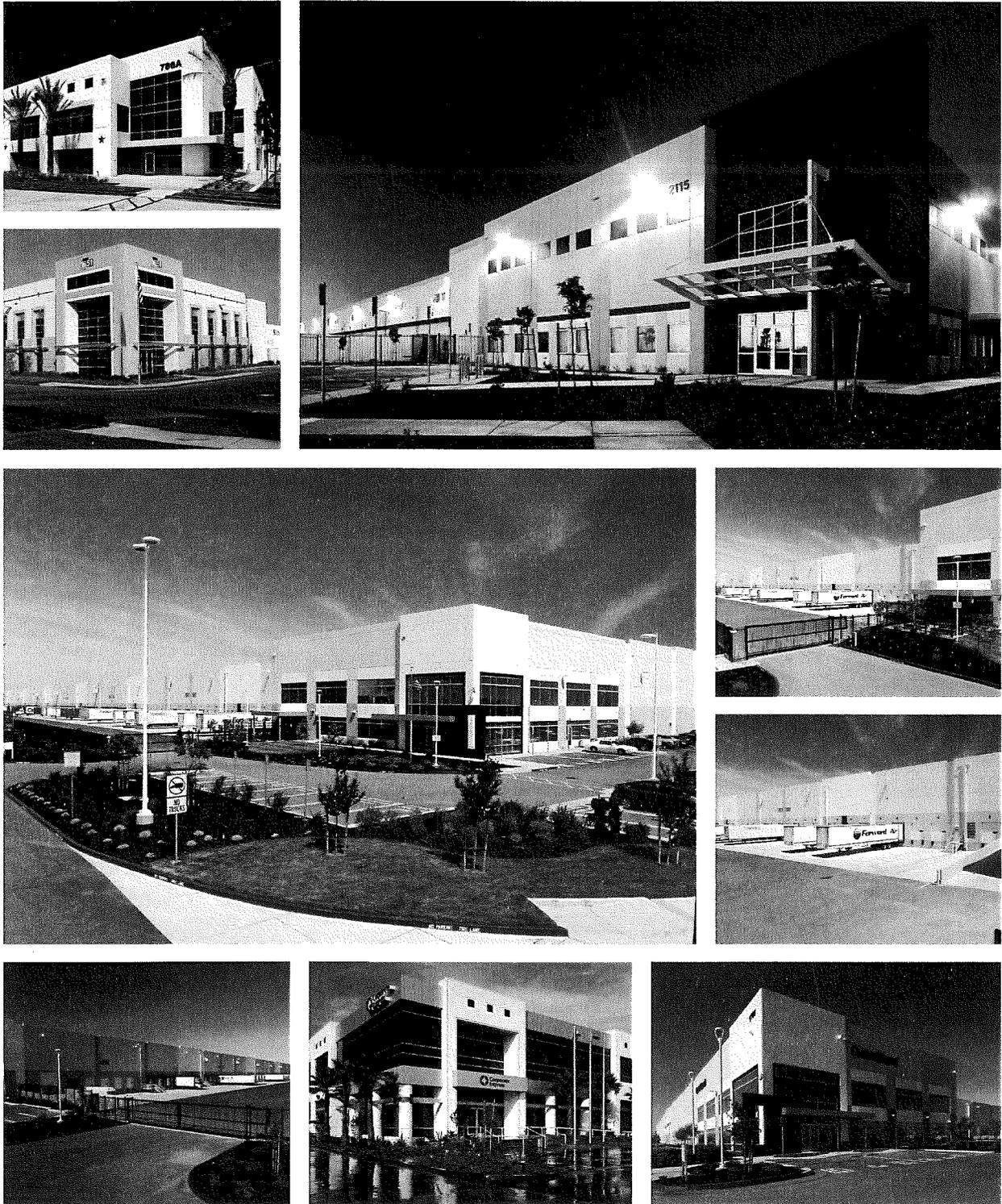


Figure 4.9, Typical Business Park Industrial Architectural Styles

